

Agenda

Planning Committee

Date: Wednesday, 10 June 2020

Time: 10.00

Venue: By Zoom Video Call

Contact: Craig Stewart, Committee Officer
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Dear Member

The Convener has directed that the powers contained in Section 43 of the Local Government in Scotland Act 2003 will be used and so Members will attend this meeting of the **Planning Committee** remotely. The business is shown on the attached agenda.

Yours faithfully

JOYCE WHITE

Chief Executive

Distribution:-

Councillor Jim Finn (Chair)
Bailie Denis Agnew
Councillor Gail Casey
Councillor Karen Conaghan
Councillor Ian Dickson
Councillor Diane Docherty (Vice Chair)
Councillor Daniel Lennie
Councillor Douglas McAllister
Councillor Marie McNair
Councillor Lawrence O'Neill

All other Councillors for information

Date of Issue: 28 May 2020

PLANNING COMMITTEE
WEDNESDAY, 10 JUNE 2020

AGENDA

1 APOLOGIES

2 DECLARATIONS OF INTEREST

Members are invited to declare if they have an interest in any of the items of business on this agenda and the reasons for such declarations.

3 MINUTES OF PREVIOUS MEETING 5 – 24

Submit for approval as a correct record, the Minutes of Meeting of the Planning Committee held on 13 May 2020.

4 NOTE OF VISITATION 25

Submit, for information, Note of Visitation carried out on 12 May 2020.

5 OPEN FORUM

The Committee is asked to note that no open forum questions have been submitted by members of the public.

6 PLANNING APPLICATIONS

Submit reports by the Strategic Lead – Regulatory in respect of the following planning applications:-

- (a) DC19/260 – Erection of 2 six storey flatted blocks, comprising a total of 48 units with associated road access, parking and infrastructure, open space and landscaping at land on Dumbarton Road, Old Kilpatrick by The Link Group Ltd.

27 - 46

- (b) DC20/018 – Demolition of former St Cuthbert's Church and the erection of 24 flatted residential units at Dalton Avenue, Clydebank by Clydebank Housing Association.

47 – 62

- (c) DC20/076 – Erection of storage facility and ancillary works at 2 Auld Street, Clydebank by The Storage Zone.

63 – 74

**7 QUEENS QUAY DESIGN CODES: CONSULTATION
RESPONSES AND STREET NAMING STRATEGY**

75 - 147

Submit report by the Strategic Lead – Regulatory informing on the main points of the representations received from the consultation exercise, details of the Proposed Street Naming Strategy and advising on the next steps.

PLANNING COMMITTEE

At a Meeting of the Planning Committee held by Conference Call on Wednesday, 13 May 2020 at 10.00 a.m.

Present: Bailie Denis Agnew and Councillors Gail Casey, Karen Conaghan, Ian Dickson, Diane Docherty, Daniel Lennie, Marie McNair and Lawrence O'Neill.

Attending: Pamela Clifford, Planning, Building Standards and Environmental Health Manager; Ross Lee, Lead Planning Officer; Gail Macfarlane, Strategic Lead – Roads & Transportation; Nigel Ettles, Section Head – Litigation (Legal Officer) and Craig Stewart, Committee Officer.

Apologies: Apologies for absence were intimated on behalf of Councillors Jim Finn and Douglas McAllister.

Councillor Diane Docherty in the Chair

CHAIR'S REMARKS

Councillor Docherty, Vice Convener, welcomed everyone to the May meeting of the Planning Committee which was being held remotely, in terms of Section 43 of the Local Government in Scotland Act 2003.

Accordingly, the Vice Convener advised that a process/procedure had been developed for the meeting (a copy of which had previously been circulated to Members), and that the Legal Officer would take charge of the meeting and would manage the business of the meeting in this regard. Thereafter, the Committee agreed to note the procedure in place for the conduct of the meeting and the meeting then commenced by conference call.

Note: Councillor Lennie left the meeting at this point in the proceedings.

DECLARATIONS OF INTEREST

Councillor Docherty declared an interest in Planning Application DC20/003 on the agenda and advised that she would take no part in the decision on the application. It was noted that Councillor Conaghan would take the Chair for that item only.

MINUTES OF PREVIOUS MEETING

The Minutes of Meeting of the Planning Committee held 11 March 2020 were submitted and approved as a correct record.

NOTE OF VISITATIONS

A Note of Visitations carried out on 10 March 2020, a copy of which forms Appendix 1 hereto, was submitted and noted.

OPEN FORUM

The Committee noted that no open forum questions had been submitted by members of the public.

PLANNING APPLICATIONS

Reports were submitted by the Strategic Lead – Regulatory in respect of the following planning applications:-

- (a) DC19/264 – Erection of three storey extension to Golden Jubilee National Hospital, Agamemnon Street, Clydebank by National Health Service (NHS).**

Reference was made to a site visit which had been undertaken in respect of the above application. The Planning, Building Standards and Environmental Health Manager was then heard in further explanation of the report.

The Legal Officer invited Mr Alan Campbell, objector, to address the Committee. Mr Campbell was heard in respect of his representation and in answer to Members' questions.

The Legal Officer then invited Mr John Scott, applicant, to address the Committee. Mr Scott was heard in support of the application and in answer to Members' questions.

After consideration and having heard the Planning, Building Standards and Environmental Health Manager and the Lead Planning Officer in answer to Members' questions, the Committee agreed to indicate that it was minded to grant planning permission and that authority be delegated to the Planning, Building Standards and Environmental Health Manager to issue the decision subject to the conditions set out in Section 9 of the report, as detailed within Appendix 2 hereto, and subject also to the satisfactory conclusion of a legal agreement or other suitable mechanism to secure the payment of a contribution towards the improvement of sustainable modes of transport.

Note: Councillor O'Neill left the meeting during consideration of the above item.

Councillor Docherty withdrew from the Chair, having previously declared an interest in the following item, and accordingly Councillor Conaghan took over the chairing of the meeting for this item of business only.

(b) DC20/003 – Planning Permission in Principle for three houses at land at former site of Dunclotha at Parkhall Road, Clydebank by Mr B. Donaghy.

The Planning, Building Standards and Environmental Health Manager was heard in further explanation of the report.

The Legal Officer invited Mr William Burke, objector, to address the Committee. Mr Burke was heard in respect of his representation and in answer to Members' questions.

It was noted that the applicant had not requested to be heard.

After consideration and having heard the Planning, Building Standards and Environmental Health Manager in answer to Members' questions, the Committee agreed to refuse planning permission for the reasons set out in Section 9 of the report, as detailed within Appendix 2 hereto.

Note: At this point, Councillor Docherty resumed the chair for the remainder of the business of the meeting.

(c) DC20/028 – Erection of 62 unit residential development with associated access road, infrastructure, open space and landscaping at land adjacent to Miller Street, Clydebank by Miller Homes.

After discussion having heard the Planning, Building Standards and Environmental Health Manager, Lead Planning Officer and the Strategic Lead – Roads & Transportation in further explanation, and in answer to Members' questions, the Committee agreed to indicate that it was minded to grant planning permission and that authority be delegated to the Planning, Building Standards and Environmental Health Manager to issue the decision subject to the conditions set out in Section 9 of the report, as detailed within Appendix 2 hereto, and subject also to the satisfactory conclusion of a legal agreement or other suitable mechanism to secure a financial payment towards open space provision in the local area.

**STREET NAME FOR NEW HOUSING DEVELOPMENT AT FORMER SITE OF
HALDANE PRIMARY SCHOOL, MILLER ROAD, ALEXANDRIA BY WEST
DUNBARTONSHIRE COUNCIL**

A report was submitted by the Strategic Lead – Regulatory recommending the allocation of street names to roads within the new housing development at the former Haldane Primary School site.

After discussion and having heard the Planning, Building Standards and Environmental Health Manager in further explanation of the report and in answer to Members' questions, the Committee agreed that Inler Drive and Inler Place be allocated as street names for the development.

The meeting closed at 12 noon.

PLANNING COMMITTEE

NOTE OF VISITATIONS – 10 MARCH 2020

Present: Bailie Denis Agnew and Councillors Karen Conaghan, Ian Dickson, Diane Docherty, Jim Finn and Marie McNair.

Attending: Pamela Clifford, Planning, Building Standards and Environmental Health Manager; Erin Goldie, Team Leader – Development Management and Ross Lee, Lead Planning Officer.

SITE VISITS

Site visits were undertaken in connection with the undernoted planning applications-

(1) Sheephill Quarry, Milton, Dumbarton

WP/98/076 & DC02/447 – Review of Minerals Permission and extension at Sheephill Quarry, Milton, Dumbarton by William Thompson.

(2) Land opposite Dalreoch Quarry, Renton Road, Dumbarton

DC19/200 – Change of use from vacant land to one pitch caravan park, erection of amenity block etc. (Retrospective) at Land opposite Dalreoch Quarry, Renton Road, Dumbarton by Mr James Brown.

DC19/264 – Erection of three storey extension to Golden Jubilee National Hospital, Agamemnon Street, Clydebank by National Health Service (NHS).

MINDED TO GRANT planning permission and that authority be delegated to the Planning, Building Standards and Environmental Health Manager to issue the decision subject to the following conditions, and subject also to the satisfactory conclusion of a legal agreement or other suitable mechanism to secure the payment of a contribution towards the improvement of sustainable modes of transport:-

1. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development on site, exact details and specifications of all proposed external materials for the extension (including the rooftop plant and the adjoining electrical servicing building) shall be submitted to and approved in writing by the Planning Authority. Thereafter, these shall be implemented as approved.
2. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development on site, full details of all hard surfaces and paths shall be submitted to and approved in writing by the Planning Authority. Thereafter, these shall be implemented as approved.
3. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development on site, full details of the design and location of all walls and fences to be erected (or retained) on site shall be submitted to and approved in writing by the Planning Authority. These details shall also include all boundary treatments and means of enclosure associated with the proposed detached 'Pump House' and the adjoining 'Electrical Servicing Building'. Thereafter, these shall be implemented and/or retained as approved.
4. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development on site, details of a landscaping scheme for the site shall be submitted to and approved in writing by the Planning Authority. Thereafter, the approved landscaping scheme shall be implemented no later than the next appropriate planting season after the occupation of the extension. The scheme shall include details of the maintenance arrangements and the landscaping shall thereafter be maintained in accordance with these details.
5. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development on site, exact details of the design, appearance, height and scale of the equipment, features and buildings for the detached 'Pump House' and the 'Stand-by Generator Units' compounds alongside their means of enclosure as shown on approved plan 'Site Plan as Proposed (Drawing No. GJ2-IBI-XX-ZZ-DR-A-11-004 Rev2)' shall be

submitted to and approved in writing by the Planning Authority. Thereafter, these shall be implemented as approved and maintained as such in perpetuity unless otherwise agreed in writing by the Planning Authority.

6. Prior to the occupation of the approved 'Surgical Centre' extension, all windows installed across the full northern elevation of the extension shall have obscured/opaque glazing. A sample of the obscured/opaque glazing shall be submitted to and approved in writing by the Planning Authority. Once installed and in place, these windows shall be maintained as such in perpetuity for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.
7. Prior to the occupation of the approved 'Surgical Centre' extension, a masterplan for the wider Golden Jubilee site shall be submitted to and approved by the Planning Authority. The masterplan shall include details of any future extensions, car parks, the development of sustainable modes of transport and facilities, landscaping, open space and linkages with the waterfront, other waterfront sites and the wider area.
8. Prior to the commencement of development on site, a Bird Hazard Management Plan shall be submitted to and approved in writing by the Planning Authority. The submitted plan shall include details of management of any flat/shallow pitched roofs within the site which may be attractive to nesting, roosting and loafing birds. Thereafter, the Bird Hazard Management Plan shall be implemented as approved following completion of the building and shall remain in force for the life of the building. No subsequent alterations are to be made to the management plan without first being submitted to and approved in writing by the Planning Authority.
9. Prior to the commencement of development on site, details of all external lighting including floodlights associated with the development and associated features and infrastructure shall be submitted to and approved in writing by the Planning Authority. All external lighting shown shall be positioned or designed as such that they do not directly face towards or shed light onto the intertidal habitat of the Inner Clyde Special Protection Area to the south of the site or the residential properties to the north of the site. Thereafter, the approved external lighting arrangements shall be erected in strict accordance with the approved details and maintained as such in their approved positions. No changes to the location or positioning of the lighting shall be undertaken unless otherwise agreed in writing by the Planning Authority.
10. Prior to the commencement of development on site, details of a 2 metre high screen fence to be erected along the entire southern boundary of the site is submitted to and approved in writing by the Planning Authority. The fence shall be comprised of a fully opaque material such that human activity within the site shall not be visible to birds utilising any area of the Inner Clyde Special Protection Area lying within 150 metres of the development site. Thereafter the approved screen shall be erected on site and must be maintained in an adequate condition throughout all construction activity taking place between

the 15th September to 30th March (inclusive). It shall be removed on completion of the construction activity associated with the development.

11. All piling works associated with the construction of the development which are being undertaken during the period of the year from the 15th September to the 30th March shall be undertaken using a helical displacement method.
12. All works and associated activity related to the breaking up of the hard-standing of the temporary car parks associated with the development shall only take place in the period of the year between the 1st April and 14th September inclusive.
13. Prior to the commencement of development on site, details of the Sustainable Urban Drainage System (SUDS) and its maintenance following installation shall be submitted for the further written approval of the Planning Authority and implemented as approved. The SUDS scheme shall thereafter be formed in accordance with the approved details prior to the occupation of the approved 'Surgical Centre' extension and maintained as such thereafter.
14. Notwithstanding the Travel Plan (Dated: August 2018) submitted, prior to the occupation of the approved 'Surgical Centre' extension, a revised and updated 'Green Travel Plan' shall be submitted to and approved in writing by the Planning Authority. This 'Green Travel Plan' shall provide (but not be limited to) the following:
 - Specific details demonstrating how sustainable travel information will be conveyed and provided to staff, patients and visitors alongside details and figures regarding flexible working patterns and remote working for staff.
 - Details regarding how pedestrians and cyclists will be given priority within the site, including through the provision of Cycle Friendly Employer Level Facilities.
 - A Parking Management Plan for remaining car trips which outlines measures to prioritise parking for essential users and actively discourage car travel by other users. This shall encompass a consideration and an assessment of nearby road networks within the locality of the Hospital complex where there are currently no parking issues experienced by local residents and businesses.

Thereafter, the approved 'Green Travel Plan' and all associated measures and proposals shall be implemented prior to the extension coming into operational use and maintained as such in strict accordance with the approved 'Green Travel Plan' unless otherwise agreed in writing by the Planning Authority.

15. Further to condition 14 and eighteen months after the approved 'Surgical Centre' extension becomes fully operational, a parking review including a methodology statement, to first be agreed with the Planning Authority, shall be

undertaken to ascertain levels of parking and the associated effectiveness of the measures associated with the implementation of the approved Green Travel Plan. The review shall include the percentage uptakes of sustainable travel methods among staff, patients and visitors as proposed in the Green Travel Plan and establish whether there are any parking related problems associated with the development. The findings and recommendations of the review shall be submitted for the written approval of the Planning Authority and any actions recommended in the review shall be implemented in a timescale agreed with the Planning Authority.

16. The approved remediation scheme (Remediation Strategy, Dated August 2019) shall be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Planning Authority. The Planning Authority shall be notified in writing of the intended commencement of remediation works not less than 14 days before these works commence on site. Upon completion of the remediation works and prior to the site being occupied, a verification report which demonstrates the effectiveness of the completed remediation works shall be submitted to and approved in writing by the Local Planning Authority.
17. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
18. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall also be free from metals, plastic, wood, glass, tarmac, paper and odours. On completion of the works and at a time and or phasing agreed by the Planning Authority, the developer shall submit a validation report containing details of the source of the material and associated test results to demonstrate its suitability for use.
19. Prior to the approved 'Surgical Centre' extension becoming operational on site, all maximum noise output limitation and noise attenuation measures shall be implemented and installed in full accordance with the details set out in the approved 'Plant Noise Impact Assessment' Report (Rev 01, Dated 17th April 2020). Once installed and in place, the noise output level limitations and associated attenuation measures shall thereafter be retained and maintained as such on site for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.

20. Further to Condition 19 above, within 4 weeks of the approved 'Surgical Centre' extension becoming operational on site, a 'Verification Report' demonstrating compliance and validation of the maximum noise projections and noise attenuation measures detailed within the approved 'Plant Noise Impact Assessment' Report (Rev 01, Dated 17th April 2020) shall be submitted to and approved in writing by the Planning Authority. This report shall also include an assessment of the effectiveness of the noise mitigation measures associated with the detached 'Pump House' and 'Back-up Generators' pursuant to Condition 5 above. This report shall be prepared and undertaken by an independent consultant and the measurements shall be carried out in accordance with BS4142:2014 – "Methods of rating and assessing industrial and commercial sound" (with respect to current best practice). These levels and measures as agreed and validated within the approved verification report shall be maintained for the lifetime of the development thereafter unless otherwise agreed in writing by the Planning Authority.
21. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise sources upon nearby residential properties and other noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
22. During the period of construction, all works and ancillary operations (including piling) which are audible at the site boundary (or at such other place(s) as may first be agreed in writing with the Planning Authority), shall be carried out between the following hours unless otherwise approved in writing by the Planning Authority:
- Mondays to Fridays: 0800-1800
 - Saturdays: 0800-1300
 - Sundays and public holidays: No working
23. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This statement shall include an assessment of the impact of the piling on surrounding properties, taking into account the guidance contained in BS 6472: 1984 'Evaluation of Human Response to Vibration in Buildings'. It shall detail any procedures which are proposed to minimise the impact of noise and vibration on the occupants of surrounding properties. This statement shall be prepared by a suitably qualified person, and the piling works shall thereafter be carried out in accordance with the approved method statement.
24. No development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing by the Planning Authority. The scheme shall identify likely sources of dust

arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.

25. Prior to the occupation of approved 'Surgical Centre' extension, the developer shall install the necessary infrastructure to enable the full development and all associated properties to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.

DC20/003 – Planning Permission in Principle for three houses at land at former site of Dunclutha at Parkhall Road, Clydebank by Mr B. Donaghy.

REFUSE planning permission for the following reasons:-

1. The proposal is contrary to Policies H5, GD1, E4 and E5 of the West Dunbartonshire Adopted Local Plan (2010), Policies BC4 and GN5 of the West Dunbartonshire Local Development Plan 1: Proposed Plan (2016) and Policies H4 and E4 of the West Dunbartonshire Local Development Plan 2: Proposed Plan (2018) as the proposal is considered to be over development of the site that would result in the loss of existing trees which are of significant amenity value and contribute to the appearance and character of the wider residential area.
2. An appropriate Tree Survey has not been submitted in order to demonstrate that the development will not compromise or adversely impact upon both protected and unprotected trees within the site and the neighbouring site. Due to the absence of such information, the proposal is therefore contrary to the specific requirements of Policy E5 of the West Dunbartonshire Adopted Local Plan (2010).
3. The proposed three houses are likely to have a lower level of amenity due to inadequate sun penetration or daylighting by reason of site and plot orientation and proximity to existing trees which may lead to the loss of further trees in the future that make a valuable contribution to the character and amenity of the area.
4. An appropriate Habitat/Ecological Assessment has not been submitted in order to demonstrate that the proposal will not compromise and detrimentally impact upon potential biodiversity and wildlife within the site and the immediate surrounding area. Due to the absence of such information the proposed development is contrary to policy E1 of the West Dunbartonshire Local Plan (2010), policy GN5 of the West Dunbartonshire Local Development Plan 1: Proposed Plan (2016) and policy ENV1 of the West Dunbartonshire Local Development Plan 2: Proposed Plan (2018).

DC20/028 – Erection of 62 unit residential development with associated access road, infrastructure, open space and landscaping at land adjacent to Miller Street, Clydebank by Miller Homes.

MINDED TO GRANT planning permission and that authority be delegated to the Planning, Building Standards and Environmental Health Manager to issue the decision subject to the following conditions, and subject also to the satisfactory conclusion of a legal agreement or other suitable mechanism to secure a financial payment towards open space provision in the local area:-

1. Prior to the commencement of development on site, exact details, specifications and samples of all proposed external materials to be used for the houses within the development site shall be submitted to and approved in writing by the Planning Authority. This shall include details of all materials and finishes as scheduled and annotated on the approved plan 'Site Layout (Drawing No. 001 Rev B)'. Thereafter, the development shall be completed in strict accordance with the approved material details and palette.
2. Further to condition 1 above, the brick type to be used for the elevation treatment of all houses within the development site shall be of the 'Western Blend Precon (Masonry Finish)' specification and variety. For the avoidance of doubt, the details, specification and samples of the final brick type provided in response to Condition 1 shall be predominantly red in tone and colour and shall be of tumbled finish.
3. Further to Conditions 1 and 2 above, prior to the approved brickwork associated with any house being constructed or installed on site, a sample panel of this brickwork shall be constructed on site in order for it and the associated mortar to be reviewed, inspected and approved in writing by the Planning Authority. Thereafter, the development shall be completed in strict accordance with the approved brick details.
4. Notwithstanding the plans hereby approved including 'Site Layout (Drawing No. 001 Rev B)', prior to the commencement of development on site, full details of all hard and soft boundary treatments for across the site shall be submitted to and approved in writing by the Planning Authority. These details shall also include a variety of boundary treatments including the use of brick walls for the front, side and rear curtilages of residential properties within the site. Thereafter, the approved boundary treatments arrangements shall be installed prior to the occupation of any associated houses unless otherwise agreed in writing by the Planning Authority and maintained as such thereafter for the lifetime of the development.
5. Prior to the commencement of development on site, details of alternative boundary treatments and means of enclosure for the existing substation building located on the north east corner of the site shall be submitted to and approved in writing by the Planning Authority. The details shall include a brick treatment and the approved boundary treatment details shall be undertaken and completed prior to the occupation of any houses within the site and

maintained as such thereafter for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.

6. Prior to the commencement of development on site, full details of all path upgrade works as indicated on approved plan 'Site Layout (Drawing No. 001 Rev B)', including material finishes, surfaces, levelling works and associated landscaping shall be submitted to and approved in writing by the Planning Authority. This shall include specific details for the works to the existing footpath located on the north eastern side boundary of the development site and the existing path located immediately neighbouring the site to the west. Thereafter, the path upgrade and improvement works approved shall be undertaken in full and completed prior to the occupation of any houses within the site unless otherwise agreed in writing by the Planning Authority.
7. Prior to the commencement of development on site, full details of the design, appearance, height and location of all retaining walls and other retention features and associated levelling works across the site shall be submitted to and approved in writing by the Planning Authority. Thereafter, the agreed retention and levelling details shall be implemented prior to the occupation of any associated houses unless otherwise agreed in writing by the Planning Authority. No changes or deviations from the approved and agreed levels and retention arrangements shall be undertaken without the further written consent of the Planning Authority.
8. The development hereby approved shall be constructed strictly in accordance with the finished site levels and finished floor levels as shown on approved plan 'Levels Layout for Planning (Drawing No E03 Rev A)'. Any alterations to these finished site and floor levels shall first be agreed in writing with the Planning Authority.
9. Prior to the commencement of development on site, details of the design and location of cycle storage provision, street furniture (including bin stores) and external lighting, shall be submitted to and approved in writing by the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of any of the houses and thereafter maintained, unless otherwise agreed in writing with Planning Authority.
10. The soft landscape arrangements approved under drawing 'Landscape Proposals (Drawing No.109/103/02 Rev C)' including the associated planting schedule and maintenance arrangements shall be implemented no later than the next appropriate planting season or after occupation of the 30th property. Any trees, shrubs or plants forming part of the approved landscape scheme which die, are removed or become seriously damaged or diseased, within a period of 5 years from the date of their planting, shall be replaced in the next planting season with others of similar sizes and species unless the Planning Authority gives written approval to any variation. The landscaping arrangements as approved shall thereafter be maintained in accordance with these details for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.

11. Notwithstanding approved plan 'Play Area Landscape Proposals (Drawing No. 109.103.02 Rev B)', a revised 'Play Equipment Strategy' for the site shall be submitted to and approved in writing by the Planning Authority. This shall include (but not be limited to) the following:

- Incorporate the reuse of existing trees within the site which are being felled as part of the development to create natural forms of play equipment.
- Details and specifications of other forms of natural play equipment and structures which offer design solutions that effectively navigate and manage the increase in gradient of the central public open space area within the site.
- Provide full details regarding the maintenance arrangements for all play equipment and associated features within the site.

Thereafter, the approved play equipment strategy and all associated features and specifications shall be installed prior to the completion of the 30th unit within the development unless otherwise in agreed in writing by the Planning Authority and maintained in accordance with the approved details for the lifetime of the development.

12. Prior the commencement of development on site and further to Sections 4.2.2 and 4.3.3 of the approved 'Extended Phase 1 Habitat Survey (Dated September 2019)', both an otter and bat survey and report (prepared and undertaken by a competent and qualified ecologist) shall be submitted to and approved writing by the Planning Authority. These surveys shall be undertaken a minimum of 6 weeks prior to any construction work commencing on site. Thereafter, any recommendations or mitigation measures contained within the reports shall be implemented prior to works commencing on site and shall be maintained as such for the lifetime of the development.

13. All construction work on site relevant to the application shall be carried out in accordance with the recommendation and mitigation measures outlined within Section 4 of the approved 'Extended Phase 1 Habitat Survey' (Dated September 2019). These measures and recommendations shall remain in effect until all construction work associated with the development is complete on site.

14. Further to condition 12 and 13 above, all ground or vegetation clearance works, including any tree felling or demolition works, shall take place outwith the main bird breeding season (i.e. outwith the period of April to July inclusive), and no demolition or ground or vegetation clearance works are permitted between April to July in this respect. If this is not possible, a suitably qualified ornithologist/ecologist shall be engaged to survey any buildings, grounds and trees immediately prior to such works to advise the applicant/contractor/developer of a bird nesting activity and of any actions required to protect birds.

15. Prior to the occupation of the 30th housing unit within the site, the drainage of surface water shall be completed in accordance with the approved Sustainable Urban Drainage System (SUDS) design, as set out in the approved 'Drainage Strategy Report' (Dated August 2019) and the approved plans. The SUDS and associated features including the planted swales once installed shall thereafter be maintained on site in accordance with the approved details, unless otherwise agreed in writing by the Planning Authority.
16. No house shall be occupied within the site until the vehicle parking spaces associated with that house unit have been constructed provided within the site in accordance with approved plan 'Site Layout (Drawing No. 001 Rev B)'. Further to the approved plans, each vehicular parking space shall be constructed to include a marking which clearly identifies it to the house number it is to be attributed within the site or alternatively be marked to identify it as a visitor parking space where relevant. The aforementioned parking shall thereafter be retained and be capable of use at all times and shall not be removed or altered without the prior written approval of the Planning Authority.
17. Further to condition 16 above and unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development with the site, details of the location and design of an electric charging point(s)/unit(s) to serve the development shall be submitted to and approved in writing by the Planning Authority. The approved car charging point(s)/unit(s) and associated infrastructure shall thereafter be installed in accordance with the approved details at a timescale agreed by the Planning Authority and maintained as such thereafter.
18. Notwithstanding the plans hereby approved including 'Site Layout (Drawing No. 001 Rev B)', specific details of traffic calming measures and features to be incorporated into the junctions and accesses at Kilbowie Road and Miller Street from within the development site shall be submitted to and approved in writing by the Planning Authority. The measures proposed shall include the use of speed tables or similar features and shall all be designed with the intention to inform drivers that the road network is residential in nature. Thereafter, the approved traffic calming measures and features shall be installed prior to the occupation of any house within the site and shall be maintained as such within the site for the lifetime of the development.
19. Within twelve months of the occupation of the last house within the development and no sooner than 6 months after full occupation, a vehicle count survey shall be undertaken to ascertain volumes and levels of vehicle movements entering and exiting the site from Kilbowie Road and Miller Street respectively. The findings and recommendations of the survey shall be submitted to and approved in writing by the Planning Authority. If required by the review, any measures to reduce or deter such vehicle movements and address any traffic issues identified shall be formed and installed on site at a timescale to be agreed in writing by the Planning Authority. Thereafter, these

measures shall be maintained as such within the site for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.

20. Unless otherwise agreed in writing, no development shall commence on site until a Travel Plan which includes details promoting sustainable travel to and from the site and appropriate measures and actions to reduce car dependence for the development is submitted for the written approval of the Planning Authority. The Travel Plan shall include details of the contents of an information pack which will be provided to future occupants of the development to ensure that they are aware of their public transport and active travel options available within the area. The approved Travel Plan and all associated measures and actions shall be in place and implemented in full prior to the occupation of the 1st house associated with the development and maintained as such unless otherwise agreed in writing by the Planning Authority
21. Notwithstanding the 'Ground Investigation Report' (Dated February 2020) submitted, no development (other than investigative works) shall commence on site until such time as a detailed report on the nature and extent of any contamination of the site has been submitted to and approved in writing by the Planning Authority. The report shall be prepared by a suitably qualified person and shall include the following:
 - a) A detailed site investigation identifying the extent, scale and nature of the contamination on the site (irrespective of whether this contamination originates on the site).
 - b) An assessment of the potential risks (where applicable) to:
 - Human health
 - Property (existing and proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes
 - Groundwater and surface waters
 - Ecological systems
 - Archaeological sites and ancient monuments
 - c) An appraisal of remedial options, including a detailed remediation scheme based on the preferred option.
22. No development (other than investigative works) shall commence on site until such time as a detailed remediation scheme for the site has been submitted to and approved in writing by the Planning Authority. The scheme shall be prepared by a suitably qualified person and shall detail the measures necessary to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property, and the natural and historical environment. The scheme shall include details of all works to be undertaken, the remediation objectives and criteria, a timetable of works and/or details of the phasing of works relative to the rest of the development, and site management procedures. The scheme shall ensure that upon completion of the remediation works the site will not qualify

as contaminated land under Environmental Protection Act 1990 Part IIA in relation to the intended use of the land after remediation.

23. The approved remediation scheme shall be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Planning Authority. The Planning Authority shall be notified in writing of the intended commencement of remediation works not less than 14 days before these works commence on site. Upon completion of remediation works and prior to any house being occupied, a verification report which demonstrates the effectiveness of the completed remediation works shall be submitted to and approved in writing by the Planning Authority.
24. If required, a monitoring and maintenance scheme to include monitoring the long-term effectiveness of the proposed remediation over a period of years determined by the scheme shall be submitted to and approved by the Planning Authority. Any actions ongoing shall be implemented within the timescale agreed by the Planning Authority in consultation with Environmental Health measures. Following completion of the actions/measures identified in the approved remediation scheme a further report which demonstrates the effectiveness of the monitoring and maintenance measures shall be submitted to and approved in writing by the Planning Authority.
25. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
26. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall be free from metals, plastic, wood, glass, tarmac, paper and odours. Prior to placement of any of the material, the developer shall submit a validation report for the approval in writing of the Planning Authority and it shall contain details of the source of the material and associated test results to demonstrate its suitability for use. Thereafter the development shall be undertaken in accordance with the approved details.
27. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This statement shall include an assessment of and take into account the following:
 - The impact of the piling on surrounding properties.

- Detail any procedures which are required to minimise the impact of noise and vibrations on the occupants of surrounding properties.

This statement as submitted shall be prepared by a suitably qualified person and shall take into account the guidance contained in BS6472:1984 'Evaluation of Human Response to Vibration of Buildings'. The piling works shall thereafter be carried out in accordance with the approved method statement until they are completed on site.

28. No development shall take place on site until such time as a Noise Impact Assessment has been submitted to and approved in writing by the Planning Authority. This Noise Impact Assessment (alongside any recommendations in respect of attenuation measures) shall be prepared by a suitably qualified person and shall include an assessment of the potential for occupants of the development to experience noise nuisance arising from nearby industrial / commercial units and from the impact of road traffic noise. Where a potential for noise disturbance is identified, proposals for the attenuation of that noise shall be submitted to and approved in writing by the Planning Authority. Any such approved noise attenuation scheme shall be implemented prior to the occupation of any of the houses within the site and shall thereafter be retained in accordance with the approved scheme unless otherwise agreed in writing by the Planning Authority.
29. Further to Condition 28 above, within 4 weeks of the completion of the development on site, a 'Verification Report' demonstrating compliance and validation of the maximum noise projections and effectiveness of the noise attenuation measures detailed within the approved 'Noise Impact Assessment' Report (approved through Condition 25) shall be submitted to and approved in writing by the Planning Authority. This 'Verification Report' shall be prepared and undertaken by an independent consultant and the measurements shall be carried out in accordance with BS4142:2014 - "Methods of rating and assessing industrial and commercial sound" (with respect to current best practice). These levels and measures as agreed and validated within the approved verification report shall be maintained for the lifetime of the development thereafter unless otherwise agreed in writing by the Planning Authority.
30. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
31. During the period of construction, all external works including piling and ancillary operations shall be carried out between the following hours and at no other time, unless otherwise agreed in writing by the Planning Authority:

- Mondays to Fridays : 0800 – 1800
- Saturdays: 0800 – 1300
- Sundays and public holidays: No Working

32. Unless otherwise approved in writing by the Planning Authority, no development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing by the Planning Authority. The scheme shall identify likely sources of dust arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.
33. No development shall commence on site until details for the storage and the collection of waste arising from the development shall be submitted to and approved in writing by the Planning Authority. The agreed details shall be in place prior the occupation of the first housing unit/property within the site and thereafter maintained for the lifetime of the development.
34. Prior to the occupation of the first house within the site, the developer shall install the necessary infrastructure to enable the full development and all associated properties to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.

PLANNING COMMITTEE

NOTE OF VISITATION – 12 MAY 2020

Present: Bailie Denis Agnew and Councillor Lawrence O'Neill.

Attending: Pamela Clifford, Planning, Building Standards and Environmental Health Manager and Ross Lee, Lead Planning Officer.

SITE VISIT

A site visit was undertaken in connection with the undernoted planning application-

Golden Jubilee National Hospital, Agamemnon Street, Clydebank

DC19/264 – Erection of three storey extension to Golden Jubilee National Hospital, Agamemnon Street, Clydebank by National Health Service (NHS).

WEST DUNBARTONSHIRE COUNCIL

Report by Strategic Lead- Regulatory

Planning Committee: 10th June 2020

DC19/260: Erection of 2 six storey flatted blocks, comprising a total of 48 units with associated road access, parking and infrastructure, open space and landscaping at land on Dumbarton Road, Old Kilpatrick by the LINK Group Ltd.

1. REASON FOR REPORT

- 1.1** This application relates to a local development subject to a substantial number of objections and under the terms of the approved Scheme of Delegation, it therefore requires to be determined by the Planning Committee.

2. RECOMMENDATION

- 2.1** **Grant** planning permission subject to the conditions set out in Section 9 below.

3. DEVELOPMENT DETAILS

- 3.1** The site is located on the northern side of Dumbarton Road, Old Kilpatrick and extends to approximately 0.68 hectares and was historically part of the grounds of the former Dalnottar House. The site is bounded by established residential areas to the north, east and west and sits opposite to the Forth and Clyde Canal to the south. Until recently, the site previously had an industrial use with a warehouse and factory operated by Craft Steel Ltd. While the business has recently closed and is no longer in operation, the factory and warehouse buildings and hardstanding's remain in-situ.
- 3.2** The site comprises a rectangular parcel of land that has been cut into the hillside, with a generally level internal base where the buildings and hardstanding's are located, and sits at a lower level than the surrounding residential areas. There is a significant level change from the bottom of the site to its northern boundary which has a steep incline and is densely wooded; to the east and west the ground also rises, though not as steeply, and is also covered in shrubs and woodland. The trees on the western edge of the site at the existing entrance gate are subject to a Tree Preservation Order. At present, the site is bounded by a 1m stone wall on

its southern boundary facing onto Dumbarton Road with a gated entrance situated on the western section of the wall.

- 3.3** Planning permission is sought for 48 affordable housing units for rent comprising of 3 and 4 bedroom flatted properties in the form of two 6 storey point blocks. Four ground floor units will be wheelchair accessible and lifts are proposed to be installed within each block. Each block features a butterfly roofline on the gable elevation with solar panels on the sections facing south. A central courtyard area of amenity open space would be located between the two blocks and a further smaller amenity area would be located to the rear of the site. The blocks would have full clay brickwork elevations with areas of metal cladding and Juliet balconies on the south, east and west elevations. Boundary treatments for the site include the retention of the stone wall on the southern boundary along Dumbarton Road and 1.8 metre high timber fence arrangement along the north, east and west boundaries.
- 3.4** A new opening from Dumbarton Road will be formed to the east of the site and the existing access/egress will be infilled using stone from the down takings required to form the new access. Fifty-six parking spaces would be predominantly located to the rear of the site, with some also along the eastern boundary, in rows with complimentary screen planting and landscaping.
- 3.5** Supporting technical information has been provided as part of the application and this includes a Design and Access Statement, a Tree Survey Report, a Ground Investigation Report, a Topographical Survey, Utilities Search and a Coal Report.

4.0 CONSULTATIONS

- 4.1** West Dunbartonshire Council Roads Service have no objections subject to conditions regarding the provision of refuse servicing arrangements, disabled bays, cycle parking and arrangements for a future allocation of 4 additional parking spaces should they be required.
- 4.2** West Dunbartonshire Environmental Health Service have no objections subject to conditions regarding contaminated land and remediation, noise, dust, drainage and hours of operation.
- 4.3** Scottish Water, West of Scotland Archaeology Service have no objection to the proposed development.

5. REPRESENTATIONS

5.1 Seven representations from local residents have been received, comprising of six objections and one in support. A summary of the key points of objection are detailed below:

- Height of the development – at six storeys, the flatted blocks are too high for the location and are not in keeping with the other residential developments in area;
- Type of residence – there are already enough flats in the area and more are not required;
- Noise – noise generated during the construction of the development will impact on the wellbeing and amenity of nearby residents and noise once the development is occupied could lead to anti-social behaviour;
- Loss of daylight/sunlight – this will occur due to the height of the development and affect nearby properties;
- Loss of privacy – due to height of the development, this will result in the properties to the rear of the site being overlooked;
- Traffic – the development will lead to increased levels of traffic, congestion and pressure for parking on Dumbarton Road;
- Vibration – the piling of foundations during construction will adversely effect the stability of nearby residential properties;
- Dust and grit – the construction of the development will result in unacceptable levels of dust and grit being experienced by nearby residential properties;
- Contamination – the previous industrial use of the site will lead to problems of potential contamination;
- Japanese Knotweed – the development will result in the exacerbation of Japanese Knotweed at the north of the site;

Concerns were also raised with regards to loss of view and reduction in property values however these are not material planning considerations and will therefore not be discussed any further in this report. The above points of objection will be discussed in further detail in Section 7 below.

The representation in support of the application does not provide reasons for the support.

6. ASSESSMENT AGAINST THE DEVELOPMENT PLAN

West Dunbartonshire Adopted Local Plan 2010

6.1 While the site has previously been used for business and industry, it does not have any specific land use designation and is identified as being within

a predominantly residential area. Policy UR1 encourages the redevelopment and re-use of underused, vacant and/or derelict land and buildings for appropriate uses such as housing. Policy RD1 sets out the principle that new residential developments located on brownfield sites are preferable to those on Greenfield sites. Policy GD1 seeks to ensure that all new development is of a high quality of design and respects the character and amenity of the area. Policy H4 sets out standards expected of residential development, requiring high quality design and in terms of form, layout and materials. Policy H5 seeks to safeguard the character and amenity of existing residential areas.

- 6.2** Policy R2 specifies the open space provision required for all developments. Assessment of open space requirements has been undertaken against the more updated “Our Green Network” Planning Guidance (2015) in Section 7 below. Policies F1 and F2 aim to ensure that new development is not at risk from and does not increase the risk of flooding, and has suitable SUDs drainage infrastructure.
- 6.3** Policy T4 relates to the accessibility of new developments and requires sites to integrate with walking, cycling and public transport routes. Policy E4 and E5 relate to the protection of Tree Preservation Orders and that new development proposals are required to consider impacts on trees and incorporate suitable tree planting.
- 6.4** The proposal complies with the policies of the adopted local plan and is assessed fully in Section 7 below.

7. ASSESSMENT AGAINST MATERIAL CONSIDERATIONS

West Dunbartonshire Local Development Plan (LDP1) Proposed Plan

- 7.1** On 27 April 2016, the Planning Committee took a final decision not to accept the Local Development Plan Examination Report recommended modification in respect of including the Duntiglennan Fields site in Clydebank as a housing development opportunity, and therefore, as a result of the Scottish Ministers’ Direction, the Local Development Plan has remained unadopted but continues to be a material consideration in the determination of planning applications.
- 7.2** Similar to the Adopted Plan, the site is identified as being within an existing residential area. Policy DS1 sets out general expectations for the quality of new development, including that it be distinctive, adaptable, resource-efficient and easy to get to and move around, safe, pleasant and welcoming. Policy BC4 supports the principle of residential development within existing residential locations, provided there is no adverse impact

on neighbouring amenity or character of an area. It is considered that there will be no adverse impacts on either of these.

- 7.3** The consideration of policies DS1, GN2, DS3, DS6 and GN5 with regards to new residential development, green network, transport, flooding, drainage and trees are similar to that of the Adopted Plan. DS7 requires any potential site contamination issues to be addressed and DS3 requires significant travel generating uses to be located within 400 metres of a public transport network. DS1 seeks to ensure a high design quality in housing and being suitable for a mix of occupants rather than a specific demographic.
- 7.4** The Residential Development: Principles of Good Design Guidance applies to all developments of more than 3 units and has been taken account of in the proposed design and site layout. The proposed development is assessed against the Proposed Plan and the Residential Development Guidance in Section 7 below.

West Dunbartonshire Local Development Plan (LDP2) Proposed Plan

- 7.5** On 19th September 2018 the Planning Committee approved Local Development Plan 2: Proposed Plan for consultation. It is the Council's most up to date policy position and it is a material consideration in the assessment of planning applications. The Examination Report of LDP 2 was received on 22nd April 2020 and will be presented to the August Planning Committee for consideration together with the modified Local Development Plan 2. The findings of the Examination Report have not materially changed the content and form of the LDP 2 policies and land allocations used in the assessment of this application.
- 7.6** The site is not identified for housing in the Proposed Plan however it is still identified as being within an existing residential neighbourhood as per the Adopted Plan and Proposed Plan 1. Policy H4 requires that the residential amenity of the area is maintained.
- 7.7** Similarly to Policy DS1 of Proposed Plan 1, Policy CP1 seeks to ensure that housing is of a high quality, adaptable and is designed to be suitable for a mix of occupants. It indicates that all new development will be expected to contribute towards creating successful places by having regard to the six qualities of a successful place (distinctive, adaptable, resource efficient, easy to get to/move around, safe and pleasant, and welcoming).
- 7.8** Policies GI2, ENV4, and ENV6 are similar to the green network, tree, and flooding policies of the Adopted and Proposed Plan 1 policies. Policy CP2 requires the integration and consideration of green infrastructure from the

outset of the design process through to its maintenance and stewardship of the resource.

- 7.9** For applications referred to the Place and Design Panel, Policy CP4 is relevant requiring the outcomes to be taken into account and responded to by the applicant. Policy CON1 requires that significant travel generating uses are designed to encourage sustainable transportation. Policy ENV8 requires developments to address air quality, lighting and noise as part of the planning process, whilst policy ENV9 requires all potentially contaminated sites to be remediated where necessary to ensure that the site is suitable for the intended use. It is considered that the proposals comply with the above policies as discussed below.

Principle of Development

- 7.10** While the application site has been used for industrial and business purposes, it is not allocated for this use in the Adopted and Proposed Plans. Instead the site is identified as being within an existing and established residential area and therefore it does not require to be assessed against the industrial and business policies of these Plans. Now that the former industrial and business use is no longer in operation, the site is vacant and considered brownfield land. Its redevelopment for residential purposes would continue the pattern of residential development in the locality and be consistent with the applicable land use policies of the adopted and proposed plans which seek to prioritise the regeneration of vacant brownfield sites. The principle of the redevelopment of this site for housing is therefore acceptable.

Site Layout, Design and Appearance.

- 7.11** The proposed development would introduce a new, distinctive and strong frontage to this section of Dumbarton Road which is generally secluded given the predominance of woodland cover along the roadside. Established flatted properties already exist to the east of the site at Freeland's Crescent and Woodlands Court further west. The proposed development would therefore continue this pattern of residential development while enhancing and activating the overall streetscape along Dumbarton Road.
- 7.12** The layout of the site has been carefully considered taking its cues from the surrounding landscape, site topography and existing residential context. The two separate, six storey, flatted blocks are set back from Dumbarton Road which respects the existing street pattern in the immediate area. The blocks are located centrally within the level part of the site and are separated by 440 square metres of quality amenity open

space. Four new footpaths would ensure good connections with the site and Dumbarton Road: one along each edge of the central amenity space providing a link to the rear of the site and another smaller amenity space for residents and one each to the entrance door to each block. The layout provides a new vehicular entrance from Dumbarton Road on the east of the site which goes around the northern perimeter of the blocks leading to the onsite parking which is located to the rear of the site. The blocks have been orientated to take advantage of a range of views including the central amenity space as well as open views to the east and west along the Clyde. The site layout has embraced the principles of green infrastructure as advocated in policies CP2 and GI2 of Proposed Plan 2 as it integrates well with the surrounding dense woodland throughout the north, east and west of the site and proposes further landscaping to strengthen this integration such as further tree, shrub and wildflower planting. The form and layout of the development is considered to be acceptable.

- 7.13** The scale and massing of the two point blocks has been well considered in terms of height and building proportions and can be well accommodated within the site topography and surrounding context and is considered to be acceptable. Both blocks are of contemporary design and will create a presence and activate the street scene on Dumbarton Road. The surrounding rising topography and dense woodland backdrop will provide the development with a quality and natural setting. A simple and coherent palette of materials are proposed for the development consisting of contrasting light and dark clay bricks of neutral colour tones to break up the mass of each block, highlighting the vertical elements of the building along with the recessed frontages, while the use of metal cladding and Juliet balconies provides further visual variety and interest. A butterfly roof arrangement with a low pitch using a single ply roofing membrane provides visual interest and character to the roofscape as well as a discreet nod to the industrial history of the locality. The materials will complement each other and consist of quality roofing, rainwater goods, clay facing brick, metal cladding, slim profile aluminium windows and doors.

Residential Amenity

- 7.14** In terms of overlooking, overshadowing and privacy considerations to surrounding residential properties, the development is over 41m away from the closest five storey flatted properties to the east and between 43 and 59 metres from the two storey properties to the north and west of the site. These separation distances are considered to be acceptable. The ground level of the development also sits at a significantly lower level than

these surrounding residential properties and as a result the overall height of the proposed flats is approximately level with the roof lines of the surrounding properties. The dense woodland on the rising ground to the north, east and west of the site will also provide natural screening. Considering the separation distances, the topography of the site and surrounding area and the natural screening on the site, the proposed development will not impact adversely on neighbouring properties in terms of privacy or overlooking. The applicant has also provided a sun path analysis which shows that any overshadowing occurring as result of the development would be confined to the site itself and would have no impact on surrounding residential properties to the north, east or west. Taking these points into consideration, the proposed development would not result in any unacceptable impacts related to overlooking, loss of privacy or overshadowing and is therefore acceptable.

- 7.15** The layout, design and appearance of the development are all considered to be acceptable. The proposal will create a well-designed and integrated development which will address the six qualities of successful places by having a distinctive identity, forming safe streets, having quality green infrastructure, using high quality materials and being sustainable, all complying with policies GD1, DS1 and CP1 of the Adopted and Proposed Plans alongside the Residential Development Design Guidance.

Natural Heritage

- 7.16** The site is enclosed by dense shrub and woodland on its north, east and western boundaries and a small part of the site to the west encroaches on an area of land that is subject to a Tree Preservation Order (TPO). A tree survey was therefore carried out to determine the condition of the existing woodland and to ascertain if there would be any impact on the TPO as a result of the development. Due to the dense undergrowth and steep incline across parts of the site it was not possible to access all of the study area. A total of 8 trees were tagged on site individually while at least 200 further trees in groups were visually noted due to the restrictions described above. The survey advises that a total of 10 trees would require to be removed as a result of the proposed development. The trees identified for removal are a mix of predominantly Willow, along with Ash, Cypress and Birch. Five of these trees are small ornamental trees, four are of poor to fair quality and one has partially collapsed. None of the trees identified for removal affect the TPO. Considering the density and amount of existing trees on site (in excess of 200), the number for removal is relatively small and confined to the periphery of the base of the site where the previous buildings and hardstanding's are located and not the denser areas. The trees proposed for removal are of small scale and limited value and most importantly their removal would not result in an unacceptable impact on the remaining established woodland. The

applicant has proposed the planting of 22 replacement trees within the site as part of the landscape provision and this will be secured by planning condition. A woodland management plan has also been sought by condition to ensure the tidying up of the shrub and undergrowth, to ascertain any necessary minor intervention works to existing trees such as crown lifting, to secure treatment and management of Japanese knotweed and the long term management of the site.

Open Space

- 7.17** The open space provision within the development is considered to be proportionate for this site. The main area of open space is positioned centrally between the two blocks and will provide a visual focal point and amenity area which is well integrated within the development and accessible to all residents. The space is well overlooked to ensure it can function as a safe and usable space. A smaller area of usable open space is located to the rear of the site which provides a further, quieter option for residents which is closer to the existing woodland which also has a high amenity and environmental value. In terms of the open space provision required by the 'Our Green Network Planning Guidance', the development has provided this and met the provisions of the Planning Guidance and applicable policies of LDP1 and LDP2.

Landscaping and play provision

- 7.18** Given the extensive tree and shrub cover around the perimeter of the site, proposed landscaping is modest but encouraging and includes shrubs around the block perimeters, tree planting at key points across the site which will add to the amenity value and attractiveness of the space and the introduction of fingers of landscaping that subtly integrate the surrounding woodland with the car park to soften its appearance. Conditions have been imposed to ensure a scheme for the provision of natural play equipment to be incorporated within the development site. The play equipment combined with the surrounding woodland will provide children with opportunities for exercise and exploration together with biodiversity benefits.

Roads, Parking and Access

- 7.19** The road layout reflects the aspirations for the Scottish Government's policy statement on 'Designing Streets' through a more pedestrian focused layout. The access arrangements for the site are considered to be acceptable and while the 56 parking spaces falls short of the required 60 spaces for this type of development, the Councils Roads service has accepted this subject to the applicant accommodating 4 additional parking spaces, should they become necessary in the future. Further details on

electric vehicle charging within the site and service arrangements for bin collection will also be secured by planning condition.

Technical Matters

- 7.20** The site is not at risk of flooding. In terms of drainage, site run off and surface water shall be treated and attenuated through permeable surfaces that will discharge into the existing drainage network in accordance with Sustainable Urban Drainage principles. Given the steep slope to the rear of the site a new cut off ditch is proposed to catch any run off from the slope and discharge it into the attenuation system. Subject to the implementation of these measures outlined above, the proposal would comply with policies F1, F2, DS6 and ENV6 of the Adopted and Proposed Plans.
- 7.21** Noting the site history which was until recently used for industrial and business purposes, the Council's Environmental Health Service have requested conditions regarding extensive site investigation reports including remediation and mitigation measures. These matters alongside others regarding noise, dust mitigation and construction activity including noise and vibration will also be addressed as planning conditions and will be designed to safeguard residential amenity in the surrounding area from adverse effects associated with construction activity. Measures will include the restriction of operation hours to limit construction noise and vibrations from piling, restriction of delivery hours and dust control.

Place and Design Panel

- 7.22** In January 2020, the proposed development was presented to the Place & Design Panel. The Panel felt that this was generally a positive development for the site and the area given its residential character and that it posed a good opportunity to improve integration of the site to the wider locality. The Panel raised a number of observations relating to permeability and movement, landscape design and architectural design. While the landscape design is still to be finalised and this will be achieved through planning conditions, the final design has clearly taken cognisance of the comments and recommendations made by the Panel and the application is considered to be compliant with Policy CP4 on this basis.

8. CONCLUSION

- 8.1** The proposed redevelopment of this brownfield site for residential purposes is in compliance with the Adopted and Proposed Local Plans. The proposals have been subject to extensive discussions with officers and the design of the site has clearly evolved through and been informed by the feedback provided through this process. The development will provide high quality affordable housing and amenity space as well as a strong frontage to this part of Dumbarton Road while integrating the site with the surrounding residential neighbourhood. The scale and design of the development is acceptable without adversely impacting on visual or residential amenity.

9. CONDITIONS

1. Prior to the commencement of development on site, exact details, specifications and samples of all proposed external materials to be used for the flatted blocks within the development site shall be submitted to and approved in writing by the Planning Authority and shall include, roofing material, rainwater goods, clay facing brick, metal cladding and aluminium windows and doors. Thereafter, the development shall be completed in strict accordance with the approved material details and palette unless may otherwise be agree in writing by the Planning Authority.
2. Further to Condition 1 and prior to the approved brickwork associated with any of the flatted blocks being constructed or installed on site, a sample panel of this brickwork shall be constructed on site in order for it and the associated mortar to be reviewed, inspected and approved in writing by the Planning Authority. Thereafter, the development shall be completed in strict accordance with the approved brick details.
3. Notwithstanding the plans hereby approved including 'Proposed External Works (Drawing No. 2577-L(90)010 Rev B)' and prior to the commencement of development on site, full details of all hard and soft boundary treatments for across the site shall be submitted to and approved in writing by the Planning Authority. Thereafter, the approved boundary treatments arrangements shall be installed prior to the occupation of the first unit unless otherwise agreed in writing by the Planning Authority and maintained as such thereafter for the lifetime of the development.
4. Notwithstanding the plans hereby approved including 'Proposed External Works (Drawing No. 2577-L(90)010 Rev B)' and prior to the commencement of development on site, full details of all hard and soft landscaping across the site in the from of a 'Landscape Management Plan' shall be submitted to and approved in writing by the Planning

Authority. The soft landscape arrangements shall be implemented no later than the next appropriate planting season or after occupation of the 30th property. Any trees, shrubs or plants forming part of the approved landscape scheme which die, are removed or become seriously damaged or diseased, within a period of 5 years from the date of their planting, shall be replaced in the next planting season with others of similar sizes and species unless the Planning Authority gives written approval to any variation. The landscaping management plan as approved shall thereafter be maintained in accordance with these details for the lifetime of the development unless otherwise agreed in writing by the Planning Authority.

5. Prior to the commencement of development on site, a Woodland Management Plan shall be submitted for the approval in writing of the Planning Authority and shall include:
 - a. a comprehensive survey of all trees within the site including recommendations and a timetable for any intervention works
 - b. A method statement and phasing plan for the control and eradication of Japanese Knotweed including post treatment monitoring of the site to ensure a continuous 12 month period of where no Japanese Knotweed is identified as growing on the site. In the event that any Japanese Knotweed is identified as growing during the 12 month monitoring period then treatment shall resume and continue until a 12 month period of no growth of Japanese Knotweed occurs.
 - c. A scheme for the management and on-going maintenance of the woodland and landscaped areas of the site
 - d. Details of how the trees and their roots on site will be protected during the construction phase of the development

Thereafter, all works and maintenance arrangements on site shall be carried out and implemented in full accordance with the approved Woodland Management Plan.

6. Unless as may otherwise be agreed in writing by the Planning Authority by virtue of the the woodland mangment plan in condition 5, no trees other than those detailed in the approved Tree Survey Report (dated April 2020) shall be removed either within or adjacent to the site without the prior written approval of the Planning Authority.
7. Notwithstanding the plans hereby approved including 'Proposed External Works (Drawing No. 2577-L(90)010 Rev B)' and prior to the commencement of development on site, full details of all hard surfaces shall be submitted to and approved in writing by the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of the first unit.

8. Prior to the commencement of development on site, details of the design and location of bin storage, cycle storage provision, street furniture and external lighting, shall be submitted to and approved in writing by the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of the first unit and thereafter maintained, unless otherwise agreed in writing with Planning Authority.
9. Prior to the commencement of development on site, details of a 'Play Equipment Strategy' for the site shall be submitted to and approved in writing by the Planning Authority. This shall include (but not be limited to) the following:
 - details and specifications of natural play equipment and structures.
 - full details regarding the maintenance arrangements for all play equipment and associated features within the site.

Thereafter, the approved play equipment strategy and all associated features and specifications shall be installed prior to the completion of the development unless otherwise in agreed in writing by the Planning Authority and maintained in accordance with the approved details for the lifetime of the development.

10. Prior to the commencement of development, an updated site layout plan shall be submitted for the written approval of the Planning Authority which provides details of an additional four car parking spaces within the site. Twelve months after completion of the development, a parking review shall be undertaken within the development to ascertain levels of car ownership and whether there are any parking related problems within the development. The findings and recommendations of the review shall be submitted for the written approval of the Planning Authority. Thereafter the four additional parking spaces which can be accommodated on site shall be formed, if required, in accordance with the agreed recommendations and the approved details.
11. Prior to the commencement of development with the site, details of the location and design of an electric charging point(s)/unit(s) to serve the development shall be submitted to and approved in writing by the Planning Authority. The approved car charging point(s)/unit(s) and associated infrastructure shall thereafter be installed in accordance with the approved details at a timescale agreed by the Planning Authority and maintained as such thereafter.

12. Unless otherwise agreed in writing, no development shall commence on site until a Travel Plan which includes details promoting sustainable travel to and from the site and appropriate measures and actions to reduce car dependence for the development is submitted for the written approval of the Planning Authority. The Travel Plan shall include details of the contents of an information pack which will be provided to future occupants of the development to ensure that they are aware of their public transport and active travel options available within the area. The approved Travel Plan and all associated measures and actions shall be in place and implemented in full prior to the occupation of the first unit and maintained as such unless otherwise agreed in writing by the Planning Authority
13. Notwithstanding the 'Ground Investigation Report' (Dated April 2020) submitted, no development (other than investigative works) shall commence on site until such time as a detailed report on the nature and extent of any contamination of the site has been submitted to and approved in writing by the Planning Authority. The report shall be prepared by a suitably qualified person and shall include the following:
- a) A detailed site investigation identifying the extent, scale and nature of the contamination on the site (irrespective of whether this contamination originates on the site).
 - b) An assessment of the potential risks (where applicable) to:
 - Human health, Property, Groundwater and surface waters, Ecological systems
 - c) An appraisal of remedial options, including a detailed remediation scheme based on the preferred option.
14. No development (other than investigative works) shall commence on site until such time as a detailed remediation scheme for the site has been submitted to and approved in writing by the Planning Authority. The scheme shall be prepared by a suitably qualified person and shall detail the measures necessary to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property, and the natural and historical environment. The scheme shall include details of all works to be undertaken, the remediation objectives and criteria, a timetable of works and/or details of the phasing of works relative to the rest of the development, and site management procedures. The scheme shall ensure that upon completion of the remediation works the site will not qualify as contaminated land under Environmental Protection Act 1990 Part IIA in relation to the intended use of the land after remediation.

15. The approved remediation scheme shall be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Planning Authority. The Planning Authority shall be notified in writing of the intended commencement of remediation works not less than 14 days before these works commence on site. Upon completion of remediation works and prior to the first unit being occupied, a verification report which demonstrates the effectiveness of the completed remediation works shall be submitted to and approved in writing by the Planning Authority.
16. If required, a monitoring and maintenance scheme to include monitoring the long-term effectiveness of the proposed remediation over a period of years determined by the scheme shall be submitted to and approved by the Planning Authority. Any actions ongoing shall be implemented within the timescale agreed by the Planning Authority. Following completion of the actions/measures identified in the approved remediation scheme a further report which demonstrates the effectiveness of the monitoring and maintenance measures shall be submitted to and approved in writing by the Planning Authority.
17. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
18. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall be free from metals, plastic, wood, glass, tarmac, paper and odours. Prior to placement of any of the material, the developer shall submit a validation report for the approval in writing of the Planning Authority and it shall contain details of the source of the material and associated test results to demonstrate its suitability for use. Thereafter the development shall be undertaken in accordance with the approved details.
19. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This

statement shall include an assessment of and take into account the following:

- The impact of the piling on surrounding properties.
- Detail any procedures which are required to minimise the impact of noise and vibrations on the occupants of surrounding properties.

This statement as submitted shall be prepared by a suitably qualified person and shall take into account the guidance contained in BS6472:1984 'Evaluation of Human Response to Vibration of Buildings'. The piling works shall thereafter be carried out in accordance with the approved method statement until they are completed on site.

20. No development shall take place on site until such time as a Noise Impact Assessment has been submitted to and approved in writing by the Planning Authority. This Noise Impact Assessment (alongside any recommendations in respect of attenuation measures) shall be prepared by a suitably qualified person and shall include an assessment of the potential for occupants of the development to experience noise nuisance arising from nearby industrial / commercial units and from the impact of road traffic noise. Where a potential for noise disturbance is identified, proposals for the attenuation of that noise shall be submitted to and approved in writing by the Planning Authority. Any such approved noise attenuation scheme shall be implemented prior to the occupation of the first unit within the site and shall thereafter be retained in accordance with the approved scheme unless otherwise agreed in writing by the Planning Authority.
21. Further to Condition 20 above, within 4 weeks of the completion of the development on site, a 'Verification Report' demonstrating compliance and validation of the maximum noise projections and effectiveness of the noise attenuation measures detailed within the approved 'Noise Impact Assessment' Report shall be submitted to and approved in writing by the Planning Authority. This 'Verification Report' shall be prepared and undertaken by an independent consultant and the measurements shall be carried out in accordance with BS4142:2014 - "Methods of rating and assessing industrial and commercial sound" (with respect to current best practice). These levels and measures as agreed and validated within the approved verification report shall be maintained for the lifetime of the development thereafter unless otherwise agreed in writing by the Planning Authority.
22. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of

- plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
23. During the period of construction, all external works including piling and ancillary operations and deliveries shall be carried out between the following hours and at no other time, unless otherwise agreed in writing by the Planning Authority:
- Mondays to Fridays : 0800 – 1800;
 - Saturdays: 0800 – 1300;
 - Sundays and public holidays: No Working.
24. Unless otherwise approved in writing by the Planning Authority, no development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing by the Planning Authority. The scheme shall identify likely sources of dust arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.
25. No development shall commence on site until details for the storage and the collection of waste arising from the development shall be submitted to and approved in writing by the Planning Authority. The agreed details shall be in place prior the occupation of the first unit within the site and thereafter maintained for the lifetime of the development.
26. Prior to the occupation of the first unit within the site, the developer shall install the necessary infrastructure to enable the full development and all associated properties to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.
27. No unit shall be occupied within the site until the vehicle parking spaces have been provided within the site in accordance with approved plan 'Proposed Site Layout (Drawing No. 001 Rev B)'. Further to the approved plans, each vehicular parking space shall be constructed to include a marking which clearly identifies it to the house number it is to be attributed within the site or alternatively be marked to identify it as a visitor parking space where relevant. The aforementioned parking shall thereafter be

retained and be capable of use at all times and shall not be removed or altered without the prior written approval of the Planning Authority.

Peter Hessett
Strategic Lead- Regulatory
Date: 10th June 2020

Person to Contact: Pamela Clifford, Planning & Building Standards Manager
Email: Pamela.Clifford@west-dunbarton.gov.uk

Appendix: Location Plan

Background Papers:

1. Application forms and plans;
2. Consultation responses;
3. West Dunbartonshire Local Plan 2010;
4. West Dunbartonshire Local Development Plan Proposed Plan;
5. West Dunbartonshire Local Development Plan 2 Proposed Plan.
6. 'Our Green Network' Guidance
7. Residential Development Design Guidance

Wards affected: Ward 6 – Clydebank Waterfront

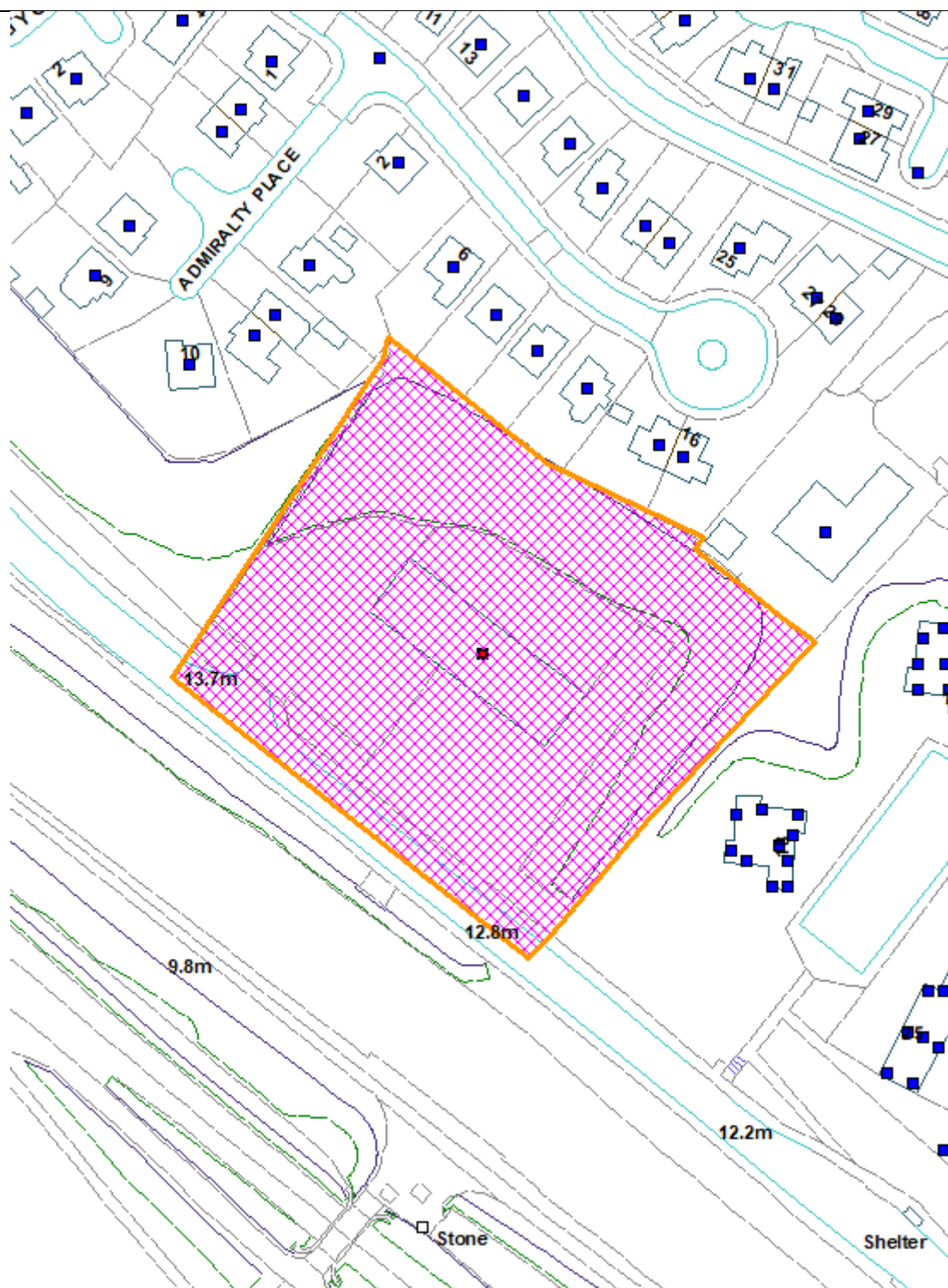
Map Register No: HQ642

Date: 28 May 2020

DC19/260

Erection of 2 six storey
flatted blocks, comprising
48 flats with associated
road access, parking and
infrastructure

Development Site
Dumbarton Road
Old Kilpatrick



WEST DUNBARTONSHIRE COUNCIL

Report by Strategic Lead- Regulatory

Planning Committee: 10 June 2020

DC20/018: Demolition of former St Cuthbert's Church and the erection of 24 flatted residential units at Dalton Avenue, Clydebank by Clydebank Housing Association.

1. REASON FOR REPORT

- 1.1 This application is subject to a representation from a Community Council and under the terms of the approved Scheme of Delegation it therefore requires to be determined by the Planning Committee.

2. RECOMMENDATION

- 2.1 That the Committee indicate that it is **Minded to Grant** planning permission and delegate authority to the Planning and Building Standards Manager to issue the decision subject to the conditions set out in Section 9 and to the satisfactory conclusion of a legal agreement or other suitable mechanism to secure a financial payment towards open space provision/ green infrastructure improvements in the local area.

3. DEVELOPMENT DETAILS

- 3.1 The 0.36 hectare site is located within the Linnvale area of Clydebank. It is situated at the end of the main access road into Linnvale and is bounded by roads to the west and north-east. To the south-east a footpath separates the site from a block of cottage-style flats and to the north of the site there is an existing large area of open space including an equipped play area. The area around the site is predominantly residential and characterised by two-storey flats and houses, however, immediately to the south-west of the site there are two blocks of four-storey high flats. The site contains the former St Cuthbert's church, a single storey brick building which has been vacant since 2017. The site is generally level although it is raised slightly above Dalton Avenue.
- 3.2 Planning permission is sought for the erection of 24 one, two and three bedroom flats. Three flats will be wheelchair accessible. The development would consist of two, three-storey high blocks joined together by a single-storey section to form an L-shaped building. The building would have a frontage onto Dalton Avenue and a second frontage facing south-west,

overlooking a 24-space carpark that would serve the development. Vehicular access into the site would be via Attlee Avenue, which lies to the north-east of the site. There would also be a number of footpaths providing pedestrians access to the site. Amenity space within the development would be provided at the 'rear' of the building, directly adjacent to the existing open space and play area to the north. Ten young ornamental trees would be removed however new trees would be planted as part of the landscaping scheme.

- 3.3 The building would have a fairly simple design with a low-pitch roof hidden behind a parapet on the front elevations. The elevation facing onto Dalton Avenue and Livingstone Street would be slightly curved with recessed balconies in the centre and at the corners. Most of the windows on the building would extend down to floor level and have 'Juliet' style balconies to maximise the amount of daylight received inside the flats. The building would be finished with a grey multi-tone, textured brick.
- 3.4 Supporting technical information has been provided as part of the application and this includes a Contaminated Land Assessment and a Phase 1 Habitats Survey,

4. CONSULTATIONS

- 4.1 West Dunbartonshire Council Roads Service have no objections subject to conditions regarding the allocation of an area of landscaping for parking should it be required in the future.
- 4.2 Environmental Health Service have no objections subject to conditions regarding land remediation and noise.
- 4.3 Glasgow Airport has no objections subject to conditions regarding landscaping and a bird hazard management plan.

5. REPRESENTATIONS

- 5.1 One representation has been received from Linnvale and Drumry Community Council on behalf of local residents. They object to the location of the vehicular access as they are concerned that it will reduce the number of on-street parking spaces on Attlee Avenue.

6. ASSESSMENT AGAINST THE DEVELOPMENT PLAN

West Dunbartonshire Adopted Local Plan 2010

- 6.1** Policy UR1 encourages the redevelopment and re-use of underused, vacant and/or derelict land and buildings for appropriate uses such as housing. Policy H4 sets out standards expected of residential development, requiring high quality design in the range of house types and sizes and in terms of form, layout and materials. Policy GD1 seeks to ensure that all new development is of a high quality of design and respects the character and amenity of the area.
- 6.2** Policy R2 specifies the open space provision required for all developments. Assessment of open space requirements has been undertaken against the more updated “Our Green Network” Planning Guidance (2015) in Section 7 below.
- 6.3** Policies T1 and T4 require sites to be integrated with sustainable travel and Policy E5 relates to trees and requires new development proposals to consider impacts on trees and incorporate suitable tree planting. Policies F1 and F2 aim to ensure that new development is not at risk from, and does not increase the risk of flooding, and has suitable SUDS drainage infrastructure. The proposal complies with the policies of the adopted local plan and is assessed fully in Section 7 below.

7. ASSESSMENT AGAINST MATERIAL CONSIDERATIONS

West Dunbartonshire Local Development Plan (LDP1) Proposed Plan

- 7.1** On 27 April 2016, the Planning Committee took a final decision not to accept the Local Development Plan Examination Report recommended modification in respect of including the Duntiglennan Fields site in Clydebank as a housing development opportunity, and therefore, as a result of the Scottish Ministers’ Direction, the Local Development Plan has remained unadopted but continues to be a material consideration in the determination of planning applications.
- 7.2** The site is identified as an Existing Neighbourhood. Policy BC4 supports the principle of residential development within existing residential locations, provided there is no adverse impact on neighbouring amenity or character of an area. It is considered that there will be no adverse impacts on either of these. Policy DS1 sets out general expectations for the quality of new development, including that it be distinctive, adaptable, resource-efficient and easy to get to and move around, safe, pleasant and welcoming.

- 7.3** The consideration of policies DS1, GN2, SD1, DS6 and GN5 with regards to new residential development, green network, transport, flooding and drainage are similar to that of the Adopted Plan. DS7 requires any potential site contamination issues to be addressed and DS3 requires significant travel generating uses to be located within 400 metres of a public transport network. DS1 seeks to ensure a high design quality in housing and being suitable for a mix of occupants rather than a specific demographic.
- 7.4** The Residential Development: Principles of Good Design Guidance applies to all developments of more than 3 units and has been taken account of in the proposed design and site layout. The proposed development is assessed against the Proposed Plan and the Residential Development Guidance in Section 7 below.

West Dunbartonshire Local Development Plan (LDP2) Proposed Plan

- 7.5** On 19th September 2018 the Planning Committee approved Local Development Plan 2: Proposed Plan for consultation. It is therefore the Council's most up to date policy position and it is a material consideration in the assessment of planning applications. The Examination Report of Local Development Plan 2 was received on 22nd April 2020 and will be presented to the August Planning Committee for consideration, together with the modified Local Development Plan 2 and associated documents. The findings of the Examination Report have not materially changed the content and form of the LDP 2 policies and land allocations used in the assessment of this application.
- 7.6** The site is identified under Policy H4 as an Existing Neighbourhood, which seeks to protect, preserve and enhance the residential character and amenity of existing residential areas at all times.
- 7.7** Similarly to Policy DS1 of Proposed Plan 1, Policy CP1 seeks to ensure that housing is of a high quality, adaptable and is designed to be suitable for a mix of occupants. It indicates that all new development will be expected to contribute towards creating successful places by having regard to the six qualities of a successful place (distinctive, adaptable, resource efficient, easy to get to/move around, safe and pleasant, and welcoming).
- 7.8** Policy GI2, ENV1, ENV4, and ENV6 are similar to the green network, tree, and flooding policies of the Adopted and Proposed Plan 1 policies, albeit that Policy GI2 sets a higher open space standard to that of the adopted Local Plan and Local Development Plan 1. Policies CP2 requires the integration and consideration of green infrastructure from the outset of

the design process through to its maintenance and stewardship of the resource.

- 7.9** Policy CON1 requires that significant travel generating uses are designed to encourage sustainable transportation and Policy CON4 sets out a need for all developments to install sufficient broadband provisions. Policy ENV8 requires developments to address air quality, lighting and noise as part of the planning process, whilst policy ENV9 requires all potentially contaminated sites to be remediated where necessary to ensure that the site is suitable for the intended use. It is considered that the proposals comply with the above policies as discussed below.

Principle of Development

- 7.10** The site is identified as an Existing Residential Area/Existing Neighbourhood within the adopted local plan and proposed local development plans. The principle of the proposed residential use is therefore acceptable and would be in keeping with the surrounding residential area. To enable the development to take place the former church building will be demolished. It is understood that the building, which was constructed in 1978, was owned by the Church of Scotland and was in use until 2017 when its congregation merged with another local church. The building is not listed and has little architectural merit. It is therefore considered that demolition of the vacant building will not have a detrimental impact on the visual amenity of the area, and it will allow the site to be redeveloped for much-needed affordable housing in this established residential area.

Site Layout, Design and Appearance.

- 7.11** The site has a prominent location, being positioned at the head of the main access route into Linnvale, and due to the layout of the surrounding streets it is surrounded by flats and houses that look onto the site. The building will therefore be a focal point and its design has been carefully considered to take this into account. The three-storey high building will provide height, which will compliment the adjacent 4 storey high blocks, providing a density and character at this location. Part of the frontage facing onto Dalton Avenue will be slightly curved to follow the bend in the road and it is this part of the building that will be most visible when travelling along the main road into Linnvale. This feature will add further to the character of the building and to the visual amenity of the area.
- 7.12** The building will be finished with a multi-tone clay facing brick while most of the surrounding residential properties are finished with render. Most, if not all, of the surrounding buildings date from the post-war period and their design and finish reflects the style of that time. The proposed

development is contemporary and its materials including clay bricks and slim profile aluminium window frames will reflect a more urban style that will be attractive and will also be durable and low maintenance. The design of the building is considered acceptable in this residential area.

- 7.13** The building will have a fairly simple design with a shallow-pitch roof hidden behind a parapet and recessed balconies on the west-facing elevation. The use of a single storey section to join the two blocks helps to reduce the overall massing of the building and adds visual interest. It is also a practical feature as it will contain the bin store. An internal cycle store with space for 30 bikes will also be accommodated within the building, which frees up space outside the building for landscaping.

Impact on residential amenity

- 7.14** It is considered that the building will not have a detrimental impact on the amenity of surrounding residential properties. The existing two storey flats that lie to the south-east of the site will be over 27 metres away from the building, which will minimise any impact that it will have in terms of overlooking or overbearing effect. The existing two storey flats that lie to the east of the site will not be significantly affected by overshadowing as the building will lie over 19 metres away and they will not be affected by overlooking as there will only be bathrooms windows on the side elevation that face it, which will have opaque glazing. The proposal is therefore considered acceptable as existing neighbouring amenity will be safeguarded.
- 7.15** It is considered that the development addresses the six qualities of successful places by having a distinctive identity, forming safe streets, having quality green infrastructure, using high quality materials and being sustainable, all complying with policies GD1, DS1 and CP1 of the Adopted and Proposed Plans alongside the Residential Development Design Guidance.

Open Space and Landscaping

- 7.16** An area of amenity open space for the development will be provided at the 'rear' of the building which will connect with the existing large area of open space and the play area directly to the north of the site. The development does not include a play area as the existing play area to the north of the site is modern, has a range of equipment and is easily accessible. In terms of the open space for the site, while quality areas are being provided, it falls short of the provision as required by the 'Our Green Network Planning Guidance', and therefore a financial contribution is required in order to meet with the requirements of the Planning Guidance and applicable policies of LDP1 and LDP2. The required financial

contribution fee is £20, 790 and this will address the shortfall and can be secured through a legal agreement or other suitable mechanism. On balance, the open space and landscaping provision proposed for the site is appropriate as the site also benefits from the existing adjacent area of open space and associated play area.

- 7.17** Trees and hedging will be planted along the northern boundary to provide some screening between the site and adjacent open space/ play area. Further landscaping including tree planting will be provided throughout the rest of the site, particularly along the south-eastern boundary of the site where the existing houses will look onto the car park. The planting of trees here will provide a more attractive outlook for residents. Approximately 10 trees and some large shrubs on the site require to be felled to accommodate the development. Most of these trees are fairly small and ornamental in appearance, and although they do provide an attractive setting for the existing building, it is considered that they do not make a significant contribution to the visual amenity of the wider area. The four large mature trees at the north-west and south-west corners of the site will be retained, which will help to provide a more established setting for the new development.

Roads, Parking, Access and Permeability

- 7.18** Vehicular access into the site would be from Attlee Avenue and a twenty-four space car park, including 3 disabled bays, would be formed along the south-eastern portion of the site. The car park is five spaces short of the number of parking spaces typically required for a development of this size. However, the site is close to the town centre and well served by public transport with a train station (Drumry) and bus stops within a few minutes walking distance. A cycle store capable of holding 30 bikes will be formed within the building and will help to support the use of this form of sustainable transport. The Council's Roads service has recommended that an area of the landscaping is identified for additional parking provision in the future should this be required. A condition is recommended which will allow the parking provision to be assessed after a period of time, and additional spaces formed if there are any issues. A number of pedestrian accesses will be formed on the eastern and western sides of the site, which means that the site will be well connected to its surroundings, including the adjacent play area.

Representation from Community Council

- 7.19** Linnvale and Drumry Community Council have indicated that local residents are concerned that the location of the vehicular access into the site on Attlee Avenue will remove the ability for residents to park on the road. At present, residents of Attlee Avenue park on one side of the road (the side closest to their homes), which allows space for cars to pass (single file) on the other side of the road. The proposed vehicular access into the site is from the opposite side of the road from where the existing residents park their cars. This access has been reviewed by the Councils' Roads Service and found to be acceptable as an access and that the new access will not prevent or have any impact on the established on-street parking by existing residents. Also, the new access into the site will be wide enough to allow cars to turn off the road and drive into the site even if there are cars parked on the road opposite the access. Taking these considerations into account, the proposed access is considered to be acceptable.

Technical Matters

- 7.20** A phase 1 contaminated land assessment has been submitted for the development and this is acceptable to the Council's Environmental Health Service at this stage and they have recommended a number of conditions relating to the submission of a detailed site investigation including remediation and mitigation measures. These matters alongside others regarding noise, dust mitigation and construction activity can be addressed as planning conditions.
- 7.21** A phase 1 habitat survey was carried out on the site. It identified the site as being of little ecological value with no connectivity to any greater areas of habitat. The report advises that a nesting bird survey would be required should any vegetation or building removal require to take place during the bird breeding season (March to September, inclusive).
- 7.22** Glasgow Airport has advised that a bird hazard management plan will be required due to the shallow pitch of the building's roof, which could attract birds. They have also advised that they will require full details of the proposed landscaping scheme to ensure that it will not pose a risk to aircraft by attracting birds. According to SEPA flooding maps the site is not at risk from flooding. To ensure that the site does not increase flood risk elsewhere, a sustainable drainage scheme for the site will be required. All can be dealt with by conditions.

8. CONCLUSION

- 8.1** The proposed development would result in the removal of a vacant building and the construction of much needed affordable housing within an existing residential area. The building has a quality, modern design that will contribute positively to the character and appearance of the local area. It will provide new homes within an established community with excellent access to public transport and to the town centre.

9. CONDITIONS

01. Exact details and specifications of all proposed external materials shall be submitted for the further written approval of the Planning Authority prior to any work commencing on site and shall be implemented as approved.
02. Prior to the commencement of development on site, full details of all hard surfaces shall be submitted for the further written approval of the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of any unit within the development.
03. Prior to the commencement of development on site, full details of the design and location of all walls and fences to be erected on site shall be submitted for the further written approval of the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of any unit within the development.
04. Prior to the commencement of development on site, details of the design and location of all external lighting shall be submitted for the further written approval of the Planning Authority and the development shall thereafter be completed in accordance with the approved details prior to the occupation of any unit within the development, unless otherwise agreed in writing with the Planning Authority.
05. No unit within the development shall be occupied until all of the vehicle parking spaces have been provided within the site in accordance with the approved plans. The spaces shall thereafter be kept available for parking at all times.
06. Prior to the commencement of development on site, full details of the foul and surface water drainage system shall be submitted for the written approval of the Planning Authority. The drainage system shall incorporate the principles of Sustainable Urban Drainage Systems within its design,

and thereafter shall be implemented prior to the occupation of any unit within the development.

07. The development hereby approved shall be constructed strictly in accordance with the finished site levels and finished floor levels as shown on approved plan L(90)100. Any alterations to these finished site and floor levels shall first be agreed in writing with the Planning Authority.
08. Notwithstanding the 'Land Contamination Preliminary Risk Assessment' (Dated April 2020) submitted, no development (other than investigative works) shall commence on site until such time as a detailed report on the nature and extent of any contamination of the site has been submitted to and approved in writing by the Planning Authority. The report shall be prepared by a suitably qualified person and shall include the following:
 - i. A detailed site investigation identifying the extent, scale and nature of contamination on the site (irrespective of whether this contamination originates on the site)
 - ii. An assessment of the potential risks (where applicable) to:
 - human health, property (existing and proposed), groundwater and surface waters, ecological systems
 - iii. An appraisal of remedial options, including a detailed remediation scheme based on the preferred option.
09. No development (other than investigative works) shall commence on site until such time as a detailed remediation scheme for the site has been submitted to and approved in writing by the Planning Authority. The scheme shall be prepared by a suitably qualified person and shall detail the measures necessary to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property, and the natural and historical environment. The scheme shall include details of all works to be undertaken, the remediation objectives and criteria, a timetable of works and/or details of the phasing of works relative to the rest of the development, and site management procedures. The scheme shall ensure that upon completion of the remediation works the site will not qualify as contaminated land under Environmental Protection Act 1990 Part IIA in relation to the intended use of the land after remediation.
10. The approved remediation scheme shall be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Planning Authority. The Planning Authority shall be notified in writing

of the intended commencement of remediation works not less than 14 days before these works commence on site.

Upon completion of the remediation works and prior to the site being occupied, a verification report which demonstrates the effectiveness of the completed remediation works shall be submitted to and approved in writing by the Planning Authority.

11. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
12. If required, a monitoring and maintenance scheme to include monitoring the long- term effectiveness of the proposed remediation over a period of years determined by the scheme shall be submitted to and approved by the Planning Authority. Any actions ongoing shall be implemented within the timescale agreed with the Planning Authority in consultation with Environmental Health measures. Following completion of the actions/measures identified in the approved remediation scheme a further report which demonstrates the effectiveness of the monitoring and maintenance measures shall be submitted to and approved by the Planning Authority.
13. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall be free from metals, plastic, wood, glass, tarmac, paper and odours. Prior to placement of any of the material, the developer shall submit a validation report for the approval in writing of the Planning Authority and it shall contain details of the source of the material and associated test results to demonstrate its suitability for use. Thereafter the development shall be undertaken in accordance with the approved details.
14. During the period of construction, all works, including piling, and ancillary operations which are audible at the site boundary (or at such other place(s) as may first be agreed in writing with the Planning Authority) ,

shall be carried out between the following hours unless otherwise approved in writing by the Planning Authority:

Mondays to Fridays:	0800-1800
Saturdays:	0800-1300
Sundays and public holidays:	No working

15. No development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing by the Planning Authority. The scheme shall identify likely sources of dust arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.
16. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
17. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This statement shall include an assessment of and take into account the following:
 - The impact of the piling on surrounding properties.
 - Detail any procedures which are required to minimise the impact of noise and vibrations on the occupants of surrounding properties.

This statement as submitted shall be prepared by a suitably qualified person and shall take into account the guidance contained in BS6472:1984 'Evaluation of Human Response to Vibration of Buildings'. The piling works shall thereafter be carried out in accordance with the approved method statement until they are completed on site.
18. No development shall commence on site until details for the storage and the collection of waste arising from the development shall be submitted to and approved in writing by the Planning Authority. The agreed details

- shall be in place prior to the occupation of the any unit within the development and thereafter maintained for the lifetime of the development.
19. A landscaping scheme for the site shall be submitted to and approved by the Planning Authority prior to commencement of development on site and shall be implemented not later than the next appropriate planting season after the occupation of the first unit within the development. The scheme shall include details of the maintenance arrangements and the landscaping shall thereafter be maintained in accordance with these details.
 20. Prior to the occupation of the development, the developer shall install the necessary infrastructure to enable the full development to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.
 21. All ground or vegetation clearance works, including any tree felling or demolition works, shall take place outwith the main bird breeding season (i.e. outwith the period of March to September inclusive), and no demolition or ground or vegetation clearance works are permitted between March to September in this respect. If this is not possible, a suitably qualified ornithologist/ecologist shall be engaged to survey any buildings, grounds and trees immediately prior to such works to advise the applicant/contractor/developer of any bird nesting activity and of any actions required to protect birds.
 22. Unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development with the site, details of the location and design of an electric charging point(s)/unit(s) to serve the development shall be submitted to and approved in writing by the Planning Authority. The approved car charging point(s)/unit(s) and associated infrastructure shall thereafter be installed in accordance with the approved details at a timescale agreed by the Planning Authority and maintained as such thereafter.
 23. Twelve months after the full occupation of the building, a parking review including a methodology statement to be agreed with the Planning Authority shall be undertaken to ascertain levels of parking and to establish whether there are any parking related problems associated with the development. The findings and recommendations of the review shall be submitted for the written approval of the Planning Authority and any actions recommended in the review shall be implemented in a timescale agreed with the Planning Authority.

24. Unless otherwise approved in writing, no development shall commence until such time as a Bird Hazard Management Plan has been submitted to and approved in writing by the Planning Authority in consultation with Glasgow Airport. The submitted plan shall include details of:

- The organisation(s) responsible for implementing the Bird Hazard Management Plan on an ongoing basis;
- Arrangements for the management of fruit and berry bearing trees and shrubs to dissuade hazardous flocking bird species that may exploit these potential food sources;
- Arrangements for the management of potential nesting and roosting habitat.

The approved Bird Hazard Management Plan shall be implemented within a timescale agreed with the Planning Authority. It shall remain in force for the life of the development. No subsequent alterations to the plan shall take place unless firstly agreed by the Planning Authority in consultation with Glasgow Airport.

Peter Hessett
Strategic Lead- Regulatory
Date: 10 June 2020

Person to Contact: Pamela Clifford, Planning & Building Standards Manager
Email: Pamela.Clifford@west-dunbarton.gov.uk

Appendix: None.

Background Papers:

1. Application forms and plans;
2. Consultation responses;
3. Representation
4. West Dunbartonshire Local Plan 2010;
5. West Dunbartonshire Local Development Plan Proposed Plan;
6. West Dunbartonshire Local Development Plan 2 Proposed Plan.
7. 'Our Green Network' Guidance
8. Residential Development Design Guidance

Wards affected: Ward 5 – Clydebank Central

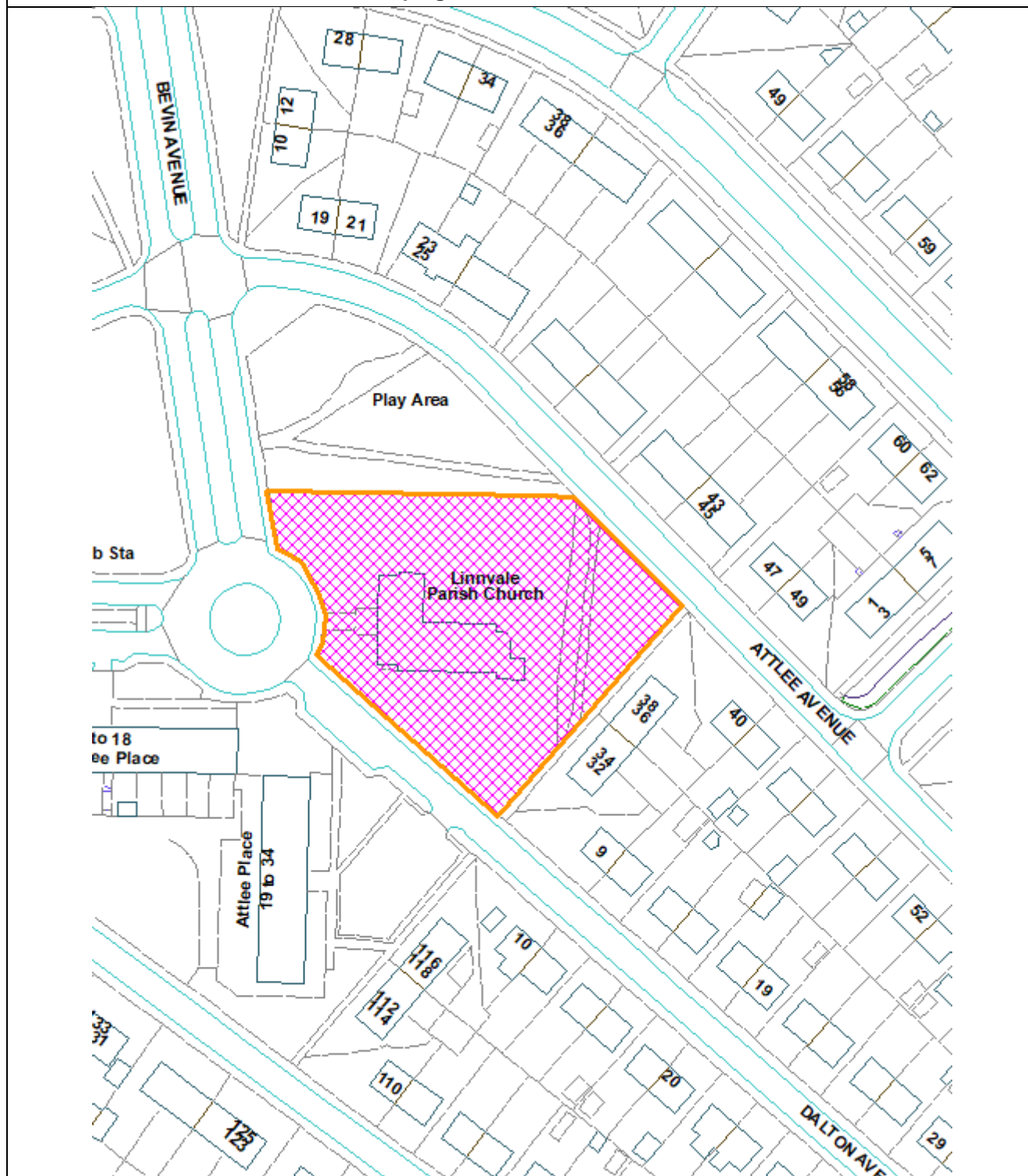
Map Register No: HQ641

Date: 28 May 2020

DC20/018

Demolition of former St
Cuthbert's Church and
erection of 24 flatted
residential units with
associated infrastructure
and landscaping

Development On Site Of The Former St
Cuthberts Parish Church
Dalton Avenue
Clydebank



WEST DUNBARTONSHIRE COUNCIL

Report by Strategic Lead - Regulatory

Planning Committee: 10th June 2020

DC20/076: Erection of storage facility and ancillary works at 2 Auld Street, Clydebank by The Storage Zone.

1. REASON FOR REPORT

- 1.1** The application raises issues of local significance and is subject to objections. Under the terms of the approved Scheme of Delegation, it therefore requires to be determined by the Planning Committee.

2. RECOMMENDATION

- 2.1** **Refuse** planning permission for the reasons set out in Section 9.

3. DEVELOPMENT DETAILS

- 3.1** The application site is a vacant plot of land on the south side of Auld Street, Clydebank. It extends to approximately 0.4 hectares in area and formerly operated as a waste transfer facility until this ceased operating around 20 years ago. The ground surface comprises of a mixture of tarmac and concrete hardstanding from its previous use with peripheral areas surfaced in gravel. Groups of mature and semi mature trees, shrub land and vegetation cover large parts of the site (including most notably along the site boundary on Auld Street) with the central area of the site also accommodating the remnants of the former waste transfer structures. Topographically, the ground surface slopes from the North towards the South; but for the most part the site is level. The site boundaries comprise post and wire fencing and there are two redundant accesses to the site on the North West and North East corner of the site respectively.
- 3.2** The surrounding area is occupied by a variety of different land uses. To the East of the site is a series of relatively new build housing areas and this includes those properties, which make up Caledonia Street, Gladstone Street and Benbow Road respectively. The closest of these to the site is a residential flatted block, which sits immediately beyond the South East boundary of the application site. Some light industrial and commercial uses are located opposite the site, including a vacant warehouse building at the corner of Caledonia Street; a bus and minibus hire business; a painting and decorating business, the Beardmore Business Centre; and a vehicle workshop/garage at the corner of Beardmore Street. Immediately neighbouring the site to the North West is another residential flatted block, which also faces onto Beardmore Street. A dismantled railway line runs along the entire length of

the rear of the site, with the Golden Jubilee National Hospital complex and grounds immediately beyond it.

- 3.3** Planning permission is sought for the erection of a containerised self-storage facility on the site. This will comprise of 115 containers within a compound secured with a 2.4 metre high metal palisade fence around the perimeter of the site. Vehicular access would be taken from the existing North East opening on Auld Street. Each container is metal in construction and measure 2.4 metres in height, 2.4 metres in width and 6.0 metres in length with a footprint of approximately 14 sqm. The core function of the business is the provision of on-site storage, for business and commercial companies and independent firms. The applicant seeks to develop the whole site through a phased approach with it being delivered in two distinct phases; Phase 1 (eastern and southern portion of site) and Phase 2 (western and northern portion of the site). Phasing will include the installation of 46 and then 69 storage containers respectively, removal of existing structures within the site, installation of a retaining wall and vehicular ramp between phase 1 and 2, installation of eight 7-metre high lighting columns with associated CCTV equipment and a security hut. A total of 2 parking spaces (5 bays in phase 1 and reduced to 2 in second phase) are proposed to support the use.
- 3.4** In support of this application both a Planning Statement and a Preliminary Geo-Environmental Appraisal have been provided. No details have been provided to indicate timescales between the completion of the Phase 1 component of the proposals and the commencement and completion of the Phase 2 component in order to deliver the overall masterplan.

4. CONSULTATIONS

- 4.1** West Dunbartonshire Council Roads Service have raised concerns regarding insufficient parking within the development and absence of details regarding staffing numbers, site management and accessibility and hours of operation associated with the use.
- 4.2** West Dunbartonshire Council Environmental Health Service have advised that a Noise Impact Assessment would be required due to concerns regarding potential noise and disturbance from the development and the impact upon residential amenity of the nearby residential properties.

5. REPRESENTATIONS

- 5.1** Three letters of representations have been received from local residents. These comprise of two letters of objection and one representation, which seeks clarification on matters related to the proposals. A summary of the points raised in the representations are as follows:

- Junction between Auld Street and Beardmore Street is unsafe at present and given it is a concealed entrance; this poses road safety issue for residential properties on Auld Street.
- Need for parking restrictions along the road and near to the junction of Auld Street and Beardmore Street.
- Volume of commercial traffic that currently use Auld Street and Caledonia Street would increase due to the proposed development.
- Lack of parking within the premises to support the use as proposed.
- Concerns regarding lack of detail regarding traffic control and the potential as a result for users of the development to park on private car parking associated with the neighbouring flats either side of the site.
- Concerns regarding noise associated with use particularly given the proximity of nearby flatted properties.
- Concerns regarding the significant noise generated from the shipping containers when in use.
- Concerns regarding the level of usage and lack of details regarding the hours of operation of the use.
- Lack of information on the management of the site raises concerns for the potential of the development to operate unrestricted and on a 24 hours a day.
- Lack of information regarding control of materials to be stored on site including hazardous materials.
- Concerns that the CCTV equipment may bring risk of invasion of privacy and overlook neighbouring properties.
- Impact of floodlighting in terms of glare and disturbance for neighbouring residential properties, particularly in the evening.
- Concern that a series of shipping containers instead of a purpose built unit or building would be harmful to the visual amenity of the area and bring down the appearance of the area which is predominantly residential in nature.

6. ASSESSMENT AGAINST THE DEVELOPMENT PLAN

West Dunbartonshire Local Plan 2010

- 6.1** The site is allocated as a Housing Opportunity Site within the Adopted Plan and Policy H3 seeks to protect and safeguard allocated housing sites to ensure that they are reserved for residential development only. The development is contrary to this policy as it proposes an alternative land use on the site with insufficient justification to offset the allocation of the site for housing.
- 6.2** Policy LE3 is also relevant and this requires robust justification to be provided for proposals for industrial, business and warehousing developments, which are to be located on sites outwith those specifically zoned and designated for such uses. This justification must demonstrate that the proposed use could not be accommodated on other available industrial/business sites, that it will have significant overall economic benefits and that there will be no significant undesirable impacts on landscape and amenity or major infrastructure implications as a result of it. Irrespective of the fact that the site is allocated for

housing, insufficient justification has been provided to address any of the above criteria and the proposed development is therefore contrary to Policy LE3. This matter is considered in more detail in Section 7 below.

- 6.3** Policy GD1 seeks to ensure that all new development is of a high quality design and respects the character and amenity of the area and Policy H5 seeks to ensure that residential amenity is maintained and preserved. Policy E5 sets an expectation for development proposed on sites with existing trees to take account of these at the beginning of the design process and includes a requirement for a tree survey. The proposal by virtue of its design and appearance will have an adverse impact upon the amenity of the area and given it seeks to remove all trees within the site without any justification, it is contrary to these policies.

7. ASSESSMENT AGAINST MATERIAL CONSIDERATIONS

West Dunbartonshire Local Development Plan (LDP1) Proposed Plan

- 7.1** On 27 April 2016, the Planning Committee took a final decision not to accept the Local Development Plan Examination Report recommended modification in respect of including the Duntiglennan Fields site in Clydebank as a housing development opportunity, and therefore, as a result of the Scottish Ministers' Direction, the Local Development Plan has remained unadopted but continues to be a material consideration in the determination of planning applications.
- 7.2** Similar to the Adopted Plan, the site is also an allocated housing site in LDP1. Policy BC2 safeguards allocated housing sites to ensure that these sites are reserved for residential use only. The proposed use conflicts with the site allocation including Policy BC2.
- 7.3** Policy GE1 is in place to safeguard sites designated for business, industrial and storage use, identifying these as the priority sites for developments such as the one proposed as part of this application. No justification has been provided to evidence that any of these designated sites within the Plan area have been considered in the first instance. Equally, no justification has been provided to evidence why these designated and safeguarded sites are unsuitable to accommodate the proposed storage use. The proposals are therefore contrary to the requirements of Policy GE1 and this is considered in more detail further in Section 7 of this report.
- 7.4** Policy DS1 seeks to ensure a high design quality in all development and Policy BC5 seeks to protect the residential amenity of the surrounding area. Policy GN5 covers similar matters as the tree policy within the Adopted Plan with this focused on ensuring the protection and enhancement of trees. Policy GN5 states that development that would result in the loss of trees or woodland of amenity or biodiversity value will not be permitted unless clear justification can be given and appropriate replanting can be agreed. The containers by their nature are not of a high design quality nor are they considered to be complimentary to the neighbouring residential properties and as such they are contrary to Policy DS1 and BC5. Given the proposals will also compromise all

of the trees within the site no justification to support this approach; the development is also contrary to Policy GN5.

West Dunbartonshire Local Development Plan (LDP2) Proposed Plan

- 7.5** On 19th September 2018 the Planning Committee approved Local Development Plan 2: Proposed Plan for consultation. The Examination Report of Local Development Plan 2 was received on 22nd April 2020 and will be presented to the August Planning Committee for consideration, together with the modified Local Development Plan 2 and associated documents. The findings of the Examination Report have not materially changed the content and form of the LDP 2 policies and land allocations used in the assessment of this application.
- 7.6** Similar to both the Adopted Plan and LDP1, the site is also an allocated housing site in this Plan. Policy H2 of LDP2 functions in the same as the applicable housing policies in the other Plans with this in place to safeguard and reserve allocated housing sites such as this for residential development only. The proposed use conflicts with the land use allocation of this site and is contrary to Policy H2 as a result.
- 7.7** Policy E1 is also similar to Policy GE1 of LDP1 with this policy directing proposals for business, industrial or storage and distribution sites to land designated specifically for such uses. The proposals seek to introduce a storage use on land which is not designated for such a use and it has not been evidenced that any consideration has been given to those sites specifically zoned for storage type of uses or developments. The proposals are therefore contrary to Policy GE1.
- 7.8** Policy E2 offers support for alternative uses of land, which have been previously used for business, industrial or commercial uses subject to them meeting a number of specific criteria. This includes a need to demonstrate that the use will not have an adverse impact on the operation of existing uses or the potential future business, industrial or employment use within the area. The policy also requires that proposals demonstrate that they will protect and enhance the attractiveness of the area and that they will bring tangible economic benefits as a result of their introduction. Insufficient justification has been provided to address any of the above and the development is contrary to Policy E2. This matter is considered in more detail in Section 7 below.
- 7.9** Similarly to Policy DS1 of Proposed Plan 1, Policy CP1 seeks to ensure that developments are of a high quality, adaptable and reflect a design led approach. Policies CP1 and CP2 both require developments to take account of on-site and off-site green infrastructure including trees. The development is not considered to be of a high quality and it does not seek to incorporate the on-site trees into the design approach. As such, it is contrary to Policy CP1 and CP2 of the LDP2.
- 7.10** Policy H4 focuses on the safeguarding of amenity and sets an expectation to protect, preserve and enhance the residential character and amenity of

existing residential areas at all times. As part of this, the policies specifies a presumption against the establishment of non-residential uses within, or in close proximity to, residential areas which potentially have detrimental effects on local amenity. The design, appearance and nature of the development is not compatible to the area and it will have a detrimental impact upon existing residential areas and properties neighbouring the site. Therefore it is contrary to Policy H4.

- 7.11** Policy ENV8 seeks to ensure that developments do not have a significant impact on established residential areas and properties by way of air, light and noise pollution. Where required proposals that have the potential to impact in any of these areas will require to demonstrate that their impact is not significant and provide adequate mitigation where necessary. Whilst the development has the potential to impact in terms of both light and noise, it has not been supported by any formal assessments. Therefore, the proposals are also contrary to Policy ENV8, with this considered in more detail in Section 7 below.

Principle of Development

- 7.12** The site is allocated in all 3 plans as a housing opportunity site and the Council's safeguarding of this site for housing has been consistent for the last 10 years. The redevelopment of the site for residential purposes remains the Council's priority and this is evidenced in LDP2, which is the Council's most up to date policy position. In this Plan, the allocation is important as the site contributes towards the housing land requirements within the Clydebank area. There is a small shortfall overall in the affordable housing land targets from 2019-2030 and the loss of the site would exacerbate this and as a consequence would result in an under provision of affordable housing within Clydebank in the plan period. The supporting information provided for the proposed storage use is insufficient to justify a departure from the plan and offset from its allocation as a housing site and its requirement to contribute to the affordable housing land requirements. Equally it has not been sufficiently demonstrated that the site is incapable of being realistically developed for residential purposes in the future which is required by the relevant policies of the Adopted and Proposed Plans when considering alternative uses to the site allocation.
- 7.13** The justification outlined in the supporting information provided essentially seeks to make a case that the site is unsuitable for housing and that due to a mixture of factors including site constraints (such as contamination, ground levels and existing infrastructure), deficit funding restrictions and poor marketability, the site is unlikely to be developed for housing in the future. However, there is no survey work or evidence such as specific site reports of ground conditions to support these claims and no evidence has been presented to suggest the site is incapable of being developed for residential purposes. The local area, while containing a mix of uses, is now predominantly residential in character. This is certainly the case of Auld Street which includes the sites immediately neighbouring the application site to the South East and North West, both of which accommodate relatively new

flatted developments. Indeed, the proximity of the recently developed adjacent sites indicates that the area is popular and marketable for housing. On this basis, it is considered that the site can realistically be developed for residential development.

- 7.14** The applicant also makes the case that this is a longstanding redundant site and that the length of time it has lay vacant evidences that it is no longer viable for residential purposes. However, how long the site has taken or will take to be developed is not the sole consideration in terms of determining its viability to deliver housing. Indeed, this position is supported in the recent Examination Report for LDP2, in relation to housing land supply, where the Reporter outlines that the requirement for local development plans is to allocate sites that are effective or are expected to become effective in the plan period to meet the housing land requirement. As part of this the Reporter sets out that it is not therefore necessary for all the identified housing sites to be effective immediately, or to be programmed for completion in a defined period, in order for them to legitimately contribute towards meeting the housing land requirement. This is certainly the case for the recently developed neighbouring sites immediately to the north west and south east of the application site which were also allocated for residential land uses. Whilst these also lay vacant for some time, they have nevertheless been developed out for housing. Crucially, the site, irrespective of its duration of vacancy, is a designated housing site and is required to contribute towards the all tenure and affordable housing land requirement of LDP2.

Site Selection

- 7.15** The policies in the Adopted and Proposed Plans support economic development by directing and prioritising proposals for industrial, commercial and business uses to the sites safeguarded for such uses. Where alternative sites such as this, that are not allocated, robust justification is required. A key aspect of the criteria associated with Policy LE3 of the Adopted Plan states that support will only be given where the proposed development could not be accommodated on any other available industrial/business site.
- 7.16** It is considered that there are a number of designated industrial and business sites which are available which could realistically satisfy the applicant's requirements. The justification provided as part of this application is insufficient as it does not evidence that other viable sites (which are specifically designated for industrial, commercial and business use) within West Dunbartonshire have been firstly considered and thereafter been appropriately discounted for justified reasons in favour of the application site as chosen. There is no shortfall of allocated industrial or business land within West Dunbartonshire and no specific reference is made to any of these designated industrial or business sites, justifying why they are unsuitable to accommodate this use as proposed. As a result, the assertion made as part of the application that there are no other sites available within West Dunbartonshire aside from the application site without any sufficient justification to evidence this, is considered to be unsubstantiated.

- 7.17** Furthermore no case for a site-specific locational need or requirement has been made, demonstrating why the use and development as proposed requires to be located at this particular site. Given the absence of any such information including a sequential test, no case as required by the relevant industrial and business policies has been made to realistically consider the proposals for the alternative proposed use.

Economic and Social considerations

- 7.18** The justification that is provided in support of the proposed development cites perceived economic and social benefits of the use once operational. However, the references to these are vague and no indication has been provided of the potential job creation or the tangible economic benefits to the area that would result from the proposals. As part of this, no assessment has been provided demonstrating a market demand, for the provision of storage at this location and the economic and social benefits it could bring to the local area and economy as a result.
- 7.19** Notwithstanding the lack of supporting information in this regard, it is considered that any economic and social benefits of this type of self-storage development would likely be limited. More specifically, it is considered any economic benefits that could be realistically gained from these proposals are unlikely to be 'significant' and outweigh the primary material considerations including the allocation of the site for housing or that designated industrial and business sites within the West Dunbartonshire that are being overlooked in favour of this site without any justification to evidence otherwise.

Impact on Amenity

- 7.20** The visual impact of 115 storage containers positioned in continuous rows and surrounded by palisade security fencing adjacent to and 'sandwiched' in-between recently completed and landscaped housing developments would appear incongruous and have a detrimental impact to the residential context and amenity. As part of the development, all trees are to be felled and the ground is to be fully hard surfaced and no landscaping or tree planting is proposed that might mitigate the containers' appearance, enhance the site and complement the presence of the neighbouring housing developments.
- 7.21** All the trees that are to be felled are species that have matured over time given the lack of activity on the site and which now make a positive visual and environmental contribution to Auld Street. With no replacement planting proposed, the development will have a negative impact in terms of the visual amenity of the site and also from an environmental perspective is contrary to the applicable tree policies. Conversely, a residential development on this site could potentially seek to incorporate a degree of tree retention into the scheme and offer new forms of landscaping and greenery within the layout which would enhance the site and the visual amenity of the area.
- 7.22** Given the proximity of neighbouring residential properties to the application site, there are also concerns regarding the compatibility of the development

and its impact on residential amenity. These concerns are echoed by both the Council's Road Service and Environmental Health Section who have both advised that the application lacks fundamental supporting information in order to competently assess the proposals. Container storage by its nature can be impactful in terms of noise generation and in terms of bringing a level of intensification and additional activity to an area. No details relating to how this business will operate have been provided including its hours of operation, site management, accessibility arrangements and vehicular trip generation and traffic control measures. A Noise Impact and Lighting Impact Assessment are also absent from the application and these would be material to the assessment of the application when considering the impact of the development on neighbouring residential properties. This information has not been sought from the application as the principle of developing this site as a storage facility is unacceptable in the first instance.

8. CONCLUSION

- 8.1** The proposed use is not an appropriate form of development at this location and it is contrary to both the Adopted and Proposed Local Plans with the site consistently allocated for housing. The site is safeguarded for residential purposes with a role to contribute to the housing land requirement for Clydebank. The evidence or justification is insufficient to allow for the alternative use to be considered as an acceptable departure from its site designation.
- 8.2** No site specific locational need for the development has been established and there is a lack of justification that the use could not be accommodated in other sites specifically designated for business, commercial and industrial uses. The economic, environmental and social benefits cited in support of the use have not been substantiated, however it is still considered that in any case these will be limited and will not be of a significance or extent as to offset from the allocation and role of the site for delivering housing.
- 8.3** The development by virtue of its design, appearance and layout will have an adverse impact upon the visual amenity of Auld Street and in particular the setting of the established residential flatted blocks that immediately neighbour the site to either side. The proposals also fail to provide appropriate information to address concerns regarding its impact upon the residential amenity of the neighbouring flatted properties by virtue of noise, light and traffic disturbance.

9. REASONS FOR REFUSAL

- 1.** The application site is an allocated and safeguarded housing site and the redevelopment of this site for this alternative storage use would be prejudicial to the spatial strategy and undermine its ability to contribute towards the housing land requirements for West Dunbartonshire. The proposals are therefore contrary to Policy H3 of the Adopted Local Plan 2010; Policy BC2 of

Local Development Plan 1: Proposed Plan (LDP 1 2016); and Policy H2 of Local Development Plan 2: Proposed Plan (LDP2 2018).

2. The proposals fails to demonstrate and evidence any reasonable consideration and assessment of designated industrial, commercial and business sites available elsewhere within the West Dunbartonshire area in order to discount their suitability in favour of the application site as chosen. The proposals are contrary to Policy LE3 of the Adopted Local Plan (2010), Policies BC2, GE1 and GE2 of Local Development Plan 1: Proposed Plan (LDP1 2016) and Policies E1 and E2 of Local Development Plan 2: Proposed Plan (LDP2 2018).
3. The proposed development fails to respect the character and amenity of the surrounding areas by reasons of its visual impact upon and visual relationship with neighbouring residential properties within the street scene, and its failure to consider and minimise the loss of trees. Accordingly, the proposal is contrary to Policies GD1 and E5 of the Adopted Local Plan (2010), Policies DS1 and GN5 of Local Development Plan 1: Proposed Plan (LDP1 2016) and Policies CP1, CP2 and H4 of Local Development Plan 2: Proposed Plan (LDP2 2019).
4. The proposed location of the development immediately neighbours flatted properties on Auld Street. The proposed development has failed to submit appropriate information and assessments to demonstrate that the use once operational will not adversely impact the residential amenity of these neighbouring properties by virtue of noise, traffic and light disturbance. Due to the lack of supporting information in this regard, the proposal is contrary to Policy H5 of the Adopted Local Plan (2010), Policy BC4 of Local Development Plan 1: Proposed Plan (LDP1 2016) and Policies H4 and ENV8 of Local Development Plan 2: Proposed Plan (LDP2 2019).

Peter Hessett
Strategic Lead - Regulatory
Date: 10th June 2020

Person to Contact: Pamela Clifford, Planning, Building Standards and Environmental Health Manager
Email: Pamela.Clifford@west-dunbarton.gov.uk

Appendix: None

Background Papers:

1. Application documents and plans
2. West Dunbartonshire Local Plan 2010
3. West Dunbartonshire LDP - Proposed Plan
4. West Dunbartonshire LDP - Proposed Plan 2
5. Consultation responses
6. Representations

Wards affected: Ward 6 (Clydebank waterfront)

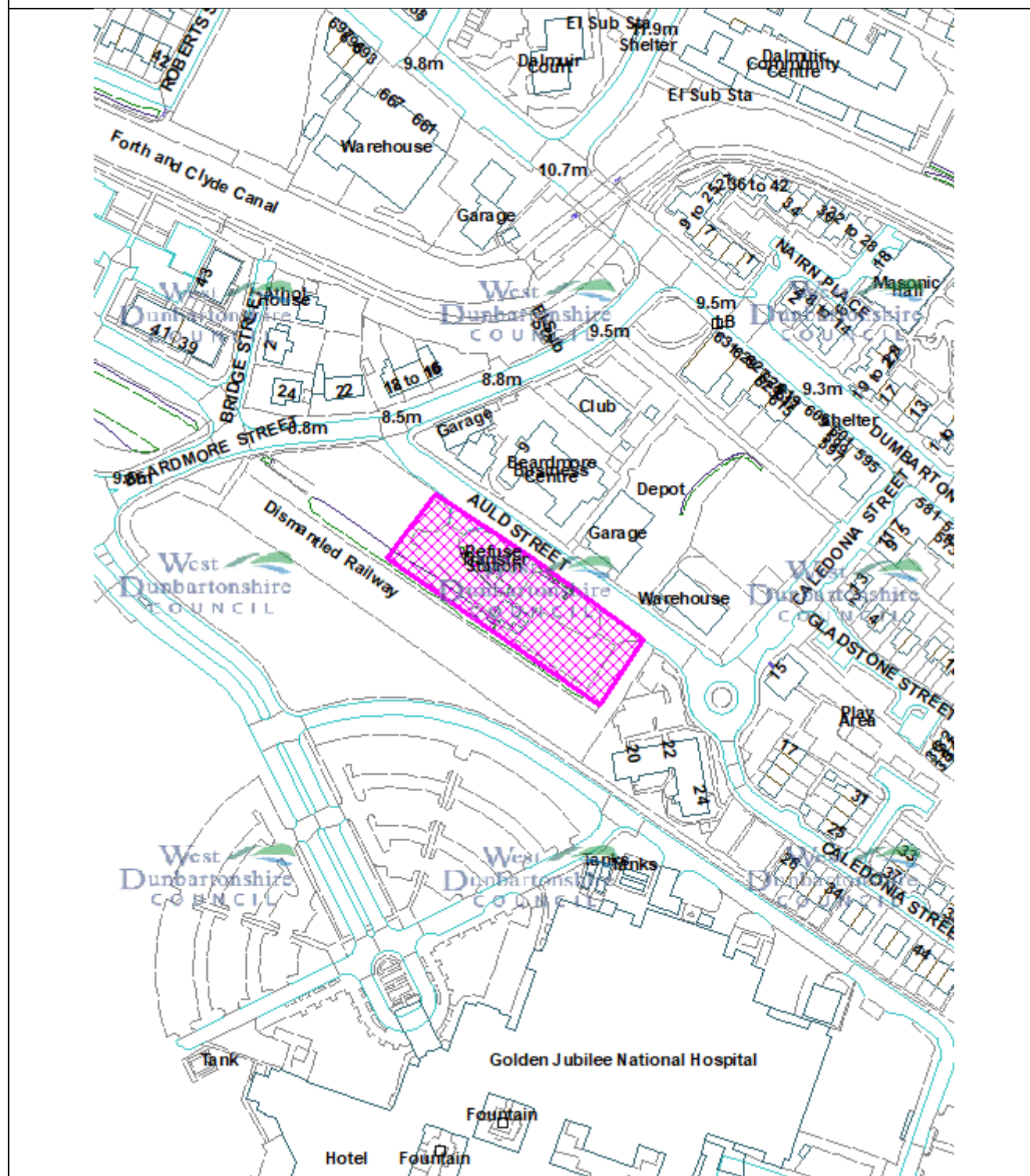
Map Register No: HQ640

Date: 28 May 2020

DC20/076

Erection of storage facility
and ancillary works

2 Auld Street
Clydebank
G81 4HB



WEST DUNBARTONSHIRE COUNCIL**Report by the Strategic Lead - Regulatory****Planning Committee: 10th June 2020**

Subject: Queens Quay Design Codes: Consultation Responses and Street Naming Strategy

1. Purpose

- 1.1** To inform the Committee of the main points of the representations received from the consultation exercise, details of the Proposed Street Naming Strategy and to advise on the next steps.

2. Recommendation

- 2.1** That the Committee notes the main points raised within the consultation responses which will shape the finalised design codes which will be presented to the August Planning Committee for approval.
- 2.2** That the Committee approve the draft Street Naming Strategy Annexe and that details of the street names based on the Strategy will be presented to the August Planning Committee for approval.

3. Background

- 3.1** A Design Framework, including Masterplan has been approved for the central 23 hectares of the site and Planning Permission in Principle has been granted for up to 1,000 new houses, commercial, health and leisure uses, public realm and road infrastructure. The Design Codes built upon these approved documents by providing detailed guidance regarding the future development form in particular the housing element.
- 3.2** The Draft Queens Quay Design Codes were approved for consultation by Planning Committee on 12th February 2020. Appendix 1 includes the Design Code document. The guidance was published for consultation on 17th February 2020 for a 9 week period, with comments sought by 20th April 2020. The guidance was made available on the Council's website, in libraries and sent to relevant organisations such as community councils, housebuilders, housing associations and relevant Council services. It was also sent directly to relevant landowners/developers and advertised in the local press. In light of the disruption of COVID 19 the consultation period was extended by another 4 weeks to the 20th May 2020.

4. Main Issues

- 4.1** In response to the consultation, 25 responses were received: 4 from key agencies and 21 from design professionals, individual developers, individuals, Council Services, partner organisations and community groups. This included the landowner and the local MP. Some of the responses received were anonymous. A short summary on the responses received is provided below together with initial responses.

Key Agency Consultees

- 4.2** The comments from the Key Agencies centred on the features of the landscape approach applied across the site that related to the specific discipline of the agency/consultee.

This included;

- Open Space provision
- Sustainable Urban Drainage solutions
- Diversity in tree and planting species
- Measure to protect the Wintering Redshank.
- Relationship of development to the Titan Crane

- 4.3** The comments are noted and the Design Codes could provide guidance around the approaches to SUDS and the diversity of planting species to compliment the site wide approaches to this. This could be linked to the planning permissions on the site and the masterplan.

Responses from design professionals/developers/ individuals/council services and community groups

- 4.4** The majority of the respondents agreed that the document was easy to use and navigate due to the formatting and approach taken and that the use of photographic examples of 'preferred' and 'not preferred' development details was particularly helpful. It was felt by respondents that the document would give comfort to developers that their development would sit within a community of consistently high standards of development, while giving room for architectural interpretation and expression. Some parties suggested that the level of prescribed detail should be increased to be more aligned with more prescriptive Design Codes, while others suggested the existing codes were too prescriptive. The respondents also helpfully identified the areas of the document where they felt this applied and where further clarity would be beneficial in communicating what the Council wished to see.

The main points of responses are summarised as follows:

- Clarity over the level of detail and prescription contained in the document and what falls to developer design.
- The importance of maintaining consistency of the quality of development proposals over time.

- Clarity around the areas that the Design Codes can influence and those that fall under the remit of the Planning in Principle Permission.
- Additional detail around the character and built form of the waterfront in the diagrams, explanations and images.
- More information on roads and parking infrastructure.
- Consideration of the local climate and the impact on development.
- Suggestion to include details of public art in the document.
- Consideration of street naming to link in with addressing the culture and history of the site, including pre-industrial heritage.
- The inclusion of health and wellbeing as a thread underpinning the document.

4.5 Many of the comments received through the consultation response will require further consideration to ensure that there is clarity around the detail, the purpose and application of the Design Codes. It has always been envisaged that the Design Codes document would strike a balance between providing guidance and being overly prescriptive to deliver high quality places. This would be integrated into the planning and design processes that would shape the Queens Quay environment. An over prescriptive document would weaken the approach and vision for the Queens Quay site, however, a level of guidance and advice that still allows freedom for developers to innovate would be encouraged with marketability and commercial viability would be embedded in the Design Codes.

Response from the landowner

4.6 The landowner/developer (CRL) responded on the online survey and this was supplemented further with a detailed review of the Design Codes prepared by a consultant architect firm in order to communicate more fully their views on the Codes. The document 'Queens Quay Design Guidelines Review' acknowledges that the aspirations to realise a high quality development that creates distinctive and memorable regeneration of the site are goals that are shared between the Local Authority and the developer. The importance of balancing flexibility, creative response and the shifting economic and social picture is highlighted in the document.

The main points of responses are summarised as follows:

- The document is aligned in many respects with the approach taken in the Design Codes to offer a flexible way forward for developers with enough detail to create cohesion across the whole site.
- Demonstrates possible detailed approaches to the design of the development plots and streets leading to the waterfront while highlighting where further information would be beneficial.
- Seeks additional detail around roof pitches, massing and building heights.
- It mentions the section on 'materiality and robustness' as a particular strength of the Design Codes.

- It shows the eastern edge of the basin and the mixed use development as pavilion blocks with surface parking (which is a departure from the Masterplan).
- Suggests the need for confirmation from council services on the acceptance of the proposed parking and roads strategies in the Design Codes.
- Looks for more detail and emphasis on the importance of the design quality on the waterfront including giving more scope for variation and flexibility in massing and height.
- Suggests that the Design Codes may be inadvertently promoting suburban development over urban and more clarity is required around this.
- Seeks clarity over some of the diagrams, including the treatment of the mews area, street widths that are dictated by the masterplan, the nature of the street/road and the approach to landscaping and character areas, especially around the basin.

4.7 The detailed response from the landowner is welcomed and the Review document which critiques the Design Codes offers clarity around where the ambitions of the Council and the developer are aligned. The suggested built form diagrams and drawings shown in the Review document give weight to the principles set in the Design Codes while demonstrating what can be achieved when using the Design Codes to take forward development on the various plots. The Review document assists in giving some direction as to where more clarity or more detail is required, for example; the response demonstrated that the reference to ‘pavilions’ at the waterfront could be misconstrued and that a very literal interpretation of the diagrams could be taken by a prospective designer. This submission will facilitate further discussion before the Design Codes are finalised.

Annexes to the Design Codes

4.8 The work done around the Design Codes by officers and some of the responses has suggested that further work to develop strategies around sustainability, street naming, heritage and the arts should be developed and taken forward. Already, work has commenced on an Energy Strategy annexe which will encourage a ‘fabric first’ approach in the design of the future homes on Queens Quay. This will ensure the sustainability credentials of new development work cohesively with the new energy centre and this will be presented to a future Planning Committee for approval.

- Annexe 1: Queens Quay Energy Strategy
- Annexe 2: Queens Quay Street Naming Strategy
- Annexe 3: Queens Quay Arts Strategy

It is recognised that a well designed place, good quality housing and the sense of identity that comes from heritage and culture have positive effects on levels of health and wellbeing. The Design Codes and subsequent annexes

for the Queens Quay site are intended to work as a suite of documents with health and well being strategies embedded as a cross cutting theme in all documents.

Annexe 2: Queens Quay Street Naming Strategy

- 4.9** A Street Naming Strategy will reflect the aspiration in the draft Queens Quay Design Codes to ensure the history and heritage of Clydebank and the surrounding area is referenced in the wider development. This document will form Annexe 2 to the final Queens Quay Design Codes and is contained in Appendix 2. This strategy seeks to proactively contribute to placemaking across the Queens Quay site, making known key heritage narratives and telling Clydebank's stories. It is intended that, through this innovative approach to street naming, both residents and visitors to the area alike will feel positively engaged in Clydebank's streetscape and the town's unique identity.
- 4.10** The strategy seeks to link with the other annexes and set out a contextualised and scalable framework for street naming that centres on a storytelling approach and a strong narrative. This may open up opportunities to create digitally available heritage interpretation and associated learning materials. Narratives to the history of Clydebank as a town of shipping and industry but may also relate to the pre burgh history or individuals such as Ian McHarg, a Clydebank born landscape architect of international reputation who offers a clear link to the wider landscape of the Kilpatrick Hills and beyond.

Responses from Elected Members Workshop

- 4.11** Elected members were presented with the Design Codes Draft Document at a workshop on the 5th December 2019. The document was well received and offered assurances that the development coming forward would be held to a high standard and that the approach across the site would be consistent. Members sought clarity on how the details provided can help to establish the character on a new development; they encouraged the use of colour on the site, guidance on garden sizes and boundary treatments onto streets and the approach to bin stores. The comments have been considered and will be taken forward with further clarity provided in the Design Codes document.

Response by Place and Design Panel

- 4.12** A Place and Design Panel session was held on 14th January 2020 where the Design Codes were presented. The Panel praised the ability of the document to tell prospective developers enough to ensure clarity around expectations but still offer flexibility for design. They could see the 'deep thinking' and 'rigour' that had been applied to the Design Codes and were in favour of the approach to limit prescription while driving quality and good design. The key areas where the Panel felt the document could be strengthened: additional detail around pavilion buildings on waterfront; the landscaping illustrations could be positioned first in the document; the application of a 5 year defects

liability period to the landscaping; cycling should feature more in the document. The Panel also suggested that consideration should be given of what it takes to establish communities for example schools. The comments of the Panel are noted and that it is considered that there is scope to include recommendations around the landscape defects period, the inclusion of more focus on cycling and the addition of more detail around the possibilities for the pavilion blocks on the waterfront. Creating a sustainable community on this site will be fundamental to the success of this site as well as for the wider Clydebank area.

Next Steps

- 4.13** Due to the level of detail raised within many of the consultation responses, further time is required to fully consider, respond and amend the Design Codes as appropriate. The representations received will result in changes to the document and these will also require to be discussed with the landowner before finalising the Design Codes, especially in relation to the comments raised to the document by the landowner themselves. A finalised version of the Design Codes, incorporating the Queens Quay Energy Strategy Annexe, Street Naming Strategy Annexe along with the full responses proposed to the points raised in the consultation will be presented to August Planning Committee for approval. A list of proposed street names for future streets in the Queens Quay development will also be presented to the August Planning Committee.

5. People Implications

- 5.1** There are no personnel issues associated with this report.

6. Financial Implications

- 6.1** There are no financial issues associated with this report.

7. Risk Analysis

- 7.1** It was not considered necessary to carry out a risk assessment on the matters covered by this report.

8. Equalities Impact Assessment (EIA)

- 8.1** Screening has been carried out on the Design Codes Document and the final document will address the issues raised in full.

9. Consultation

- 9.1** Details of the consultation are set out in Section 4.1 above and the responses to the consultation are set out in the report.

11. Strategic Assessment

- 11.1** The guidance is considered to support the Council's strategic priority of improving economic growth and employability.

Peter Hessett
Strategic Lead - Regulatory
Date: 10th June 2020

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Appendices: Appendix 1 –Queens Quay Design Codes Draft 2nd
February 2019
Appendix 2 – Proposed draft street naming strategy –
Annexe 2.

Background Papers: Committee report: Draft Design Codes Committee Report
12th February 2020.

Wards Affected: Ward 6 Clydebank Waterfront.



QUEENS QUAY DESIGN CODES

Draft 02.12.2019

INTRODUCTION

These Design Codes set out structured guidance for designers and developers. They provide a structured approach for West Dunbartonshire Council to assess Planning Applications.

The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision.

CONTEXT

CLYDEBANK CONTEXT

The site is in a prominent location on the northern bank of the River Clyde and is strategically important for Clydebank's ongoing regeneration.

The new masterplan for Queen's Quay, will see the redevelopment of a significant area of Clydebank's townscape on the site of what was originally John Brown shipyards. This is an opportunity to change the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry.



Queens Quay Masterplan Site



At its peak employing over 10,000 people, John Brown Shipyards was one of the most commercially successful shipyards in the world, becoming famous internationally for its shipbuilding quality.

Following the yard's closure in 2001, only a few features remain. Dominating the site is the Titan Crane, now an icon for the town's future regeneration.

There is also an opportunity to change the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry

Existing Townscape

The once intact townscape of Clydebank has been badly eroded through damage during the Blitz and inappropriate demolition and redevelopment during the period 1960–2000, leaving few key landmarks remaining.

The result is a lack of defined 'street edge', large areas of 'waste-land', no 'celebration of corners' and a lack of height eroding the urban townscape, all resulting in a lack of a sense of place.

Although there are some housing areas adjacent to the site, these are relatively isolated and the new development offers an opportunity to knit these in to a new urban fabric.

Existing Public Buildings

The development site is well -located for pedestrian access to a wide range of public facilities at Clydebank Town Centre.

- **Within 5 minute (400metre) walking distance:** Leisure centre, Town Hall, Clydebank College, plus the proposed Clydebank transport Hub which includes improvements to the existing train station
- **Within 10 minute walking distance:** Golden Jubilee National Hospital, Shopping Centre. Local Primary Schools, Rothesay Dock industrial area

Public Transport and Cycling

Clydebank Train station and Chalmers Street Bus Station sit within 5 minutes walking distance of the site sits. There are bus stops along Glasgow/ Dumbarton Road.

Existing Green Network

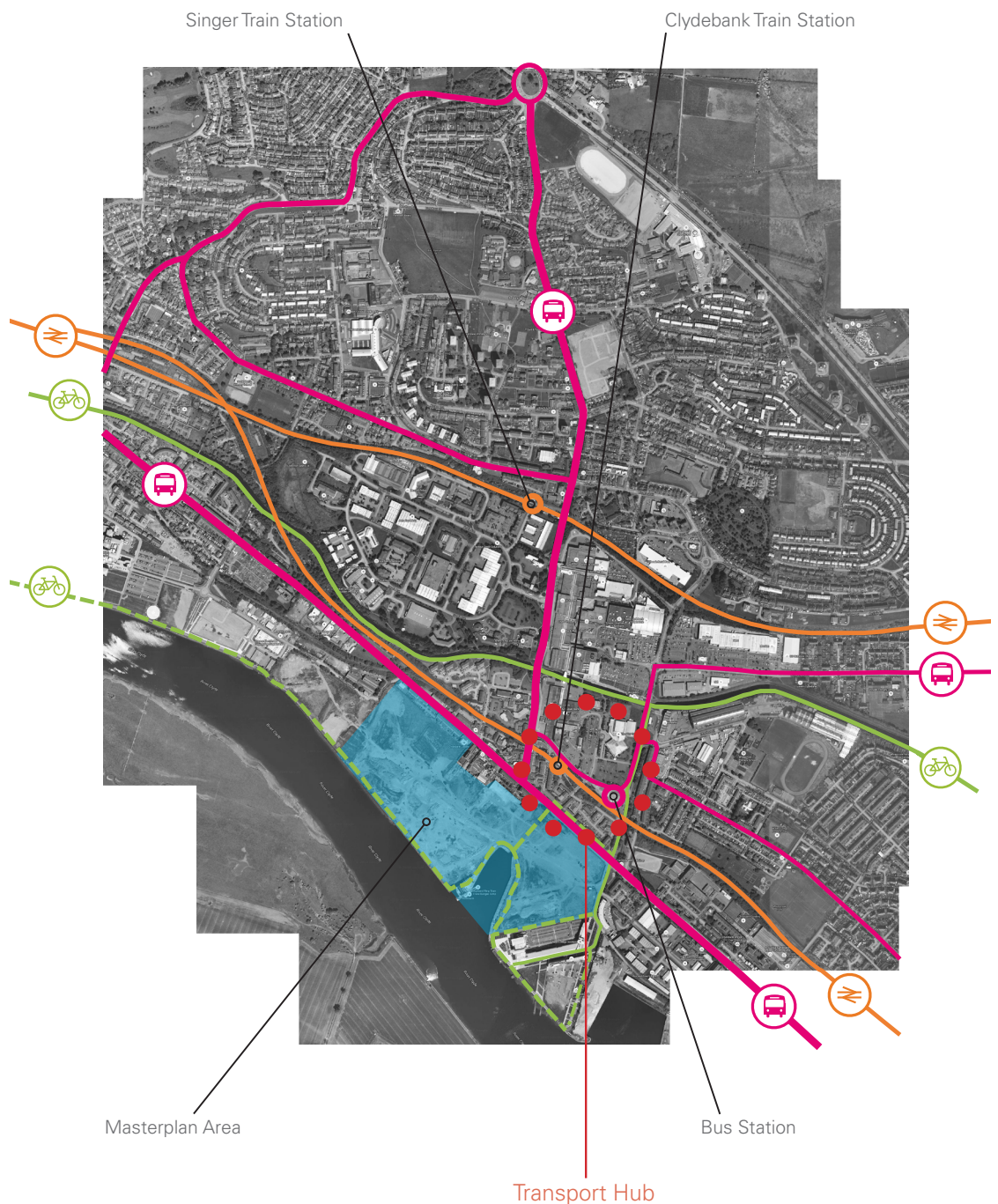
Greenspaces within Clydebank are isolated and the town centre lacks open greenspace of a significant useable scale. The riverside walkway west of the site is not accessible to the public, but the aim in the long term is to link it to the Queens Quay area to form a continuous riverside walkway.



Public Transport Hub

Connecting Clydebank is a transformational project which seeks to revitalise the civic core of Clydebank; forming essential links with the ongoing Queen's Quay development, the present town centre and the many active and public transport routes into the area. It aims to link different strands of investment to reinforce Clydebank as a destination.

The project originated from a successful Charrette process in 2015 (refer Clydebank Charrette Action Plan) and has been progressing in consultation with funding partner Sustrans following the successful funding award from the Sustrans Community Links Programme in 2016.



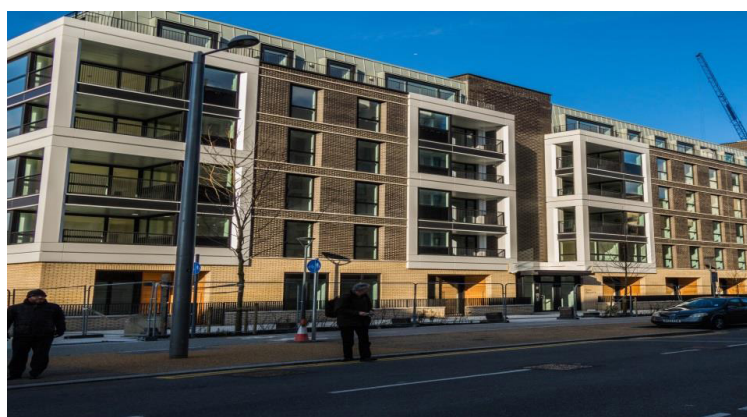
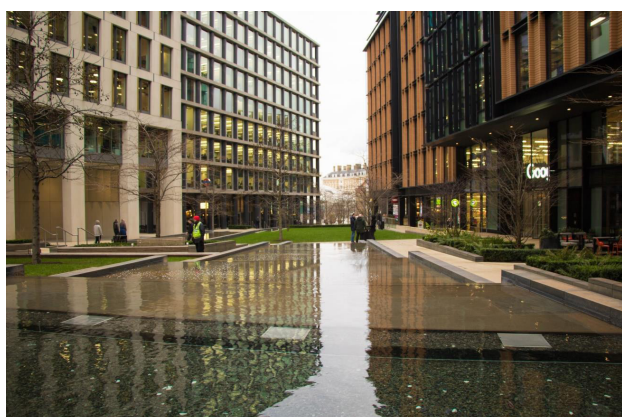
OBJECTIVES AND ASPIRATIONS

Clydebank has a rich and colourful past as an important industrial heartland. This history is a big part of what makes it a special place. In recent years Queens Quay has been an industrial wasteland, however it is now being transformed into an exciting new neighbourhood.

West Dunbartonshire Council's Local Development Plan and supporting documentation highlight Queen's Quay as a location where high quality design and attention to place making is an essential component of new development in order that Queen's Quay becomes a destination neighbourhood.

Following visits to Kings Cross and Stratford with Elected Members, the Planning Authority have set out an Aspirational Vision for Queens Quay based upon:

- Using remaining elements of the shipyard to help define the place
- Using materials that complement the industrial character but can also integrate new development.
- Minimal change to the robust character of the existing dockyard quay structures will be subject to minimal change.
- Massing, scale and detailing which complement the historic, large scale structures on the site and its previous industrial character



West Dunbartonshire Council planning and regeneration officers visited recent exemplar developments to identify precedent projects that demonstrate high design quality

THE MASTERPLAN

A masterplan has been prepared for the development area by Clydeside Regeneration and Dawn Group in partnership with West Dunbartonshire Council. The masterplan's objectives are:

- **Creating a quality public realm:** An enhanced urban (rather than suburban) environment with safe pedestrian connections to the town centre
- **Improving connections:** Linking to other development sites along the water's edge and creating better pedestrian and cycle routes to the Titan Crane and Riverside Park.
- **Creating a special place:** Developing the site as a key destination with opportunities to enhance cultural offerings in the area while creating a sustainable community.
- **Regeneration:** Changing the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry.

The current masterplan and associated planning consents include detailed proposals for infrastructure, together with some areas of public realm and greenspace:



The Fitting-Out Basin: The existing concrete deck structure will be made safe for day to day public use as well as events; it will be the location for a seating areas and information display as well as HMS Hood Park - a local play park and garden.



Riverside Park: West of the basin, a greener approach is proposed and new homes will look out over the river towards countryside. The river edge will provide a wildlife habitat; behind this the Clyde Path (a continuous 4 metre wide shared path/cycleway) sits within a pattern of subdivisions derived from the layout of the berths and buildings that previously occupied the site. This park subtly accommodates slopes to raise the development plots up above flood level. All residential streets will have path connections to this area which will be lit to enable safe use at night, as part of the cycle network.



Main Street: The proposed Main Street takes the line of the previous plate gantry, which ran between the platers shed and the fitting-out basin. A strongly defined tree-lined street is the main traffic route, with generous verge zones behind the footways providing space for an avenue of street trees

To calm traffic speed, the six metre carriageway of the Main Street is broken into sections corresponding to future junctions with residential side-streets. In-carriageway bus stops with raised kerbs are provided at intervals on either side of the street.

There are two greenspaces within the main street corridor. A Pocket Park contains play, lawn and garden seating spaces, and at Cable Depot Road a Community Orchard has a selection of fruit trees significant to the region, planted on a five metre grid amongst meadow with seating and mown grass paths.

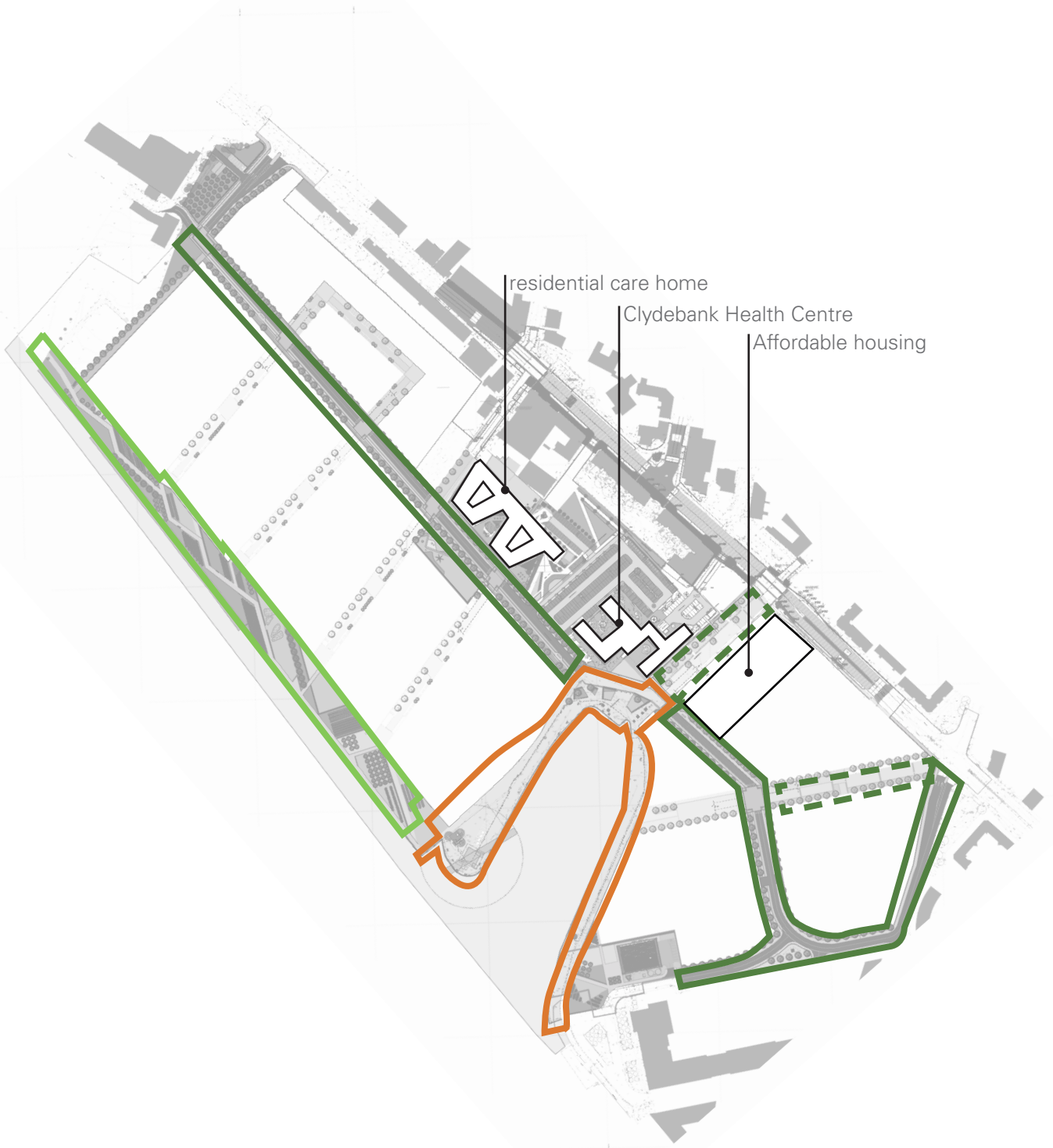


Boulevards: Spatial connection between the basin and Dumbarton Road is provided by two broad avenues which intersect at the basin. They are aligned axially with the Titan Crane and provide strong visual and spatial connections to and from Dumbarton Road, bridging between the town and its reclaimed, revitalised waterfront.

These linear spaces are intended to be busy and dynamic, with strong built frontages and flexible shared space to enable commercial life. Trees line the axial view to emphasize a green connection to the river. Materials will co-ordinate with the current Council proposals for public realm on Dumbarton Road.

Neither will connect onto Dumbarton Road for vehicles, but instead will provide pedestrian and cycle connections with occasional use by emergency vehicles.

DIAGRAM: Masterplan components



West Dunbartonshire Council Planning Policy and Guidance

West Dunbartonshire Council's Local Development Plan and supporting documentation highlight Queen's Quay as a location where high quality design and attention to place making is an essential component of new development in order that Queen's Quay becomes a destination neighbourhood.

Following visits to Kings Cross and Stratford in London with Elected Members, the Planning Authority have set out an Aspirational Vision for Queens Quay:

- Using remaining elements of the shipyard to help define the place, such as service gallery and mooring bollards
- Using materials that complement the industrial character but can also integrate new development.
- The robust character of the existing dockyard quay structures will be retained, with minimal change.
- Providing active travel routes which connect Queens Quay to the town centre
- Engaging with existing streets and creating a new urban street pattern
- A new health centre will be a busy focus
- An area wide heating system
- Improved green connections with boulevards and linear greenspace, pocket parks and edible landscapes.

The Queens Quay design codes build on these aims and objectives, setting out design parameters for development of the remaining plots.

Relevant planning consents

The following planning approvals have been granted for the Queens Quay site, to date:

- A - DC16/240 Spine Road: formation of new spine road (including junction alterations at Cart Street, Alisa Road and Cable Depot Road) formation of public realm around the basin and river edge (including hard and soft landscaping and linear park) and landscaping strategy for full masterplan site . Infrastructure is ongoing
- C - DC16/244 Care Home: a two /three storey home for life and a day care centre
- F - DC17/231 Energy Centre: the largest ambitious district heating system in Scotland powered by heat pumps which will extract water from the River Clyde
- G - DC18/033 Connecting Clydebank: new controlled and uncontrolled crossings, improved footpaths, better cycle access, public realm and road works to Dumbarton Road, Glasgow Road and Hall Street in order to improve the public realm connecting the main shopping area to the north of the site and the new Queens Quay development to the south.
- B - DC18/057 Clydebank Health Centre: this will act as a catalyst for significant change, bringing a wide range of health services together - a focal point and landmark feature.
- D - DC18/272 Mixed Use Development on Titan Boulevard (part of plot 5 which is excluded from the scope of the design codes).
- E - DC18/275 Titan Boulevard: new street, public space, landscaping and associated street furniture.

THE DESIGN CODES

THE DESIGN CODES

Scope

The Queens Quay design codes build upon West Dunbartonshire Council's Aspirational Vision, setting out the parameters for development of the remaining plots.

These Design Codes set out structured guidance for designers and developers. They provide a structured approach for West Dunbartonshire Council to assess Planning Applications.





Although design codes guide the development in three dimensions, a two dimensional Regulating Plans are included with them, to enable code users to locate where the provisions of the code will apply, and to express how the codes relates to plots and different character areas.

The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision. Some design components are already described in detail in the masterplan, or consents have already been granted for development. These are described in more detail in the following pages.

It is anticipated that development at Queens Quay will be phased with development ongoing for 5 to 10 years. As a consequence, the overall development site will be parcelled into smaller development plots (*see following page for details*). In some locations, each plot plus an adjacent residential road will be parcelled together.

Site	Residential road	Greenspace	Use
1	no	no	mixed
2	no	no	mixed
3	no	no	leisure
4&5	no	no	mixed
7	yes	no	residential
8	no	yes	residential
9	yes	yes	residential
10	yes	yes	residential
11	yes	yes	residential
12	yes	yes	mixed

Parcel subdivisions

-  plot boundary - housing
-  plot boundary - mixed use
-  greenspace associated with each plot
-  residential access associated with each plot



Transport and Access Hierarchy

Access hierarchy outwith Design Code areas



Dumbarton Road



Masterplanned vehicular access routes - currently under construction



Pedestrian and cycle route with occasional access for emergency vehicles



Pedestrian and cycle route only

Access hierarchy within Design Code areas



Residential access road - vehicles, pedestrians and cyclists



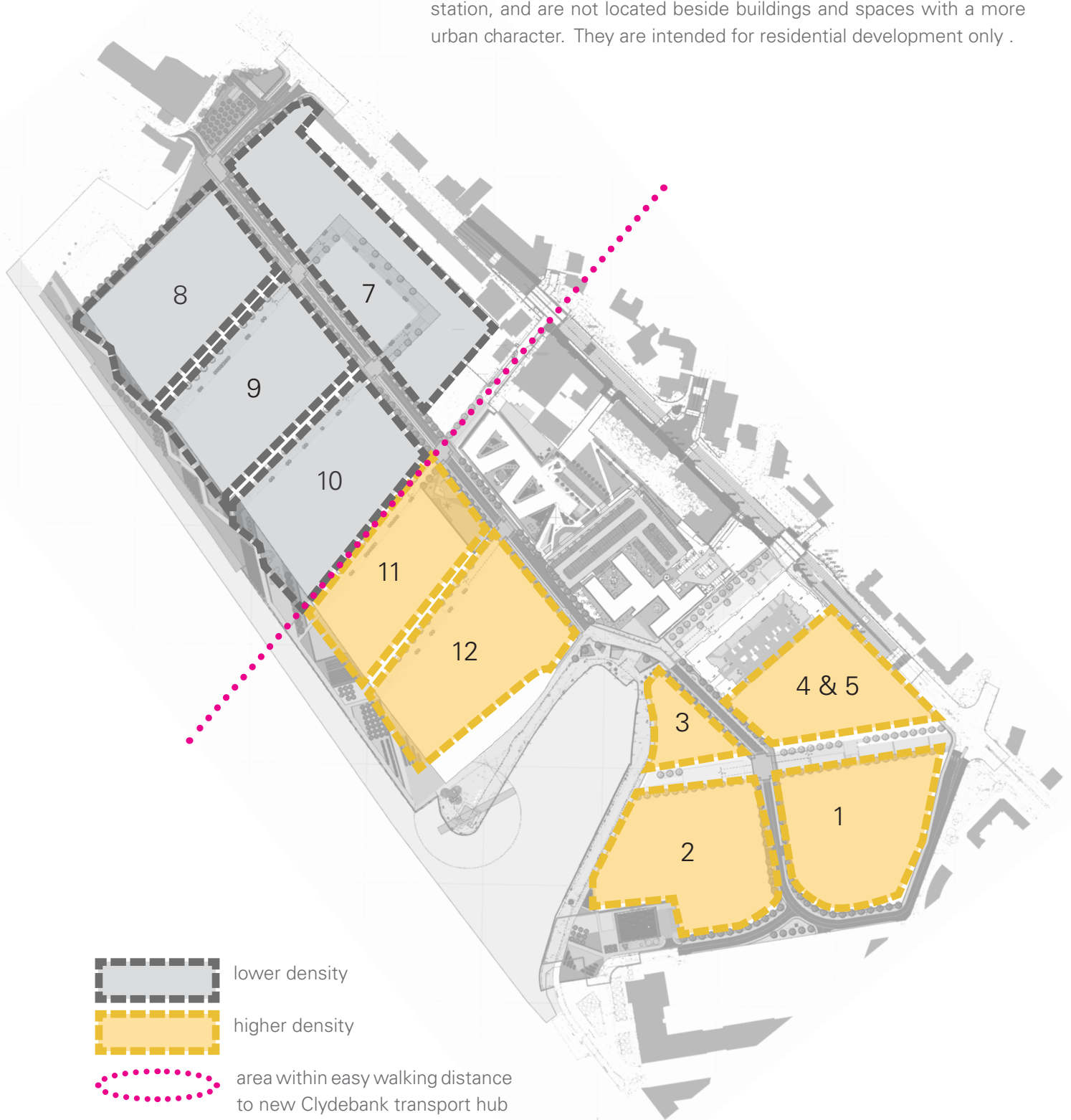
Residential pedestrian and cycle links with occasional access for emergency vehicles



Density

There is a natural division within Queens Quay. The plots shaded in yellow are closer to the train station and public transport hubs. They are also located in an area where a higher density is more appropriate because of the scale of development beside them and the proposed mix of uses.

The plots shaded in grey are further from the transport hubs and train station, and are not located beside buildings and spaces with a more urban character. They are intended for residential development only.



Road hierarchy & street network

In addition to compliance with local and national design guidance, the Design Codes define a user hierarchy for different types of streets - based on their significance in terms of both place and movement for pedestrians, cycles and vehicles. Applying the hierarchy will help to create a series of attractive, sociable urban spaces as well as controlling traffic and promoting the attractiveness of walking, cycling and the use of public transport. The principles of the street hierarchy are mandatory, although their exact alignment and design will be fixed through further planning consents, on a plot by plot basis.

New residential roads within Design Codes areas link the Main Street to development plots; these form a route to off-street parking areas and are also the location for on-street parking.

Routes running through the centre of the riverside plots are envisaged as fully pedestrianised routes primarily for the residents of Queens Quay. Other routes are predominantly for pedestrians and cyclists, and will be only occasionally used for emergency traffic, delivery and maintenance.

Table 2; street design and geometries (design code areas only)

	Residential access roads	Residential pedestrian & cycle links	Residential pedestrian & cycle links + emergency access
Design speed			
Target speed for traffic	20 mph	Not applicable	10 mph
Street dimensions			
Minimum carriageway width	4.8 metres	2.5 metres	3.8 metres
On street parking	Yes either or both sides, 2.6m width	Not applicable	Not applicable
Footway	1.8m to either side	Not applicable	Not applicable
Verge	Yes – same width as on street parking zone	Not applicable	Not applicable
Direct plot access for traffic	No	Not applicable	Not applicable
Vehicular access to parking courts	Yes	Not applicable	Not applicable
Public transport			
Pedestrian access to transport hub	Within 400 metres walking distance	Within 400 metres walking distance	Not applicable
Bus access	No (Main Street only)	No (Main Street only)	Not applicable
Street design details			
Traffic calming	Yes (linear route)	No	
Vehicle swept paths	Yes – all vehicles	No	Yes – emergency vehicles only
Junction sightlines	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction spacing	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction radius	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only

Parking

Queens Quay is very close to the improved Clydebank Transport Hub, so there is an opportunity to create a new urban area which prioritises movement on foot and by cycle, rather than by car. The Design Code therefore stipulates maximum parking numbers for individual development plots.

There is a natural division within Queens Quay where parking density changes. Plots which are closer to the train station and public transport hub also contain elements of primary building frontage to Dumbarton Road and the Basin. This is an area where proximity to the new transport hub, plus a higher dwelling density will support a more urban character. This can potentially be undermined by larger areas of parking so a maximum parking ratio of 70% is to be provided in these areas.

It is likely that car ownership will be higher for plots which are further from the transport hubs and train station. In these locations the density is lower. Because of this, they can achieve higher parking levels. A maximum parking ratio of 100% is to be provided in these areas. (Parking allowances should include garage spaces).

The Building Standards require 5% of parking spaces to be 'accessible'. These spaces require a wider parking bay and must be close to the building entrance.

Car club spaces should be provided to offset lower numbers of parking spaces. (WDC to confirm requirements)

Electric charging points should be provided (WDC to confirm requirements)

Visitor parking for cars, small vans and motorcycles should generally use shared, public on-street parking.

Cycle storage should comply with Cycling by Design by Transport Scotland which sets standards for cycle parking

Parking ratios



70% parking



area within easy walking distance to new Clydebank transport hub

Plot No	Parking ratio PER RESIDENTIAL UNIT	Cycle storage PER RESIDENTIAL UNIT
1	70%	2 spaces
2	70%	2 spaces
3	70%	2 spaces
4&5	70%	2 spaces
7	100%	1 space
8	100%	1 space
9	100%	1 space
10	100%	1 space
11	70%	2 spaces
12	70%	2 spaces



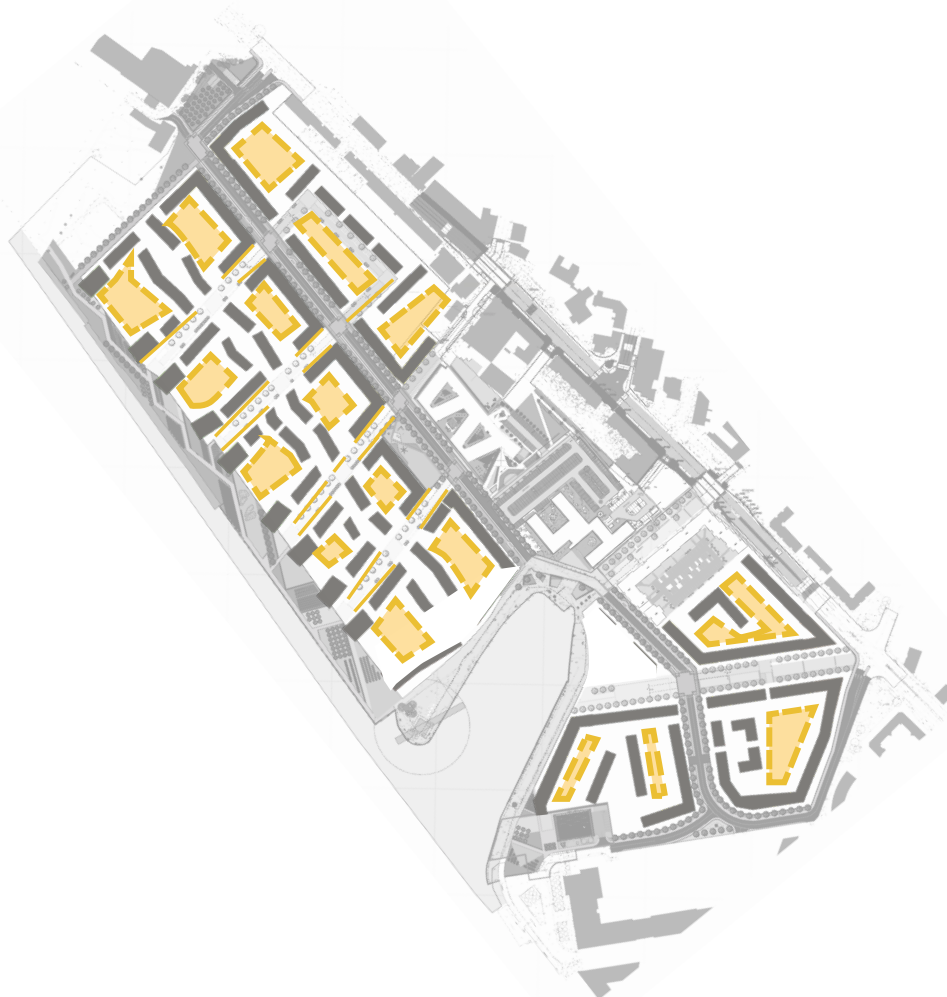
Parking character



parking court



on street parking



Waste and recycling

The preference is for a circular route for cleansing vehicles through rear courts; this will entail a coordinated approach between developers, which should be included as a condition for planning consents. This approach will allow bins to be wheeled to the rear of properties.

There should be no more than 7m of a 'pull distance' for the bins from the location of the bin stores to the vehicle. Each household will require 3no. 240 litre bins (grey/brown/blue) emptied on a fortnightly cycle.

At flats, this figure is translated into 1100litre bins on a ratio of 3/1 waste/recycling bins. For example, 13 flats would have 4 no. 1100 litre bins (3 waste, 1 recycling). These should be stored in a communal collection area.

Where properties are higher rise (for example 7 stories) an external bin store should be provided rather than an internal space. Where blocks are mixed use, this store should provide separate space for commercial uses.

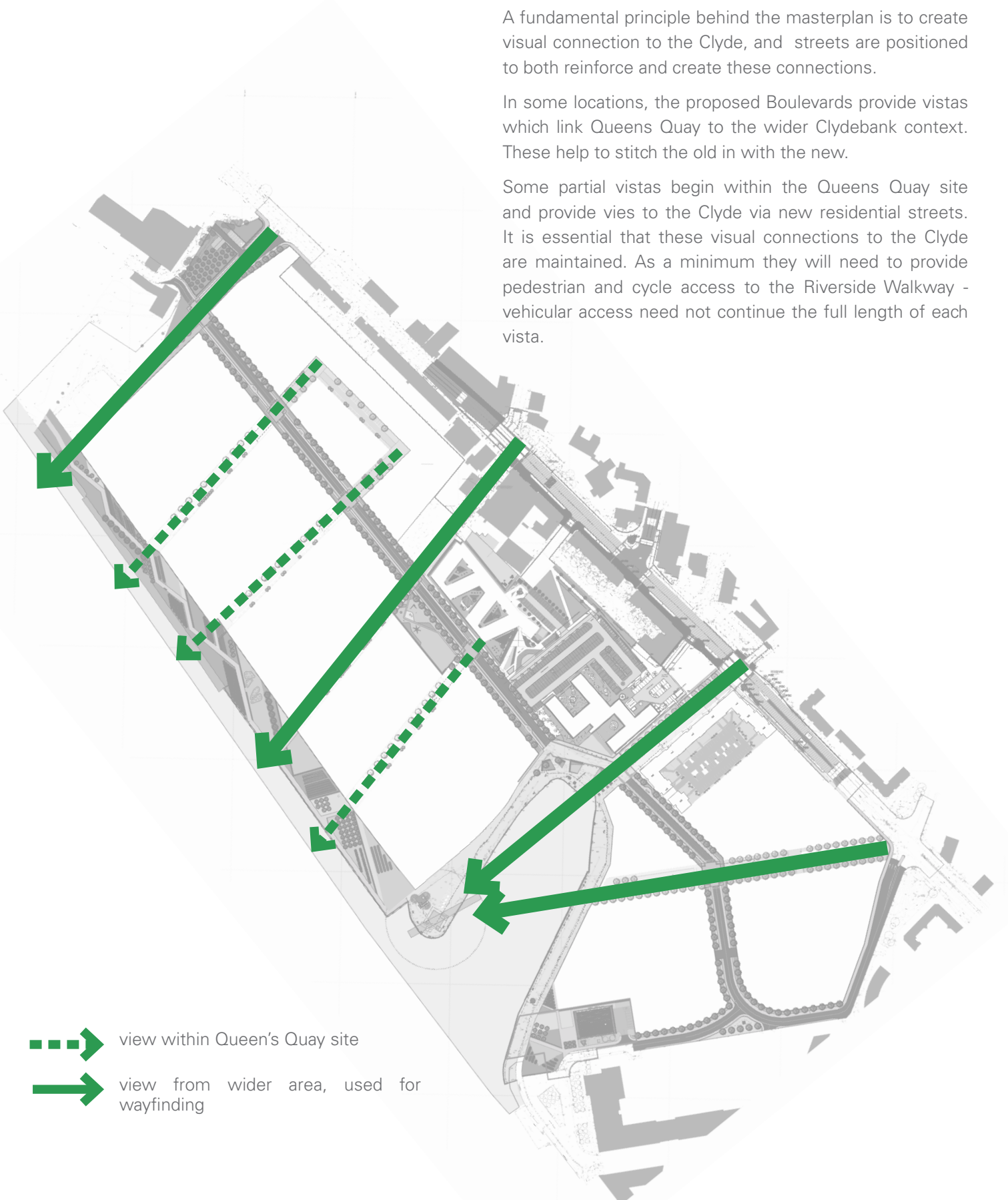
Cleansing department vehicle routes through parking courtyards

Views and vistas








A fundamental principle behind the masterplan is to create visual connection to the Clyde, and streets are positioned to both reinforce and create these connections.

In some locations, the proposed Boulevards provide vistas which link Queens Quay to the wider Clydebanks context. These help to stitch the old in with the new.

Some partial vistas begin within the Queens Quay site and provide views to the Clyde via new residential streets. It is essential that these visual connections to the Clyde are maintained. As a minimum they will need to provide pedestrian and cycle access to the Riverside Walkway - vehicular access need not continue the full length of each vista.

- 
- The map illustrates the layout of the Queens Quay site, showing various streets and building footprints. Green arrows indicate specific views and vistas. Dashed green arrows represent views within the site, while solid green arrows represent views from the wider area used for wayfinding. The arrows point towards the Clyde, reinforcing the visual connection mentioned in the text.
- view within Queen's Quay site
 - view from wider area, used for wayfinding

The site has been divided into 8 landscape character areas/ typologies. The landscape requirements for each character area are set out in the design codes; these include key information on relationships and layout, precedents, furniture, materials and planting.

-  **Streets - Urban:** a formal civic character relating to the scale of the wider street and town houses on the main route through Queens Quay
-  **Streets - Parkland:** residential areas that reflect the soft green landscape of the riverside park
-  **Nodes:** areas of enhanced public realm and landscaping
-  **Mews:** intimate, smaller scale housing along a pedestrian route running east west through residential blocks
-  **Parking Courts:** informal but functional back of house spaces: communal parking, servicing and bin storage areas
-  **Backcourts:** less dense parking courts incorporating communal parking, servicing and bin storage areas set within amenity and green space
-  **Park Interface area:** where new development links to and borders the expansive riverside park

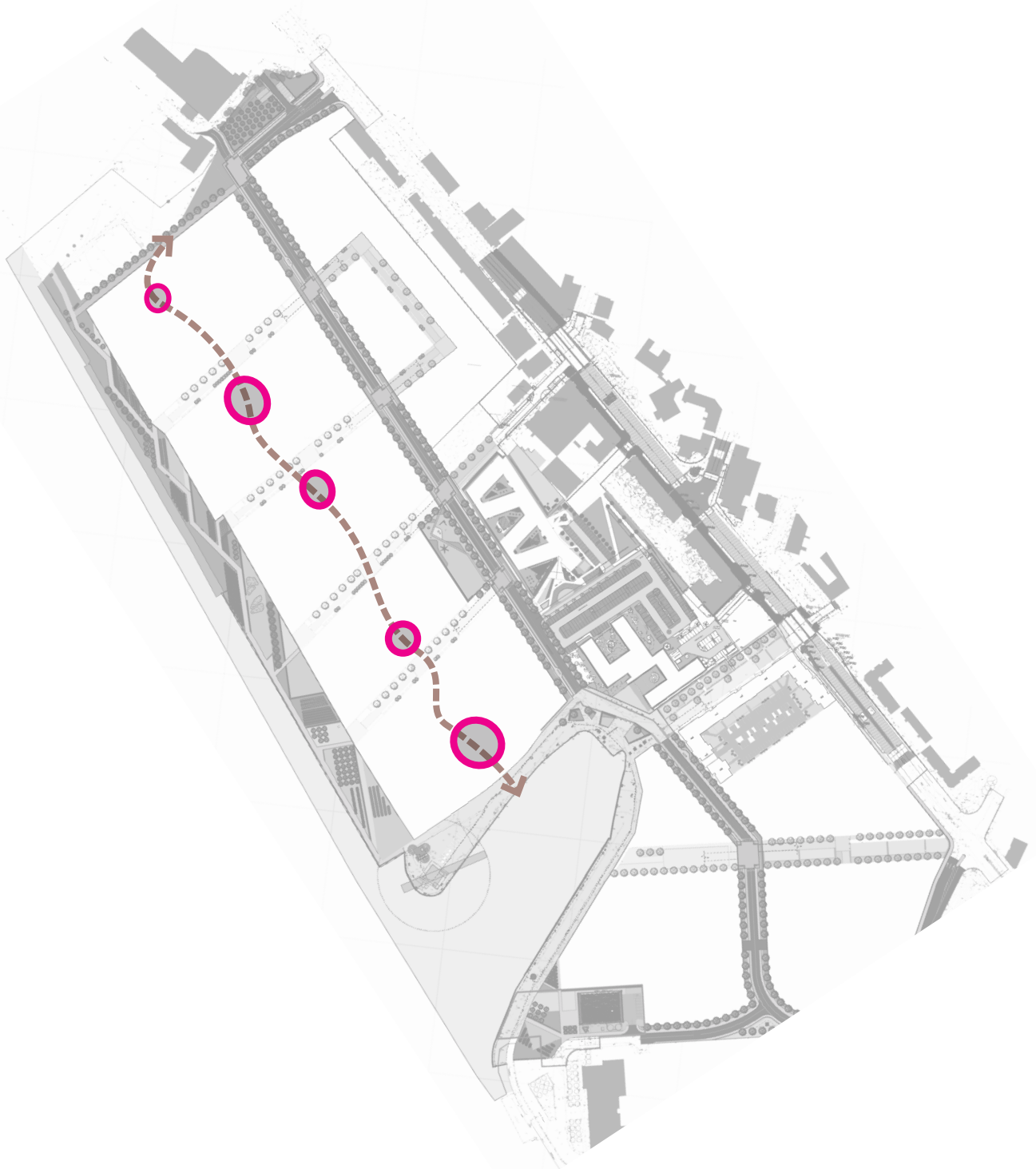


Hierarchy of Nodes

Along the route running through the centre of the riverside plots, it is proposed to create neighbourhood nodes. At these spaces the street should open up to create an area of public space that is sheltered from the prevailing winds and weather.

Each node should be activated by building frontage and soft landscaping. The pedestrian should be dominant in these spaces.

These nodes increase in significance, not in size, as they move towards the basin. Any proposed developments should take cognisance of this and demonstrate to the Planning Authority how each node relates to the other.



Aggregated Diagrams

This drawing collates and displays all of the previous diagrams. The attributes of each diagram come together to work as a whole, creating a mesh of urban design parameters for Queens Quay.

It is essential that these parameters are met by each development plot to ensure conceptual continuity through the Queens Quay area. This will then establish Queens Quay as a valued and desirable location to live, work and socialise.



Location of building typologies

- Houses
- Mews
- 3 storey flats
- 4 storey flats
- 4 storey flats (above commercial units)
- 6 storey flats
- 8 storey flats
- mixed use (landmark development)



Typologies	Building height	Frontage continuity	Privacy distances* from frontage to footpath	Commercial/retail use at ground floor
Development facing on to Fitting Out Basin	4 to 8 stories	Yes	Not required where there is a retail/commercial use at ground floor, otherwise minimum 1.5 m	Preferred
Boulevards with some commercial uses (Mixed use typologies)	4 or 6 stories	Yes	Not required where there is a retail/commercial use at ground floor, otherwise minimum 1.5 m	Preferred
Main Street	2 to 3 stories	Yes	Minimum 1.5 metres	No
River frontage	3 - 8 stories	No – pavilion type blocks to allow views to river	Minimum 1.5 metres	No
Residential streets	2-3 stories	Preferred	Minimum 1.5 metres	No
Mews houses facing onto residential pedestrian and cycle links	2 stories	Preferred	Minimum 1.5 metres	No

MATERIALITY

History + Community

The redevelopment of Queens Quay will contribute to the wider regeneration of Clydebank. As such it is important to be sensitive to the cultural significance of the sites history.

The Queens Quay site was formerly the John Brown Shipyards. Before that it was owned by the Thomson brothers. Prior to that it was agricultural land owned by the Cochno's and was miles from Glasgow's urban borders. The rapid development of Clydebank is directly related to the ship building activities. People moved to the area in droves for work in the yards. As such it is important that this part of the history of Clydebank is referenced in the new buildings of Queens Quay.

Likewise it is to be recognised that the shipyards are also a substantial source of ill health in the community. Legacy issues around exposure to dust and chemicals including asbestosis still prevail from the shipyards labour force, who still reside in the area. The decline of the shipyards is also linked to the social deprivation in the area.

As such the proposals should have a **nod to the past** but must take cognisance of the circumstances this caused, and so **look to the future**. Consequently literal interpretations or representations of industry are not felt to be appropriate. New designs should be aspirational and point towards a healthier Clydebank, with a new urban heart.



Historical Site Remnants

Based on a 1918 Map of John Brown Shipyards the original workshops have been shown dashed on the adjacent diagram. Entrance Gates to the shipyard sat at the ends of Hall st and Wallace street. What remains today of historical significance includes the Titan Crane, the quay wall, the Town Hall, Library and the Baths. While the culturally important buildings are sandstone, the majority of buildings in the shipyard were brick. As such it is felt that a predominantly brick character is important for Queens Quay.

Beyond brick, it is clear that metal is of core significance to the site. Primarily steel, as this was the main product from the foundry. This was used to form the ships hulls, mechanics and structures. The shipyard and possibly even Clydebank, would not have existed without the foundry.

Secondary in importance to steel are copper, zinc (galvanising) and brass.

This historical and contextual analysis provides the basis for a robust material selection for the site. These materials are in wide use today, however when applied as a material palette they contain an embedded link to the social history of Clydebank.

The future designers of Queens Quay must take care to apply these materials sensitively, so that the buildings and streets exhibit a nod to the past but demonstrate a clear view on an aspirational future.



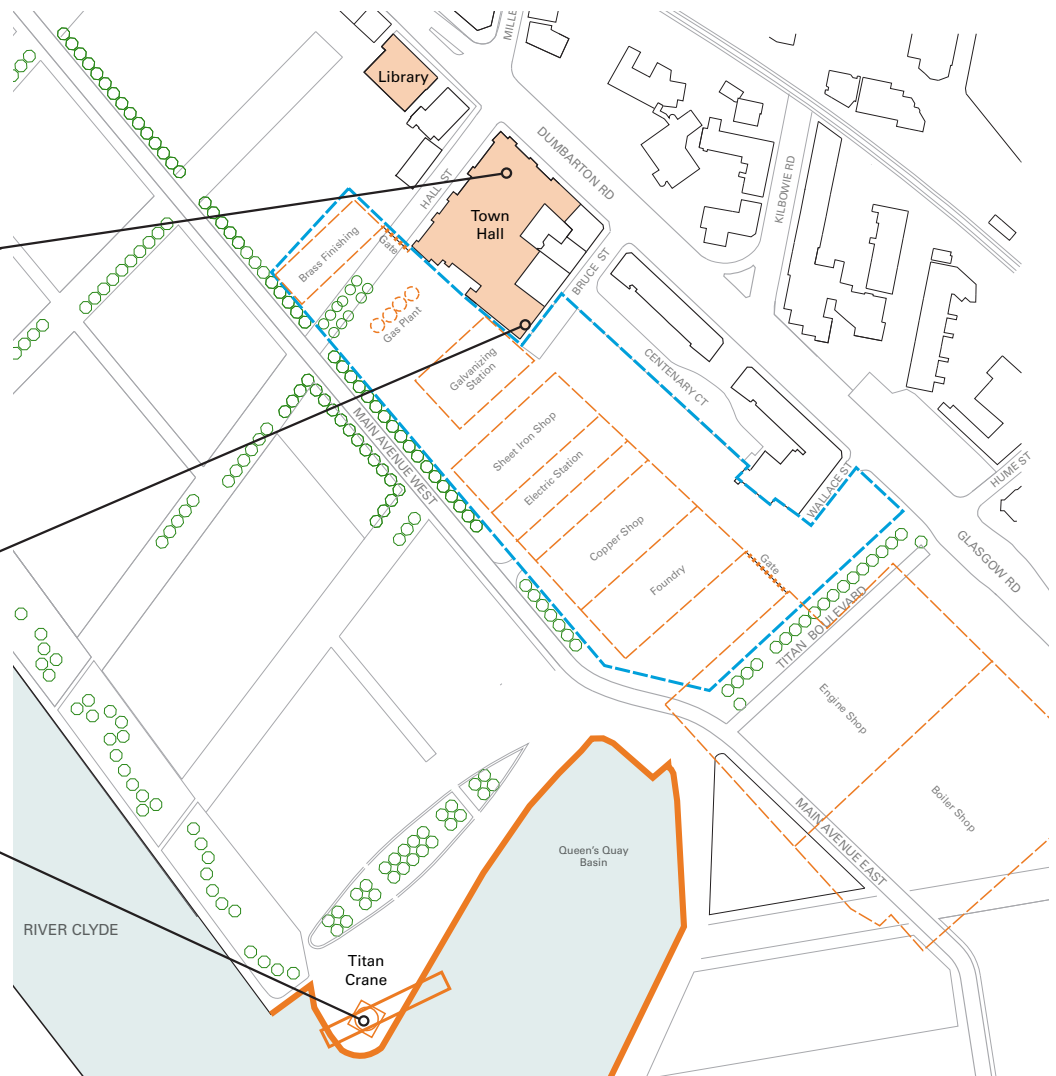
Clydebank Town Hall



Facade of Old Baths



Titan Crane and
Queens Quay Basin



Robustness

To ensure the longevity of Queens Quay, a detailed analysis was carried out to identify materials used in the locality and how they had performed over time. It is known that the weather conditions on the banks of the Clyde differ to inner city locations, through prevailing winds and a greater exposure.

Following the analysis the materials used were categorised into two broad categories, successful materials and less successful materials.

The images below describe materials that are felt to be less successful in this climate.

Smooth panels, particularly in large format had not weathered well. Smooth tiling, brick or terracotta in a stack bond pattern do not weather well. Timber cladding weathers unevenly depending on prevailing winds. Laminated panels were noted to exhibit signs of delaminating.

As such designers should not propose this type of material or detailing at Queens Quay.

Less Successful



Robustness

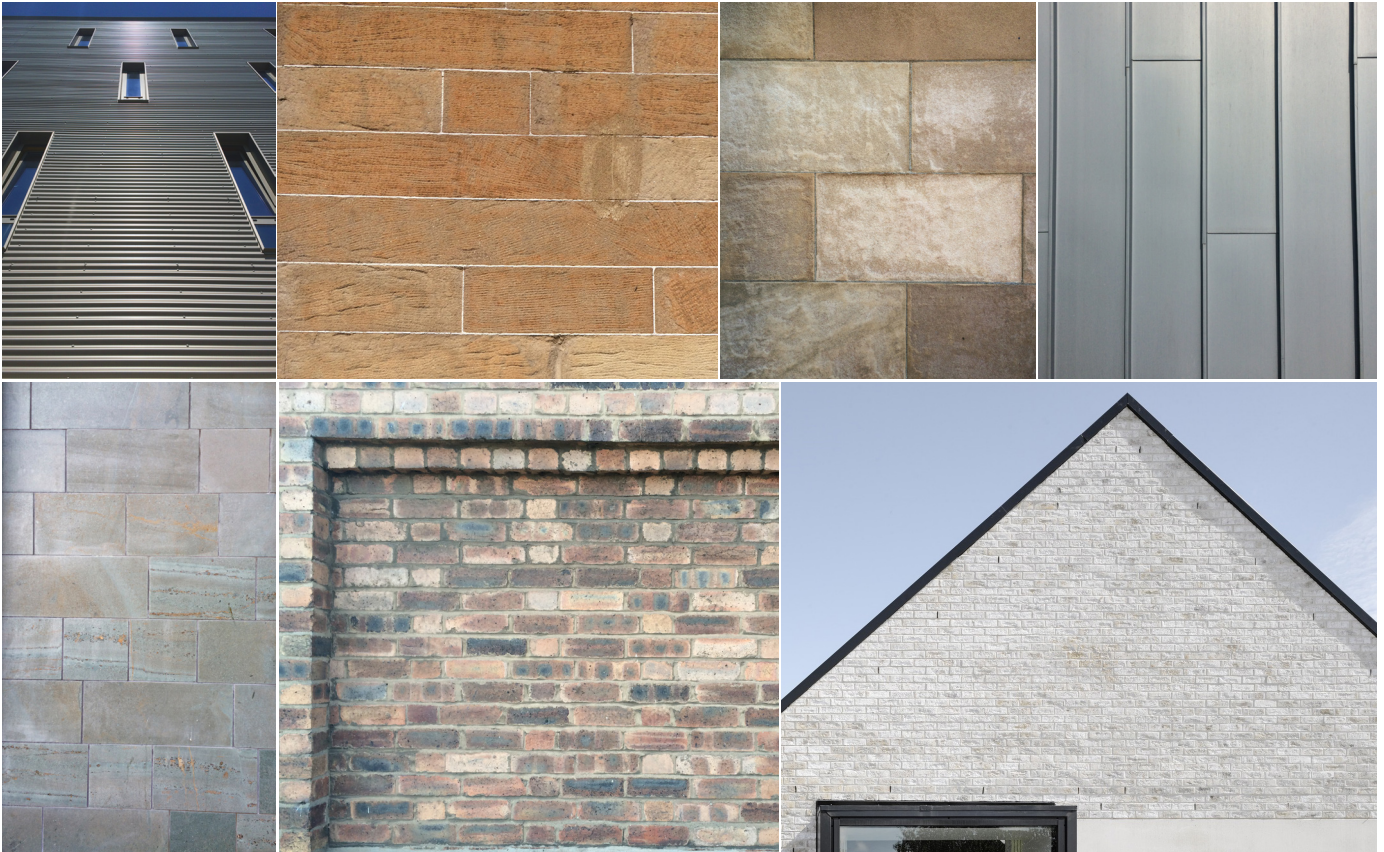
Materials that were successful exhibited a number of common traits. These should be utilised in the material selection and detailing at Queens Quay.

Surface texture is an important feature, traditional style brick laid in stretcher bond, bricks multi with a variety of colour and depth of tone, traditional stonework and ribbed metal, or standing seam panels all aged well in Clydebank.

Cropped eaves lines are also important to achieve a consistent weathering appearance across the facade. The successful examples were evenly exposed to the elements and so were able to weather naturally.



Material Successes



Materials Palette for Queens Quay

Simplicity

For a clean and consistent aesthetic across the site, no more than two cladding materials should be used on a building. Given the scale of the Queens Quay development some variety is encouraged within a pre-determined palette of materials. It is envisaged that the buildings may be articulated differently and may vary in colouration between sites, provided they draw from the material palette noted below.

Planned Weathering

The local climate at Queens Quay can be severe, with strong prevailing winds and driving rain. Detailing on all buildings must be robust to ensure that rainwater is cast off eaves, copes and flashings.

Consideration should be given to the intended long term aesthetic of materials. For example, oil canning (wrinkling of the otherwise flat panel caused by heat expansion) of standing seam metal cladding must be considered and shown to be part of a planned aesthetic for the building. Brick detailing, such as corbelling, can provide features or areas of interest, however consideration needs to be given to the possibility of staining where water runs off or is directed.

A Materials Palette for Queens Quay

For the reasons stated previously, a palette of materials is provided below. These are deemed to be favourable by the Planning Authority at Queens Quay. The proposed materials of any building submitted to the Planning Authority in relation to Queens Quay should demonstrate compliance with this Materials Palette.

The Materials

Preferred materials are listed on the following pages.

Built Form - Materials Palette

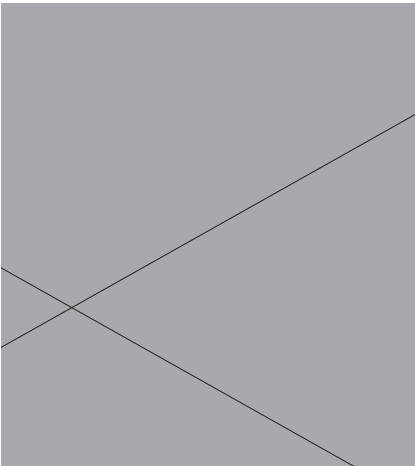
Cladding Material 01 - Brick

Traditional style clay stock brick, laid in stretcher bond, specified as a multi for depth of tone and variety of colouration. Colour matched mortar is preferred. Technical specification of the selected bricks should be appropriate to the local climate. Performance of moisture absorption, durability and active soluble salts must be considered.

Buff / Cream brick is favoured. All proposed bricks should tone with one on another on a single building. A contrasting Grey / Black brick could be considered to highlight key features. All bricks should tone with one another between sites.



Preferred
Buff / Cream
Traditional
Multi Clay
Stock Brick



Preferred
Buff / Cream
Traditional
Multi Clay
Stock Brick

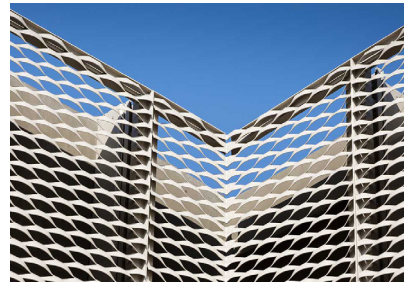


Not Preferred
Red / Brown
/ Orange /
Yellow / Blue
Bricks
Wirecut and
or Extruded

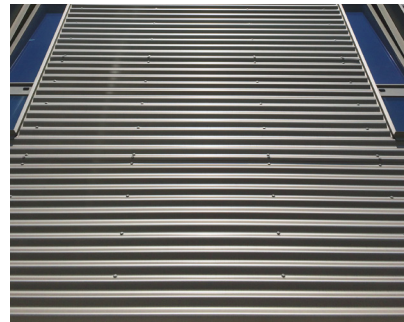
Cladding Material 02 - Metal

Steel - Corten is favoured due to its suitability to the environmental conditions. Care should be exercised to ensure that the Corten is pre-weathered off site, or detailed to weather in-situ without staining adjacent surfaces.

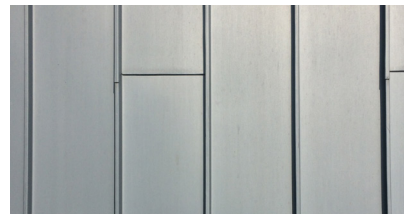
Copper, zinc and brass can be used however they should be considered in the context of surrounding buildings and streetscape. Smooth jointing is not to be used, standing seam or ribbed panels are preferred.



Preferred
Expanded Metal
Mesh
Anodised
Aluminium or a
Metallic Finish



Preferred
Profiled Anodised
Metal Cladding
Anodised Aluminium
or a Metallic Finish
Powder Coat.



Preferred
Profiled Anodised
Metal Cladding - Zinc,
Copper or Brass.

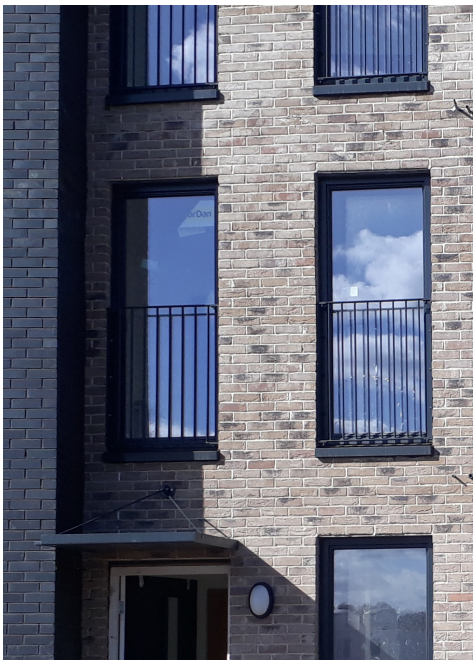


Not Preferred
Flat Smooth
Preformed Panels

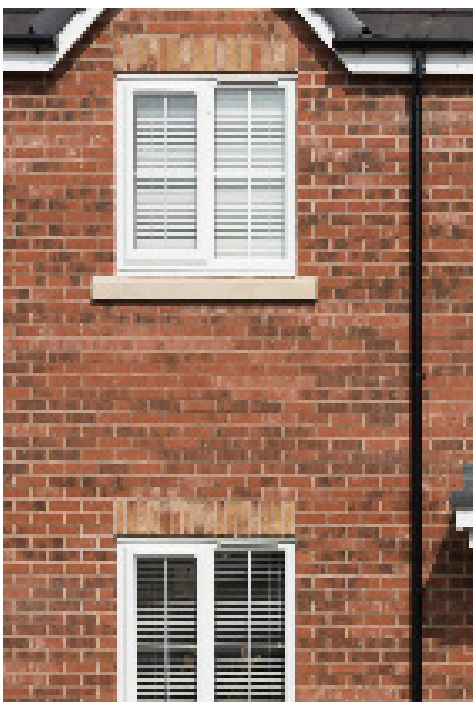
Built Form Materials Palette

Windows

ll windows should be provided in a dark grey finish, preferably polyester powder coated aluminium externally. Cills and balustrades should complement the window, window colour, cladding and aperture.



Preferred
Dark Grey PPC
Aluminium
Windows
Could be
aluminium clad
timber or all
metal window
frames.
Colour
galvanised
balustrades to
match window.
Colour matched
precast or
aluminium cills.



Not Preferred
White PVC
Windows

Roofs

The roofs on all buildings should be specified to be in keeping with the material palette noted previously. Where roofs are tiled consideration should be given to the eaves and verge details.

Verge Details - The clean lines of a polyester powder coated aluminium flashings are preferred to proprietary dry verge systems. A skew wall detail, a parapet detail, or clipped eaves with a fibre cement slate roof finish could achieve this.

Eaves Detail - Raked box eaves are preferred to traditional boxed eaves details. Clipped eaves are preferred to projecting eaves, unless a clear case is made for the aesthetic approach.



Preferred
Skew Wall



Preferred
Parapet



Not Preferred
Traditional
Box Eaves



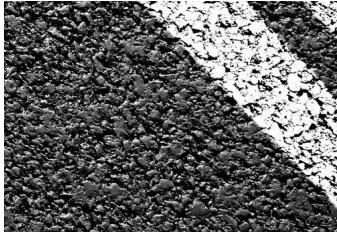
Not Preferred
Proprietary
Dry Verge

Streets - Urban landscape and streetscape materials

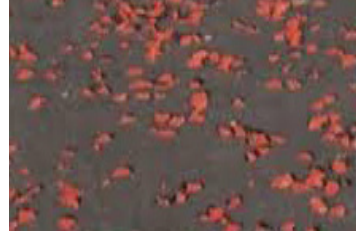
Furniture & Materiality

Hard surfaces in the Urban character area should be of same quality and finish as the materials delivered on the Main Avenue as part of the Queens Quay masterplan.

Road carriageways and pavements - hot rolled asphalt finish (no aggregate chippings preferred)

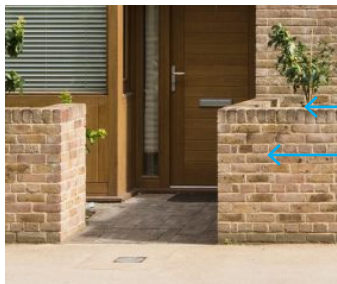


Preferred
hot rolled
asphalt finish



Not preferred
coloured
chippings

Boundary treatments - 1,200mm high brick walls to match buildings; 'brick on end' copes (noting requirement for specials/ resin bonded bricks with overhangs for protection)



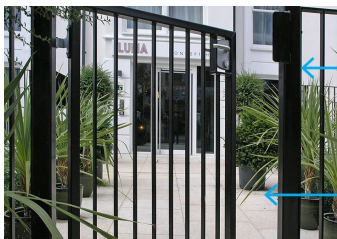
Preferred

- brick copes
- brick type & detailing to match building



Not preferred
railings or
fences that
create visually
permeable
boundary
treatments

Gates to entrances - 1,200mm high steel gates - powder coated steel gates in dark grey to match building windows and metalwork; vertical bar style, no adornment



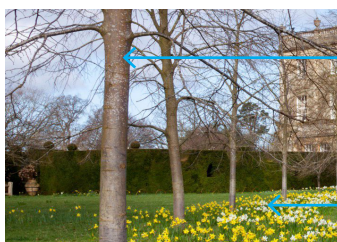
Preferred

- colour to match metal work on facades(grey);
- vertical flat bar/ simple design



Not preferred
no adornment

Planting; Street trees - Lime (Tilia) Avenue tree planting to match the boulevard - (Tillia cordata 'Greenspire' preferred - trees to be planted in tree pits with load bearing tree soil, at max 12m



Preferred

- avenue trees (max 12m spacings)
- emergent bulbs in grass verges

Streets - Parkland landscape and streetscape materials

Furniture & Materiality

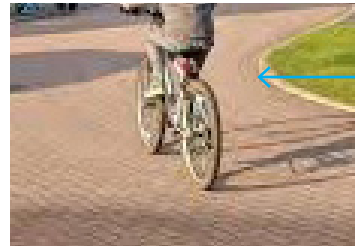
The south end of the residential streets (beyond the node spaces) relate most strongly to the waterside park should be much greener and open in character - with a focus on the pedestrian and public spaces instead of the vehicle servicing and access.

Road carriageways and pavements - vehicular carriageways should be in bitmac with parking bays and pavements in 'textured' precast concrete setts in silver-grey tones - creating safe spaces for pedestrians and a softer feel. If appropriate to the drainage strategy these could also be permeable areas.



Preferred

parking bays and pedestrian areas in textured setts
different textures define routes and spaces



Not preferred

single surface type for all road / pavement / parking areas
no definition of routes and spaces

Boundaries must be defined - but done so with hedges and planting (minimum 900mm high) - not by wall or fences. Boundaries should not be left undefined or left open



Preferred

boundaries defined by hedges and planting



Not preferred

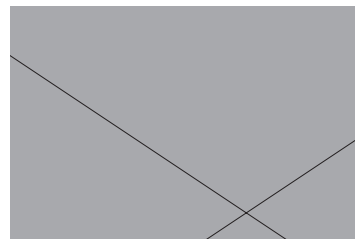
open boundaries to front gardens
large areas of lawn

The general feel should be soft and leafy as if the housing is in the park



Preferred

trees planted in groups with shrubs/ground cover below
visitor parking located within streetscape in blocks of 3 spaces



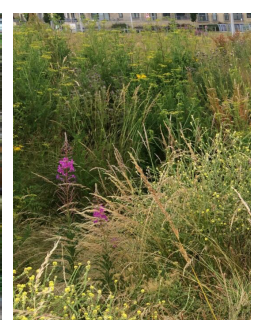
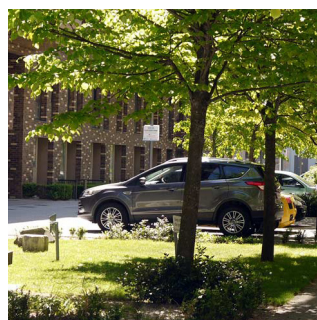
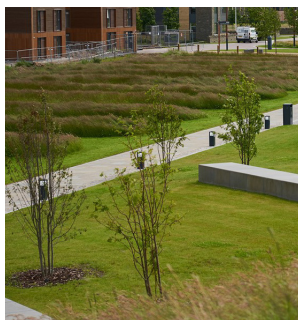
Not preferred

incutillage parking spaces

Planting

The planting to the Parkland character area should include:

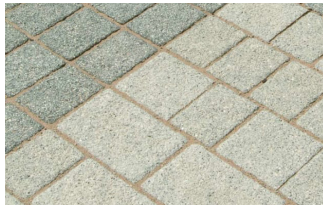
- blocks of native shrubs and grasses taking inspiration from the planting palette and overall structure as the park
- informal clusters of native trees (groups of 3-7)
- front gardens to include grass and a native tree in each front garden
- a native mixed species hedge (or beech) to the boundary (coastal hardy varieties)



Mews - landscape and streetscape materials

Furniture & Materiality The furniture and materiality to the pedestrian 'mews' must reflect a more tactile and human scale environment, with playful elements and textures used to define public/semi public/private zones.

Pedestrian environment - The mews should be detailed in small format 'textured', exposed aggregate, pre-cast concrete setts in silver-grey tones.



Preferred

'textured' setts in silver grey



Not preferred

red mono block laid herringbone

Boundary treatments - there should be no boundary to front gardens - instead buffer planting strips created with opportunities for seating / personalisation (minimum 2m wide)



Preferred

planting forms buffer to windows & area to personalise seating opportunities
no boundary



Not preferred

too narrow to buffer windows or provide opportunities for personalisation

Furniture within the social spaces along the mews should be made from timber and precast concrete and integrate with the design of the greenspace.



Preferred

robust materials

integrated with space design/landform



Not preferred

generic design

free standing bench - not integrated with planting

Incidental spaces for play (not play areas) should be incorporated within the street greenspace



Preferred

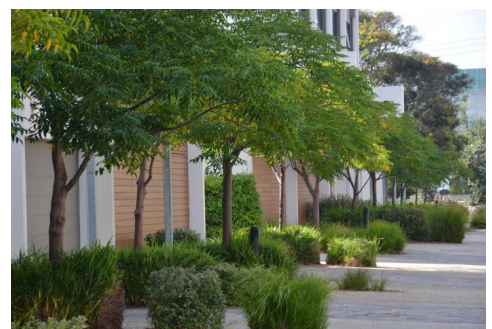
objects for play integrated into the streetscape



Not preferred

standard catalogue play equipment, rubber play surfacing, fenced off areas

Preferred planting - Clusters/ groups of small native street trees - to hold in spaces and edges. Appropriate species would be Rowan, Birch or Cherry.



Nodes - landscape and streetscape materials

Nodes should take on the furniture and materials of the area within which they are located. The general feel should be civic and like a small plaza or pocket park rather than a street junction. Feature areas (at building thresholds, around seats, under trees, furniture and greenspaces) should be surfaced in a smooth, porous alternative material such as a resin bound gravel.



Preferred

feature seating areas and routes defined by greenspace and planting



Not preferred

wide open spaces

seating not located in spaces



Preferred

brick boundary walls to use same brick type as adjacent buildings to enclose back gardens on corners (walls to be 1800mm height)



Not preferred

timber fences as boundaries on residential streets



Preferred

seating arranged around edge of space
greenspace and trees



Not preferred

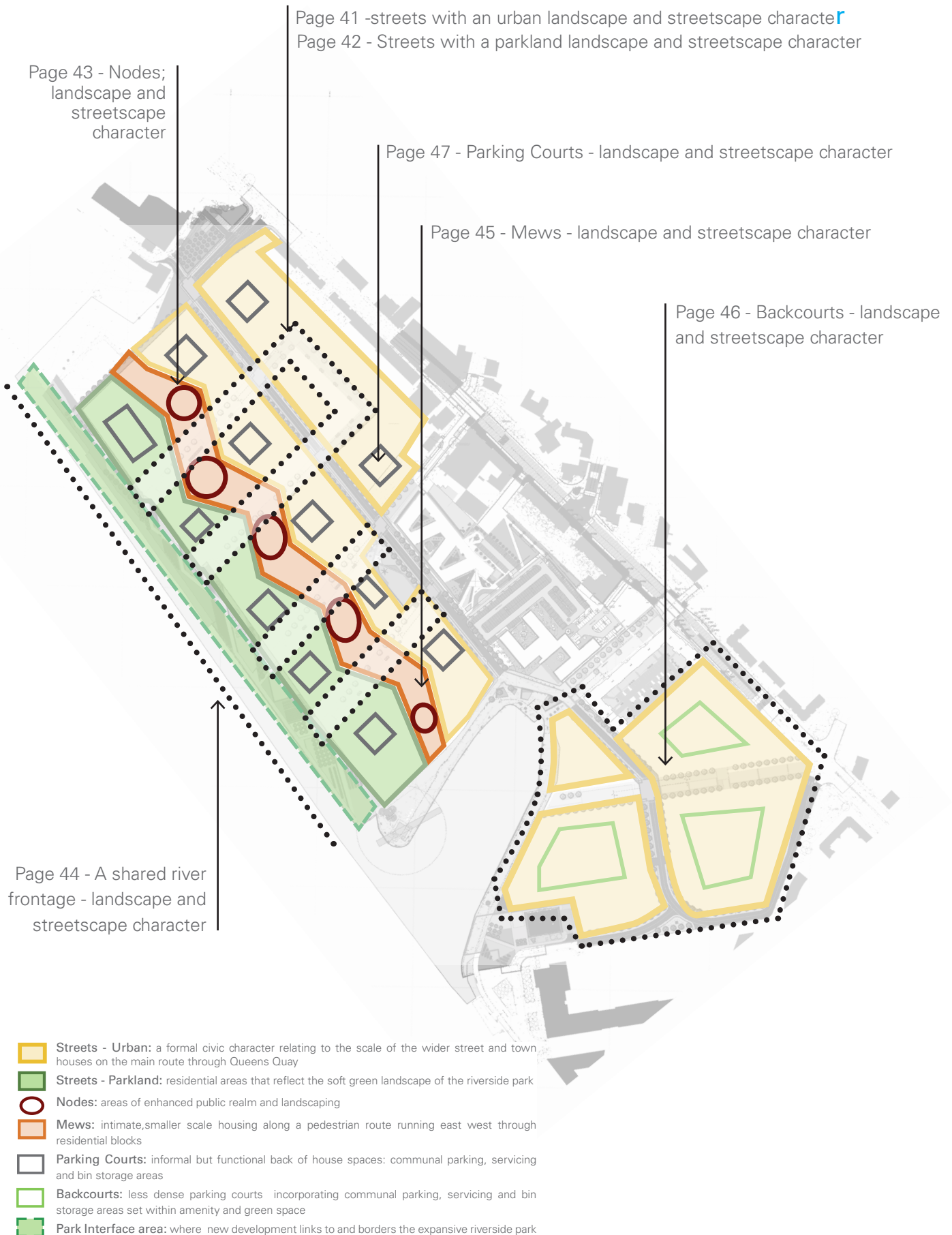
standard road junction design

no usable greenspace or trees

CHARACTER

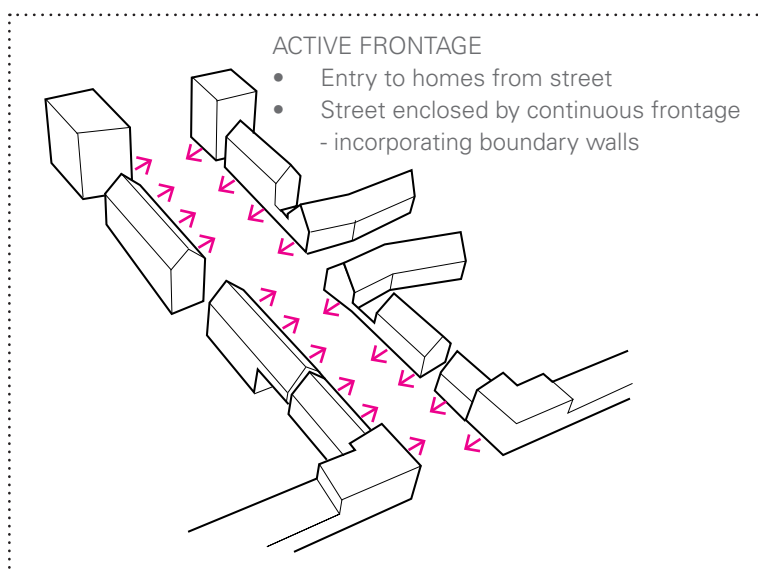
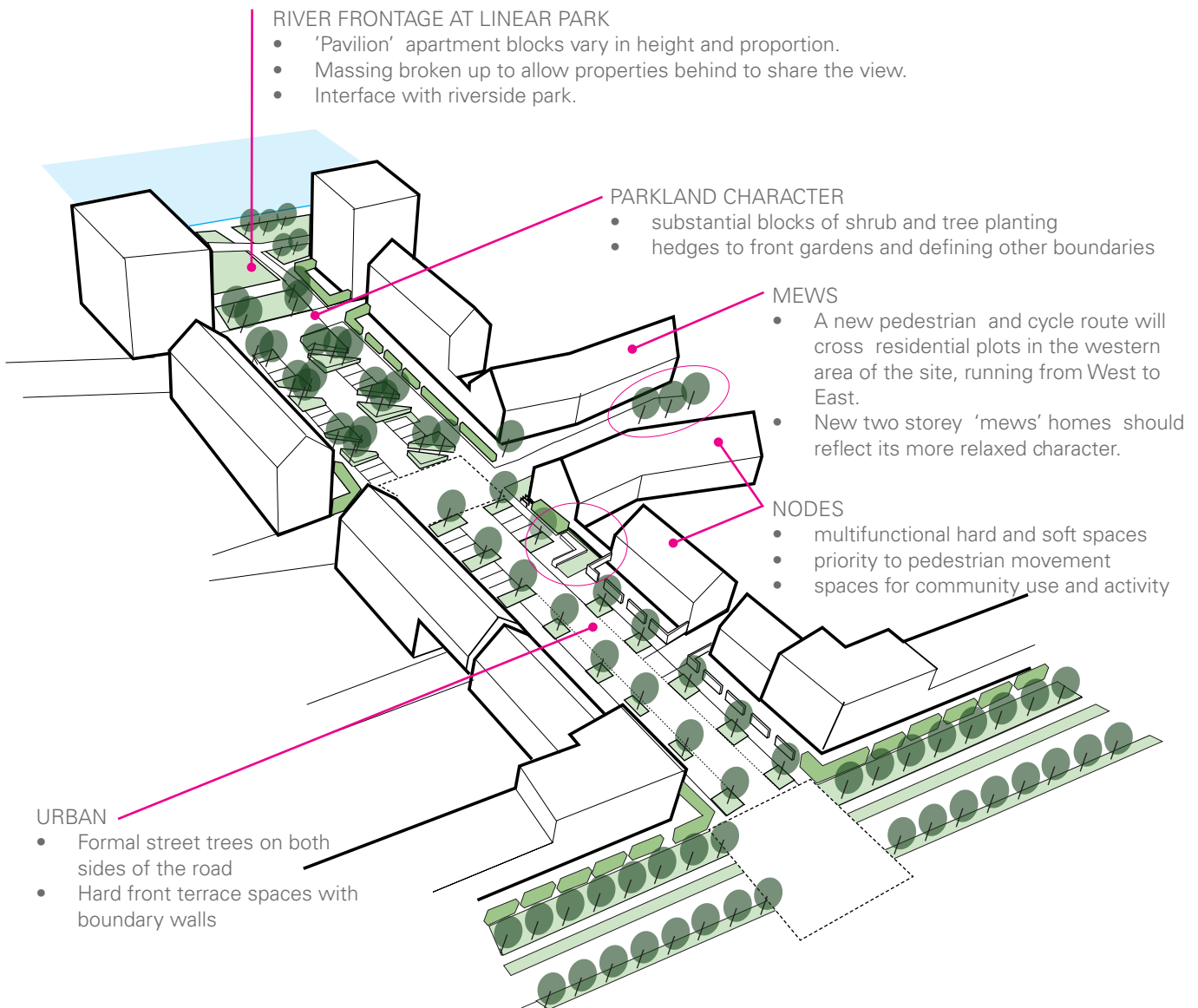
Character Zone Overview

The site has been divided into 8 landscape character areas/ typologies. The landscape requirements for each character area are set out in the design codes; the following pages illustrate these in more detail and explain the interfaces between the character areas as shown below



General development principles - overview

The diagram below shows a typical street where the landscape and streetscape treatment will vary from a more urban approach beside Main Street to a parkland character beside the river. Refer to the diagram on Page 22 for the locations of each landscape/streetscape character type

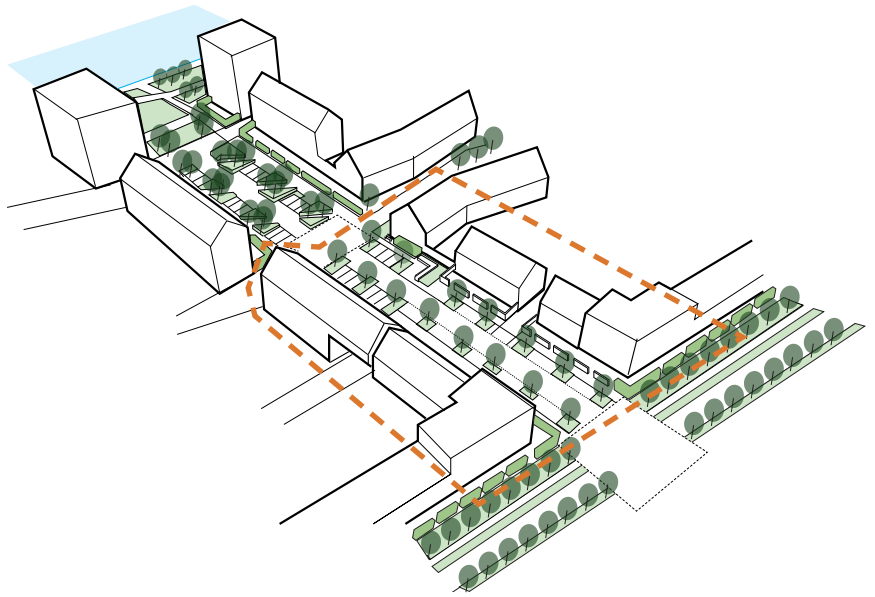


Streets with an urban landscape and streetscape character

Refer to diagram on page 22 for the location of streets with urban landscape character.

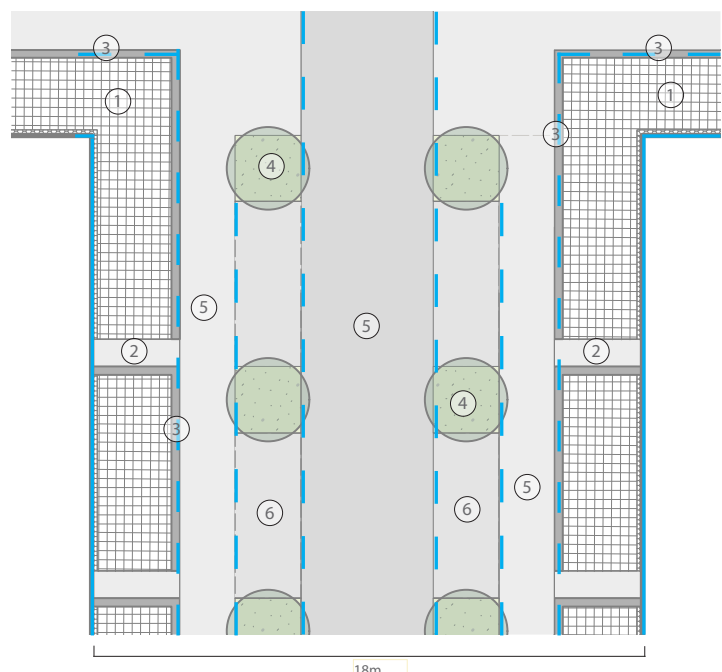
These areas have a formal civic character which relates to the scale of the wider street, flats and town-houses on the new Main Avenue and Dumbarton Road. These urban areas have formal planting and hard landscape materials.

Detailing should be crisp and rigorously geometric. Materials reflect the palette for Main Avenue within the existing masterplan, and buildings and external features should use the same materials eg. brick boundary walls and metalwork.



Typical street plan & cross section

3.1m 2m 2.5m 4.8m 2.5m 2m 3.1m
to tie in with Main Avenue

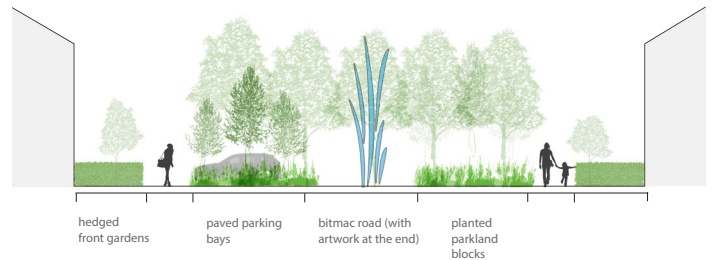
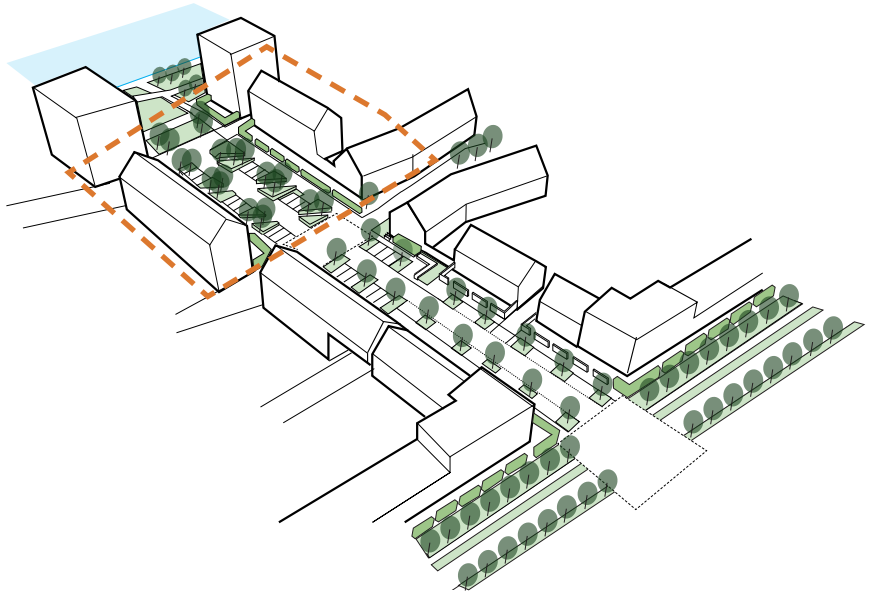


- ① Hard surfaced front courts with clearly defined thresholds
- ② Front entrances to all dwellings direct from the street
- ③ Brick boundary wall with brick cope and steel gates (1,200mm high)
- ④ Formal street trees (semi mature size) in large tree pits with grass and emergent bulbs beneath (max 12m centres)
- ⑤ Bitmac carriageway, parking bays and footways
- ⑥ Parallel visitor parking on both sides of road (max 2 spaces in a row before break)

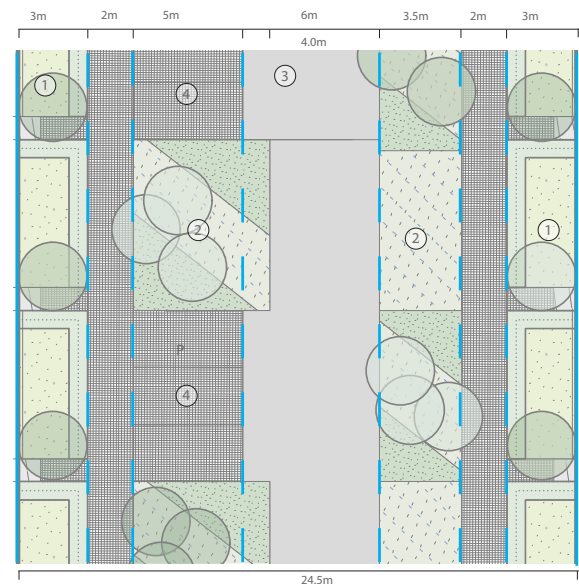
Streets with a parkland landscape and streetscape character

Refer to diagram on page 22 for the location of streets with parkland landscape character.

In this area, houses and blocks of flats feel as if they are set in the riverside park. The character is green and leafy with a palette of native plants and an informal feel. Visual and physical links to the park and the river are retained. Planting should have a native coastal palette, paving is less formal and nose-in visitor parking clusters are broken up by swathes of shrubs and grasses that reflect the park character. Careful attention should be given to creating an appropriate micro climate given the exposed nature of the site.



Typical street plan & cross section

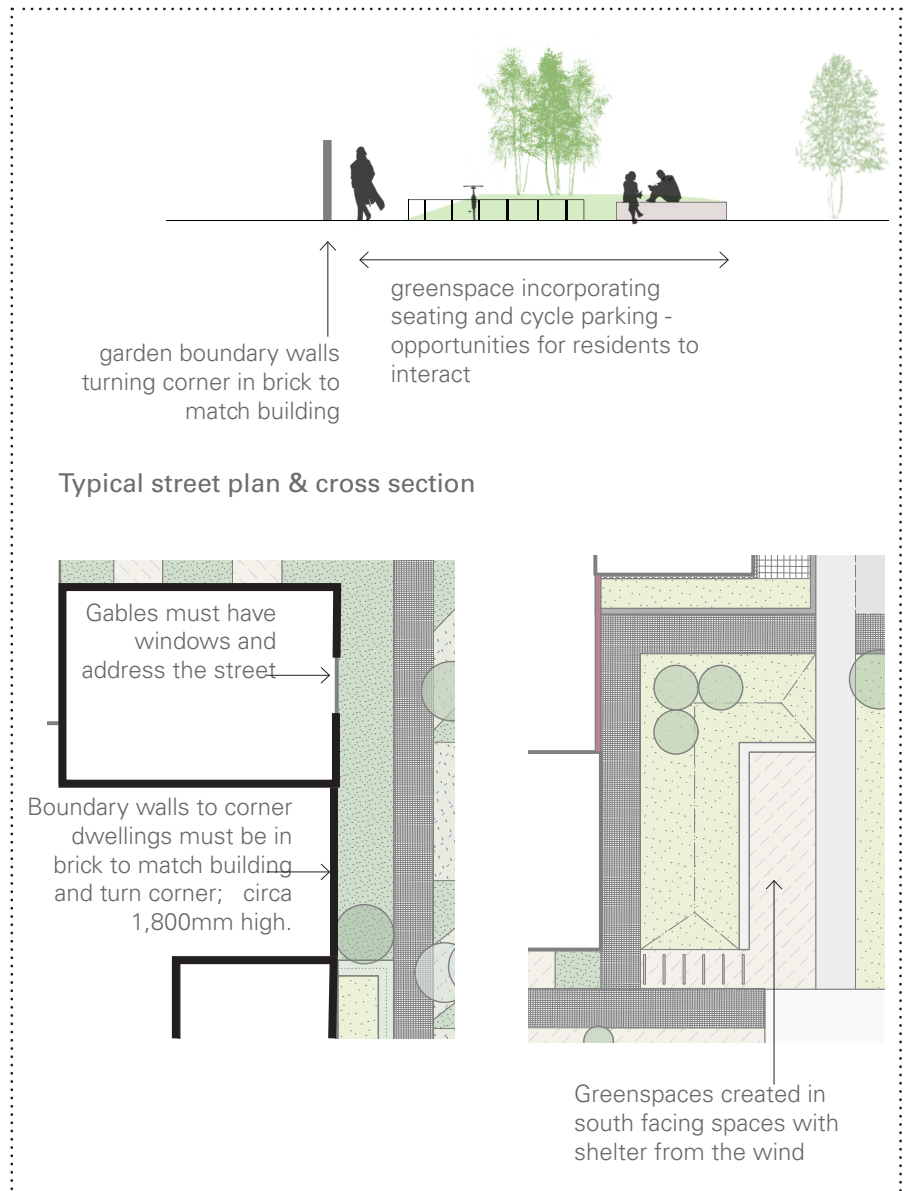
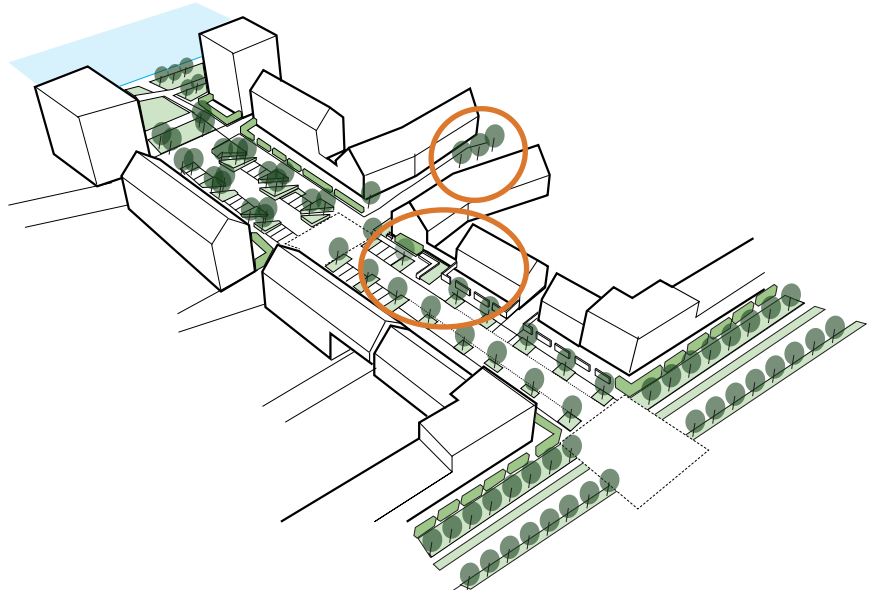


- ① Front gardens defined by a hedge and entrances marked with a tree
- ② Large scale parkland planted blocks with native shrubs and trees to extend park into housing areas
- ③ Bitmac carriageway, small unit PC paving for parking and paths between planting
- ④ Visitor parking provided as nose in spaces in blocks of 3 between planting

Nodes - landscape and streetscape character

Nodes are intended as important hubs for orientation and for social activities. In these locations, appropriate flexible space should be created for small gatherings or street-scale community activities.

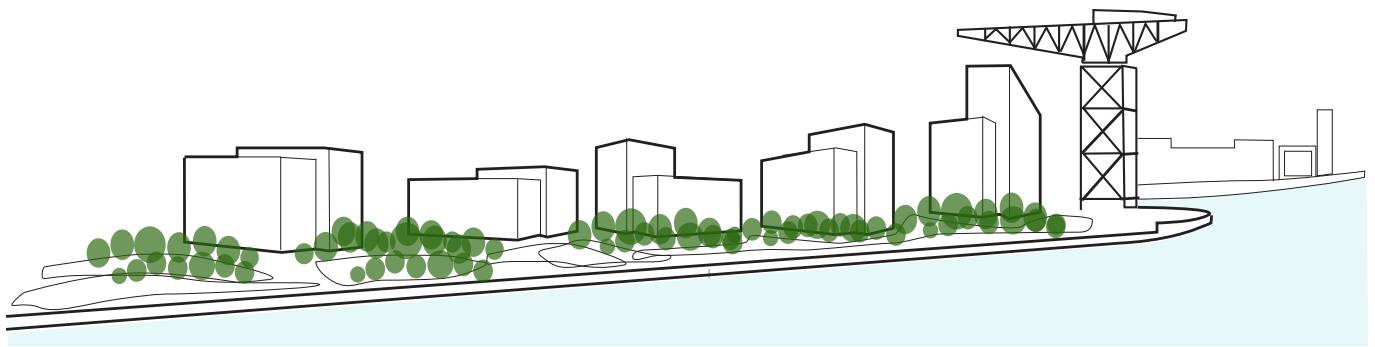
Some nodes are located within the pedestrian walkway running through mews housing. Other nodes are at residential streets and will give pedestrians priority through a large raised table. This space will allow vehicle access on a 4.8m wide carriageway, bounded by seating areas, trees, bike and visitor parking.



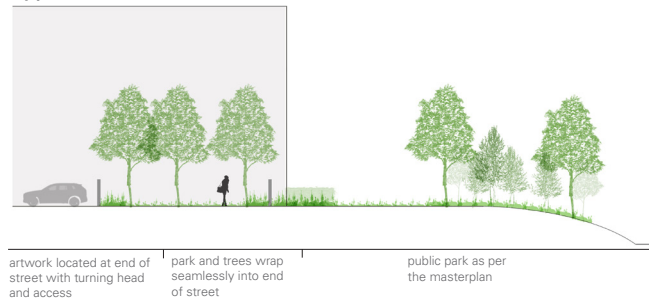
A shared river frontage - landscape and streetscape character

Refer to diagram on page 22 for the location of flats with a shared river frontage. Flats at the riverfront will make the most of views to the river for as many residents as possible. This is achieved by breaking up the massing of flats beside the river to allow properties behind to share the view. These 'pavilion' apartment blocks should vary in height and proportion.

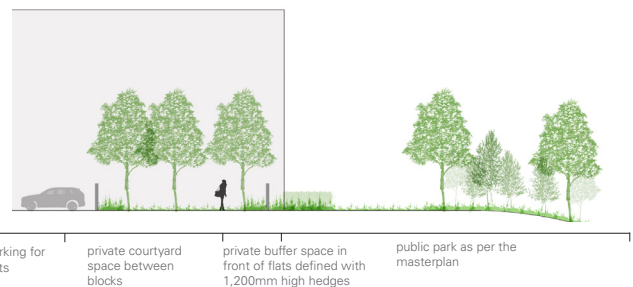
Each development plot will have an edge that seamlessly interfaces with the proposed riverside park. The apartment blocks here will sit 'in the park', with a greater extent of greenspace extending up into each street and wrapping around the buildings. A central private space is created between the higher buildings which retains views through the block. Planting in this area will be the same as the riverside park. Feature artwork could be located at the end of each residential street - connecting the two spaces.



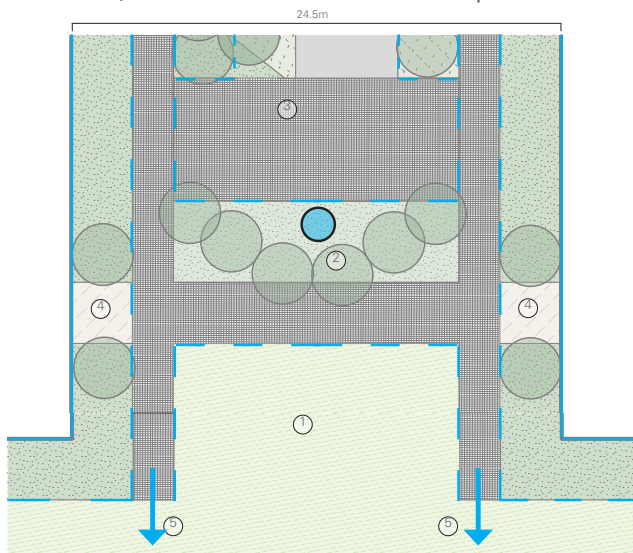
Typical cross section - end of residential street



Typical cross section - through development block



Plan; residential street meets linear park



- ① Parkland greenspace extends into street
- ② Feature artwork at the end of each street. Parkland sweeps up to the road end.
- ③ Turning head / drop off area designed in to street and extent of hard surfaces screened with planting (no parking)
- ④ Entrances to flats direct to street and across parkland
- ⑤ Direct connections into the park



greenspace at linear park beside river



busy public realm similar to Fitting Out Basin, spilling out from commercial uses at ground floor of adjacent mixed use development

Mews - landscape and streetscape character

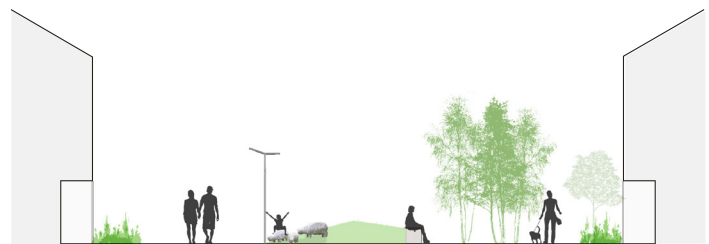
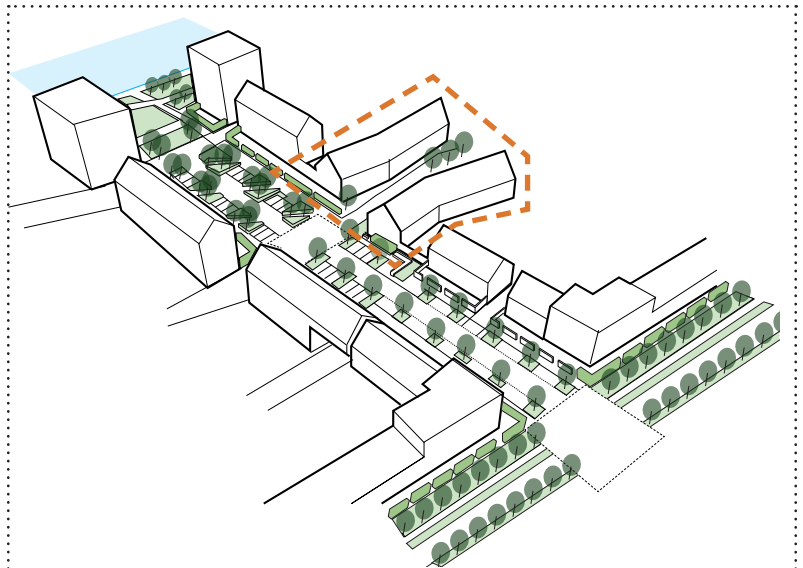
Refer to diagram on page 22 for the location of mews.

A new pedestrian and cycle route will cross residential plots in the western area of the site, running from West to East. (although service and emergency vehicles must be able to access). New two storey 'mews' homes should reflect its more relaxed character.

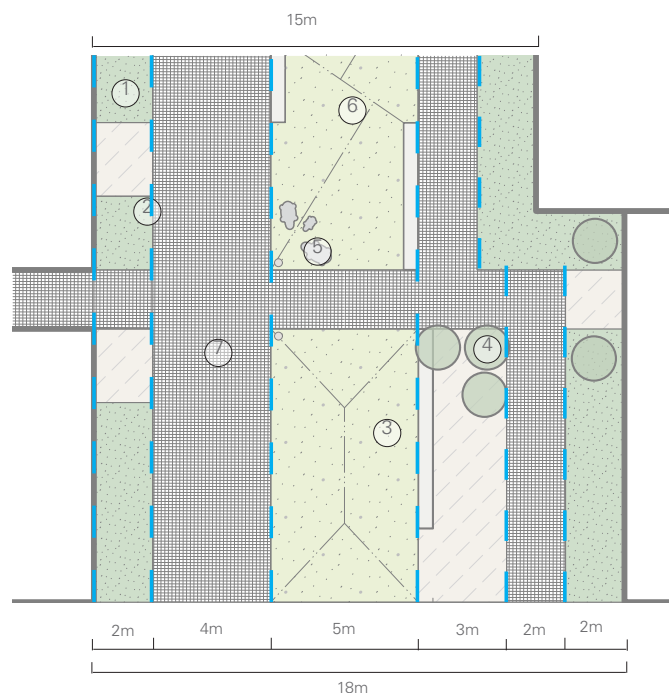
This should function as a social 'connector' - a fluid and seamless 'lane' that runs east to west through development plots.

The Mews is smaller in scale (15 metre to 18 metre in width) and has a less formal character, with groups of trees defining convivial spaces for seats and bike racks. Small scale pedestrian paths cut off the lane to create convenient links to car parks.

Playful artworks and greenspace replace the traditional road and create incidental spaces for children and a unique sense of place.



buffer planted zone pedestrian route / emergency access route grassy mounds / incidental art & play domestic scale trees buffer planted zone



- ① Buffer planting - creates privacy and defines building thresholds
- ② No boundary walls/ treatments to plot edges
- ③ Greenspaces running through the street - widening with steps in the building to create space for activities
- ④ Small domestic scale trees holding in spaces and corners
- ⑤ Incidental features/ sculptures for play
- ⑥ Landform within the greenspaces to create variety and interest and integral seating
- ⑦ Occasional / emergency vehicular access route - kept to the south (shadier) side of the street

Backcourts - landscape and streetscape character

Character Area Overview

A number of the larger development plots (1, 2, 4 & 5 and 7) will have central back court spaces that are not suited for built development. These spaces provide an excellent opportunity to enhance the development in a number of ways. These spaces could create a more dispersed parking solution, provide community space for activities such as food growing, create habitat and ecological benefit such as woodland or community greenspace, be used for rain gardens and rainwater harvesting, urban



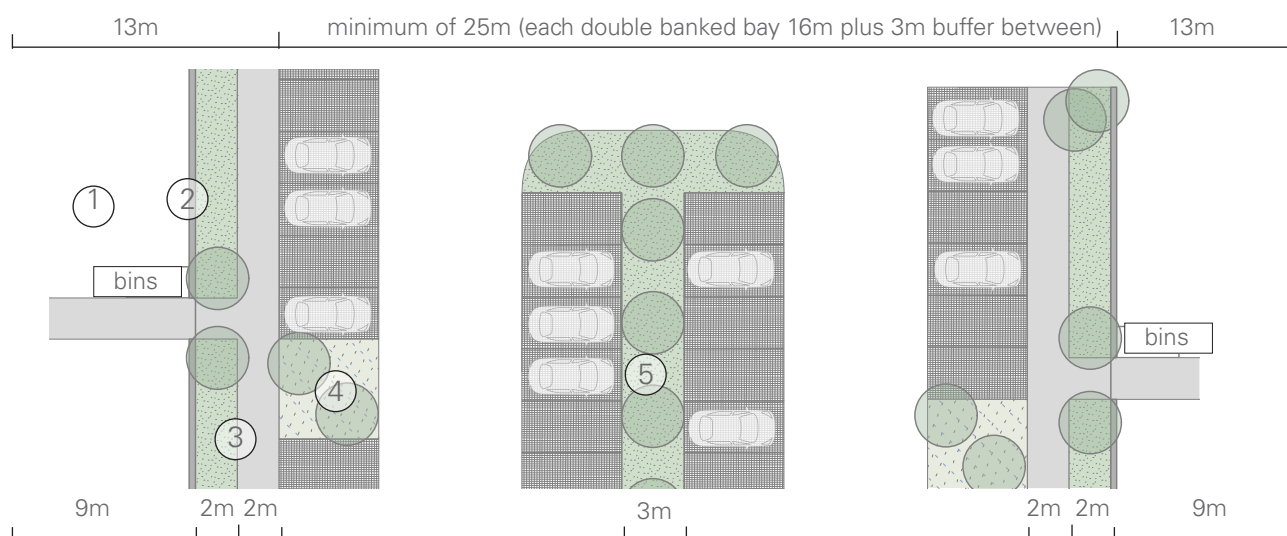
- community growing space set in community greenspace
- dispersed low density parking integrated within greenspace

Parking Courts - landscape and streetscape character

Parking courts are functional spaces which will provide residents parking, bin storage and access .

Each should have a distinctive character and a strong sense of enclosure – either through built form, tree planting or appropriate boundary treatments. Courts should be designed to accommodate wheelie bins and recycling storage so that they do not dominate views. They should provide easy and direct access to dwellings. Parking areas should not reduce useable rear garden areas and sufficient space must be given to green buffers around the edges of these spaces, and provision of trees both to the edge and at the centre of the space. To ensure the character of these areas is appropriate and does not deteriorate over time the dimensions set out below are the minimum for buffer and planted spaces.

Typical parking court plan



- ① back gardens / communal areas minimum 9m deep with bin storage at boundary allowing access from gardens and parking court
- ② boundaries treatments should be solid and not permeable (timber or brick), maximum 1,200mm high
- ③ a planted buffer including trees should be around edge
- ④ max 6 parking spaces around edge of space before broken up with a planted block - including multistem trees
- ⑤ central buffer with avenue trees between bays - minimum 2m width with trees planted in linked tree pits

Typical parking court section



multi-stem trees with blocks of robust native shrubs below low rear boundary fences with bin stores neatly located within garden space



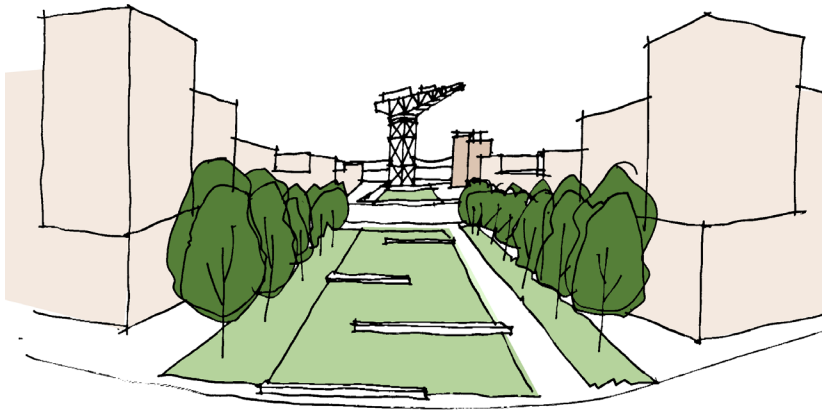
avenue street trees in centre of parking court - to bring down the scale of the space (max 6m spacing)



Masterplanned areas - landscape and streetscape character

The boulevards linking the town centre and Queens Quay, together with the area around them, will be busy, mixed use areas.

New development at each Boulevard will have a 'Gateway' function. Where buildings are situated close to Dumbarton Road, they provide an opportunity to signal the transformation of the new Queens Quay to the wider Clydebank area. High quality designs and materials are therefore essential. Some retail and commercial development is encouraged in plots alongside the boulevards, located at the ground floor of each development block.



pedestrian boulevard leading to the Titan Crane

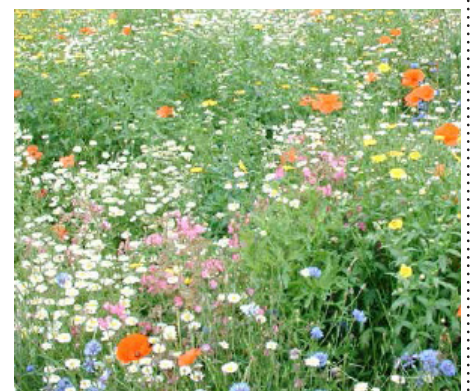
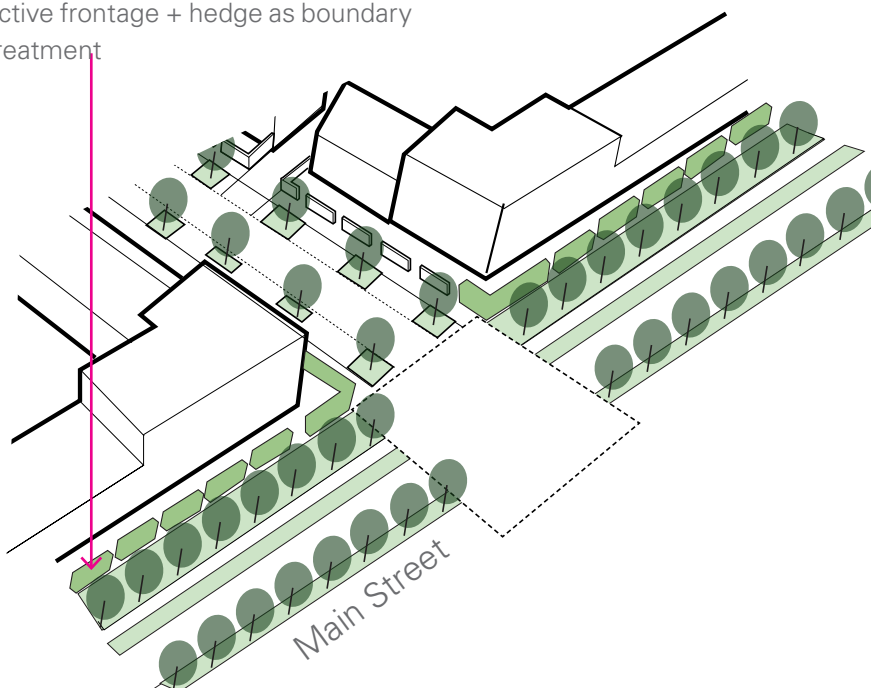


shared surface boulevard from masterplan

Development at Main Street will complement its large scale 'civic' location.

Because of Main Street's generous scale, new development beside it will need to 'contain' the space within the street, so buildings of a reasonable height, together with a continuous façade, are essential. Building typologies and massing at Main Street will change from three storey townhouses at the most western plots where the density is lower, to a maximum of seven storeys at corners closest to the Basin and the Titan Crane.

active frontage + hedge as boundary treatment



limetree avenue & wildflower planting from IWA masterplan documents

CASE STUDIES

CASE STUDIES

Each of the Case Studies on the following pages illustrates aspects of the requirements within the Design Codes. They are not intended as examples for designers to 'copy'.

Urban form: case study - Marmalade Lane designed by Mole Architects for K1 Cambridge



THEMES

- ☐ Street -urban character
- ☒ Street- parkland character
- ☒ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☐ Mews
- ☐ Mixed Use
- ☐ Parking
- ☒ Brick
- ☒ Simple eaves details
- ☒ Massing broken-up
- ☒ Vertically proportioned windows
- ☒ Simple proportions
- ☒ Good boundary treatments
- ☒ Building step up and down in height
- ☒ Active busy frontages



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Urban form: case study - Ryles Yard, Eddington

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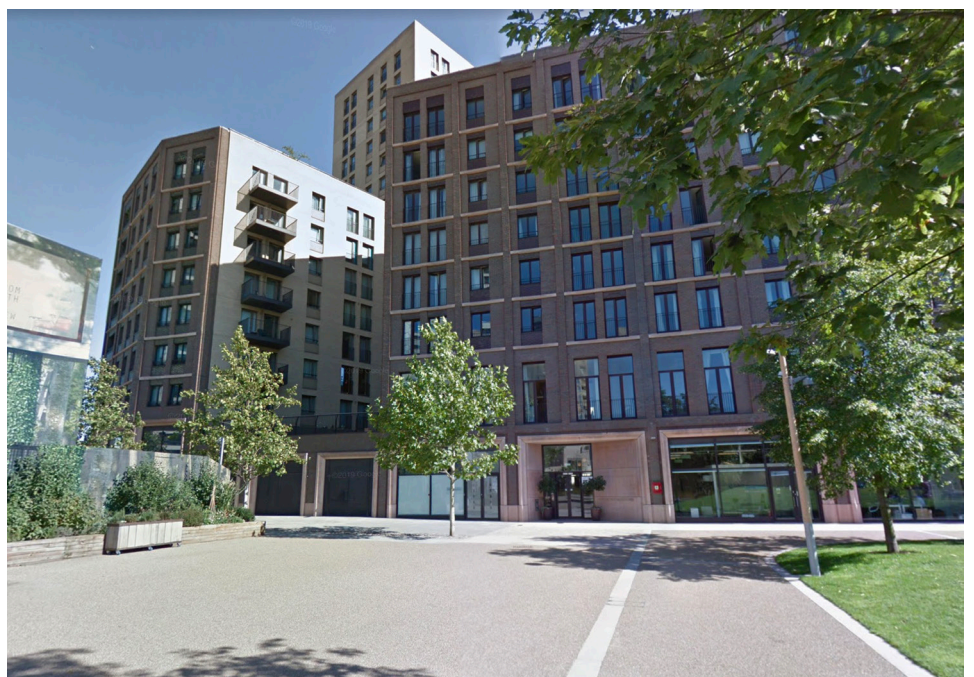
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END

Appendix 2

Queens Quay Design Codes – Annexe 2 Street Naming Strategy

1. Purpose

This annexe seeks to set out the principles to be adopted by West Dunbartonshire Council in applying a clear, contextualised and scalable street naming strategy to the Queens Quay site in Clydebank.

In line with established good practice, all street names will seek to reference the heritage of the site and surrounding area, layering narratives that will enable the new residents and its visitors to build an engaged and evolving relationship with the town and locality. The street naming approach actively seeks to contribute positively to an overarching sense of place and identity within Clydebank and West Dunbartonshire.

2. Heritage context

Although the immediate heritage context for the Queens Quay site is the redevelopment of John Brown's shipyard, it is notable that the very existence of the shipyard relied on the pre-existing contours of an enduring landscape. While the confluence of the Rivers Clyde and Cart made it possible to launch ships that travelled around the globe, to the north of the town the Kilpatrick Hills are a reminder in their archaeology that this land was settled for reasons of travel and trade long before the arrival of the shipyard.

As conveyed in the Queens Quay masterplan, this new development presents an opportunity to reconnect Clydebank and its residents with the River Clyde; previously a focus for hard industry, and now a leisure and multi-use asset. Equally, a descriptive and imaginative approach to street naming will provide opportunities for residents to follow lines of connection through the urban heart of Clydebank and into the landscapes beyond. Both the riverscape and hillscape of Clydebank have the potential to contribute positively to the health and wellbeing of new residents of the Queens Quay and the Clydebank community as a whole.

Storytelling and strong narrative will be intrinsic to the success of this street naming approach, with names selected to reference key moments in Clydebank's history, bringing focus to people, communities, industry and geography.

3. Overarching Principle

Street names will be chosen that have the potential to draw out an engaging and informative story, celebrating the overarching heritage narrative of Clydebank and the surrounding area. In addition to stories that are likely to be well known.

Applied, this storytelling principle would see a series of 'street stories' develop that engage residents and visitors alike with the history and heritage of the area. As an example:

John Browns/Titan/Tamson's Toon/Tarry Kirk/Bothy School/Foundry/East Yard (accompanied by a series of applicable suffixes, e.g. street, road, way, lane, square etc.)

The first shipyard in Clydebank was owned by J&G Thomson and later taken over by the John Brown & Company, shipbuilders. The fitting out basin of the yard has been retained as part of the riverside walkway, overseen by the Titan Crane – an A-listed monument to Clydebank's industrial past. At this shipyard three of the most famous ocean going liners in the world were built: the RMS Queen Mary, RMS Queen Elizabeth, and RMS Queen Elizabeth 2 (or the QE2).

Through this 'street story', the story of Clydebank from the coming of the shipyard to the international reputation of Clydebank-built ships is conveyed. The suggestions include some of the names as they were known locally, of the buildings lived in and frequented as part of life on the shipyard. By providing additional contextual information on-line, or via other digital platforms, and by identifying links to West Dunbartonshire Council's heritage collections, the story becomes multifaceted. The close proximity of the planned new museum in Clydebank Library and the Titan Crane visitor centre present further opportunities for additional heritage interpretation.

At the other end of the scale, an example of a lesser known story can be told focusing on Ian McHarg, the Clydebank born Landscape Architect of international reputation.

Assuming a hierarchical grouping of connected streets, McHarg's legacy could similarly be celebrated as a 'street story'. As a young man, in the years between the First and Second World Wars, McHarg often took to the Kilpatrick Hills to escape the pollution of industry that then defined Clydebank's townscape. He was involved with a group of like-minded individuals who understood the benefit to be felt from time spent outdoors. The 'Fire-sitters' of Craigallion Loch, near Carbeth, were part of a movement that saw the birth of Scottish mountaineering as it is known today and – for McHarg – a world view that continues to resonate in the field of landscape architecture. This would link into the art strategy for the health and care centre which has referenced the legacy of McHarg.

This 'street story' would therefore include: McHarg / Firesitter / Craigallion / Carbeth, etc.

Further benefit to be derived from such an approach could in turn be realised through the creation of digitally available heritage interpretation and associated learning materials. The aspiration being that by bringing these heritage references into Clydebank's present through street naming, building blocks are laid for the town's renewed identity.

4. General Principles

To further support the overarching approach to street naming in Queens Quay noted above, the principles outlined in the existing West Dunbartonshire Council Street Naming and Numbering Policy will be followed. These include various principles of good practice, including the avoidance of naming streets for living individuals; recommendations in the application of suffixes; duplication of street names; the consideration of pronunciation; and the avoidance of punctuation.

5. Street Story Principles

In order to ensure the depth and relevance of street names proposed in the context of the 'street stories' approach on Queens Quay, further considerations will be applied in the proposals of street names for the development. This will include:

- The extent to which the street story proposed references the heritage of the site and surrounding area.
- The potential to draw out an engaging and informative story that unlocks the overarching heritage narratives of the area.
- Negative associations with groups, companies or individuals that do not reflect the aspirational, forward looking ambition of the development

To assist in the fair application of these principles, a proforma will be developed to capture relevant information and make the case for each street naming proposal. Following due process, final street naming suggestions will be brought forward to the Planning Committee for approval.