

Agenda



Planning Committee

Date: Wednesday, 12 February 2020

Time: 10.00

Venue: Civic Space,
Council Offices, 16 Church Street, Dumbarton

Contact: Craig Stewart, Committee Officer
Tel: 01389 737251, craig.stewart@west-dunbarton.gov.uk

Dear Member

Please attend a meeting of the **Planning Committee** as detailed above. The business is shown on the attached agenda.

Yours faithfully

JOYCE WHITE

Chief Executive

Distribution:-

Councillor Jim Finn (Chair)
Bailie Denis Agnew
Councillor Gail Casey
Councillor Karen Conaghan
Councillor Ian Dickson
Councillor Diane Docherty (Vice Chair)
Councillor Daniel Lennie
Councillor Douglas McAllister
Councillor Marie McNair
Councillor Lawrence O'Neill

All other Councillors for information

Date of Issue: 30 January 2020

PLANNING COMMITTEE
WEDNESDAY, 12 FEBRUARY 2020

AGENDA

1 APOLOGIES

2 DECLARATIONS OF INTEREST

Members are invited to declare if they have an interest in any of the items of business on this agenda and the reasons for such declarations.

3 MINUTES OF PREVIOUS MEETING 5 – 16

Submit for approval as a correct record, the Minutes of Meeting of the Planning Committee held on 22 January 2020.

4 NOTE OF VISITATIONS 17

Submit, for information, Note of Visitations carried out on 21 January 2020.

5 OPEN FORUM

The Committee is asked to note that no open forum questions have been submitted by members of the public.

6 PLANNING APPLICATION 19 - 26

Submit report by the Strategic Lead – Regulatory in respect of the following planning application:-

DC19/235 – Change of use from beauty parlour to café (retrospective) at 129 Main Street, Renton by Mr Duncan Wrethman.

7 DESIGN CODES - QUEENS QUAY, CLYDEBANK 27 – 96

Submit report by the Strategic Lead – Regulatory seeking approval of the draft Queens' Quay Design Codes.

8 PLANNING PERFORMANCE AND FEES CONSULTATION 97 – 118

Submit report by the Strategic Lead – Regulatory informing of the Scottish Government’s “Planning Performance and Fees” consultation, and requesting consideration of the Council’s response.

9 DECISION ON APPEAL AGAINST PLANNING APPLICATION 119 - 121
FOR PETROL FILLING STATION, A JET WASH FACILITY
AND ALTERATIONS TO THE CAR PARK AT LIVINGSTONE
STREET, CLYDEBANK (DC18/209)

Submit report by the Strategic Lead – Regulatory informing on the outcome of the above planning appeal.

PLANNING COMMITTEE

At a Meeting of the Planning Committee held in the Council Chamber, Clydebank Town Hall, Dumbarton Road, Clydebank on Wednesday, 22 January 2020 at 10.00 a.m.

Present: Bailie Denis Agnew* and Councillors Gail Casey, Karen Conaghan, Ian Dickson, Diane Docherty, Jim Finn, Marie McNair and Lawrence O'Neill.

*Attended later in the meeting.

Attending: Pamela Clifford, Planning, Building Standards and Environmental Health Manager; Erin Goldie, Team Leader – Development Management; Antony McGuinness, Team Leader – Forward Planning; Ross Lee, Lead Planning Officer; Nigel Ettles, Section Head – Litigation and Craig Stewart, Committee Officer.

Apologies: Apologies for absence were intimated on behalf of Councillors Daniel Lennie and Douglas McAllister.

Councillor Jim Finn in the Chair

CHAIR'S REMARKS

Prior to commencing with the business of the meeting, Councillor Finn, Chair, took the opportunity to wish everyone a Happy New Year, and thanked Councillor Docherty, Vice Chair, for chairing meetings during his period of sickness absence.

DECLARATIONS OF INTEREST

It was noted that there were no declarations of interest in any of the items of business on the agenda.

MINUTES OF PREVIOUS MEETING

The Minutes of Meeting of the Planning Committee held on 19 November 2019 were submitted and approved as a correct record.

NOTE OF VISITATIONS

A Note of Visitations carried out on 18 November 2019, a copy of which forms Appendix 1 hereto, was submitted and noted.

OPEN FORUM

The Committee noted that no open forum questions had been submitted by members of the public.

PLANNING APPLICATIONS

Reports were submitted by the Strategic Lead – Regulatory in respect of the following planning applications:-

(1) DC19/203 – Erection of residential development at Farm Road, Duntocher, Clydebank by Barratt Homes West Scotland.

Reference was made to the site visit which had been undertaken in respect of the above application. The Planning, Building Standards and Environmental Health Manager was heard in further explanation of the report.

The Chair invited Ms Rose Harvie to speak on her own representation and also on behalf of Clydebelt in respect of theirs. The Committee also heard from Mr Alan Shields, Mr William Blair, Ms Marion Scanlan and Mr J.J. Connelly in respect of their representations in regard to the application.

The Chair then invited Mr David Campbell, agent, and Ms Heather Philp, on behalf of the applicant, to address the Committee. Mr Campbell and Ms Philp were heard in support of the application and in answer to Members' questions.

After consideration and having heard the Planning, Building Standards and Environmental Health Manager and the Team Leader – Forward Planning in answer to Members' questions, the Committee agreed to refer the application to the full Council for determination, expressing the provisional view that the application should be refused for the reasons set out in Section 9 of the report.

Note: Bailie Agnew entered the meeting while the above item was being introduced by the Planning, Building Standards and Environmental Health Manager.

ADJOURNMENT

After hearing Councillor Finn, Chair, the Committee agreed to adjourn for a short comfort break. The meeting resumed at 11.00 a.m., with same Members present as listed in the sederunt.

- (2) **DC19/186 – Residential development comprising 70, two storey terraced, semi-detached and detached houses with 65 two bedroom flats over three and four storeys with associated roads, parking and landscaping at Stanford Street, Clydebank by Turnberry Homes.**

Reference was made to the site visit which had been undertaken in respect of the above application. After discussion and having heard the Planning, Building Standards and Environmental Health Manager and the Lead Planning Officer in further explanation and in answer to Members' questions, the Committee agreed to grant full planning permission subject to the conditions set out in Section 9 of the report, as detailed within Appendix 2 hereto.

- (3) **DC19/169 – Formation of a plot for a single house with associated access, parking and garden ground at land at Dillichip Loan, Bonhill by Mr John Burleigh.**

The Chair invited Mr Jack Fordy, on behalf of Bonhill & Dalmonach Community Council, objector, to come forward to address the Committee and he was heard in respect of his representation.

After discussion and having heard the Planning, Building Standards and Environmental Health Manager and relevant officers in further explanation of the report and in answer to Members' questions, Councillor Finn, seconded by Bailie Agnew, moved:-

That the application be continued to the next meeting of the Committee, in order to enable further information to be obtained on trees and the road access/junction at the site.

As an amendment, Councillor McNair, seconded by Councillor Conaghan, moved:-

That the Committee agree the recommendation in the report and to grant planning permission in principle subject to the conditions set out in Section 9 of the report.

On a vote being taken, 4 Members voted for the amendment and 4 for the motion. There being an equality of voting, Councillor Finn, Chair, exercised his casting vote in favour of the motion which was accordingly declared carried.

NAME FOR A NEW COMMUNITY PARK AT THE FORMER ST EUNAN'S PRIMARY SCHOOL SITE IN CLYDEBANK

A report was submitted by the Strategic Lead – Regulatory requesting the allocation of a new name to a community park on the former St Eunan's Primary School site in Clydebank.

After discussion, the Committee agreed that Melfort Park be approved as the new name for the community park at this location.

The meeting closed at 11.40 a.m.

DRAFT

PLANNING COMMITTEE

NOTE OF VISITATIONS – 18 NOVEMBER 2019

- Present:** Councillors Diane Docherty and Karen Conaghan.
- (The above lists Members who attended at least one site visit).
- Attending:** Pamela Clifford, Planning, Building Standards and Environmental Health Manager.

SITE VISITS

Site visits were undertaken in connection with the undernoted planning applications:-

(1) **Former Highdykes Primary School site, Braehead, Bonhill**

DC19/144 – Erection of residential development comprising 49 terraced houses and cottage flats with associated roads, parking and landscaping by AS Homes (Scotland) Ltd.

(2) **Main Street, Renton**

DC19/193 – Demolition of existing Primary School & erection of new build education campus (including Renton Language & Communication Unit and Riverside Early Learning and Childcare Centre with associated parking and landscaping) by WDC.

DC19/186 – Residential development comprising 70, two storey terraced, semi-detached and detached houses with 65 two bedroom flats over three & four storeys with associated roads, parking and landscaping at Stanford Street, Clydebank by Turnberry Homes.

GRANT full planning permission subject to the following conditions:-

1. Exact details, specifications and samples of all proposed external materials for the buildings shall be submitted for the further written approval of the Planning Authority prior to any work commencing on site. This shall include details of all materials and finishes as scheduled in Section 8.5 of the approved 'Design and Access Statement' (Dated August 2019). Thereafter, the development shall be completed in accordance with the approved material details.
2. Prior to the brickwork associated with any housing unit/property being constructed or installed on site, a sample panel of all brickwork shall be constructed on site in order to determine the appropriate brick and colour of mortar that should be used which shall be approved by the Planning Authority. Thereafter the development shall be completed in accordance with the approved mortar.
3. Prior to the commencement of development on site, full details of all hard surfaces, paths, walls and fences shall be submitted for the approval of the Planning Authority. This shall include details for all footpaths and path networks within the site as well as the surfacing and finishes for all public realm and open space areas. Thereafter, these shall be implemented prior to the occupation of any associated houses unless otherwise agreed in writing by the Planning Authority.
4. Prior to the commencement of development on site, details of the design and location of cycle storage provision for houses/flats, street furniture (including bin stores) and lighting, shall be submitted for the further written approval of the Planning Authority. The development shall thereafter be completed in accordance with the approved details prior to the occupation of any of the houses and thereafter maintained, unless otherwise agreed in writing with Planning Authority.
5. Prior to the commencement of development on site, details of measures to protect the trees and hedgerows located within and adjacent to the site (including those forming part of the tree belt to the northern site boundary) as outlined to be retained in the approved 'Tree Survey and Arboricultural Constraints Report' (Dated July 2019) shall be submitted for the written approval of the Planning Authority. The trees shall be protected during the course of development by the erection of fencing in accordance with British Standard BS 5837(2012) 'Trees in Relation to Construction', or by such other means of protection as shall be agreed in advance in writing with the Planning

Authority. No storage of building materials or piling of soil shall take place within the protected areas established pursuant to this condition. Thereafter, the development shall be undertaken in accordance with the approved details.

6. Prior to the commencement of development on site, a detailed soft landscape scheme shall be submitted to and approved in writing by the Planning Authority. These details shall include a full planting schedule and maintenance arrangements. Once approved, this shall be implemented no later than the next appropriate planting season or after occupation of the 50th property. The landscaping arrangements as approved shall thereafter be maintained in accordance with these details unless otherwise agreed in writing by the Planning Authority.
7. Further to condition 6 above and prior to the commencement of development on site, details of the design, appearance and maintenance arrangements for the proposed open space and public realm areas as shown on approved drawing 'Site Plan Open Space Areas (Drawing No. AL(0)03)' shall be submitted to and approved in writing by the Planning Authority. This shall include details for the area to the north of the site including the works associated with the connection points to the canal towpath and the maintenance for these areas. Thereafter, the development shall be undertaken in accordance with the approved details.
8. Prior to the commencement of development on site, full details of the design and location of all retaining walls and other retention features and associated levelling works required along the northern and eastern boundary of the site shall be submitted to and approved in writing by the Planning Authority. Thereafter, the agreed retention and levelling details shall be implemented prior to the occupation of any associated houses unless otherwise agreed in writing by the Planning Authority. No changes or deviations from the approved and agreed levels and retention arrangements shall be undertaken without the further written consent of the Planning Authority.
9. All construction work on site relevant to the application shall be carried out in accordance with the recommendation and mitigation measures outlined within Sections 8.3 and 8.5 of the approved 'Extended Phase 1 Habitat Survey' (Dated June 2019) including those associated with the protection of foraging and commuting otters.
10. Further to condition 9 above, all ground or vegetation clearance works, including any tree felling or demolition works, shall take place out with the main bird breeding season (i.e. outwith the period of April to July inclusive), and no demolition or ground or vegetation clearance works are permitted between April to July in this respect. If this is not possible, a suitably qualified ornithologist/ecologist shall be engaged to survey any buildings, grounds and trees immediately prior to such works to advise the applicant/contractor/developer of an bird nesting activity and of any actions required to protect birds.

11. Prior to the occupation of the 50th housing unit/property within the site, the drainage of surface water shall be completed in accordance with the approved Sustainable Urban Drainage System (SUDS) design, as set out in the approved 'Drainage Strategy Report' (Dated August 2019) and the approved plans. The SUDS and associated features including the planted swales once installed shall thereafter be maintained on site in accordance with the approved details, unless otherwise agreed in writing by the Planning Authority.
12. No housing unit/property shall be occupied until the vehicle parking spaces associated with that house unit have been provided within the site in accordance with approved plan 'Site Plan (Drawing No. AL(0)03 Rev A)'. The aforementioned parking shall thereafter be retained and be capable of use at all times and shall not be removed or altered without the prior written approval of the Planning Authority.
13. Further to condition 12 and unless otherwise agreed in writing by the Planning Authority, prior to the commencement of development with the site, details of the location and design of an electric charging point(s)/unit(s) to serve the development shall be submitted to and approved in writing by the Planning Authority. The approved car charging point(s)/unit(s) and associated infrastructure shall thereafter be installed in accordance with the approved details at a timescale agreed by the Planning Authority and maintained as such thereafter.
14. No development (other than investigative works) shall commence on site until such time as a detailed report on the nature and extent of any contamination of the site has been submitted to and approved in writing by the Planning Authority. The report shall be prepared by a suitably qualified person and shall include the following:
 - a) A detailed site investigation identifying the extent, scale and nature of the contamination on the site (irrespective of whether this contamination originates on the site).
 - b) An assessment of the potential risks (where applicable) to:
 - Human health
 - Property (existing and proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes
 - Groundwater and surface waters
 - Ecological systems
 - Archaeological sites and ancient monuments
 - c) An appraisal of remedial options, including a detailed remediation scheme based on the preferred option.

15. No development (other than investigative works) shall commence on site until such time as a detailed remediation scheme for the site has been submitted to and approved in writing by the Planning Authority. The scheme shall be prepared by a suitably qualified person and shall detail the measures necessary to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property, and the natural and historical environment. The scheme shall include details of all works to be undertaken, the remediation objectives and criteria, a timetable of works and/or details of the phasing of works relative to the rest of the development, and site management procedures. The scheme shall ensure that upon completion of the remediation works the site will not qualify as contaminated land under Environmental Protection Act 1990 Part IIA in relation to the intended use of the land after remediation.
16. The approved remediation scheme shall be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Planning Authority. The Planning Authority shall be notified in writing of the intended commencement of remediation works not less than 14 days before these works commence on site. Upon completion of remediation works and prior to any dwellinghouse being occupied, a verification report which demonstrates the effectiveness of the completed remediation works shall be submitted to and approved in writing by the Planning Authority.
17. If required, a monitoring and maintenance scheme to include monitoring the long-term effectiveness of the proposed remediation over a period of years determined by the scheme shall be submitted to and approved by the Planning Authority. Any actions ongoing shall be implemented within the timescale agreed by the Planning Authority in consultation with Environmental Health measures. Following completion of the actions/measures identified in the approved remediation scheme a further report which demonstrates the effectiveness of the monitoring and maintenance measures shall be submitted to and approved in writing by the Planning Authority.
18. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
19. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall be free from metals, plastic, wood, glass, tarmac, paper and odours. Prior to placement of any of the material, the developer

shall submit a validation report for the approval in writing of the Planning Authority and it shall contain details of the source of the material and associated test results to demonstrate its suitability for use. Thereafter the development shall be undertaken in accordance with the approved details.

20. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
21. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This statement shall include an assessment of and take into account the following:
- The impact of the piling on surrounding properties.
 - Detail any procedures which are required to minimise the impact of noise and vibrations on the occupants of surrounding properties.

This statement as submitted shall be prepared by a suitably qualified person and shall take into account the guidance contained in BS6472:1984 'Evaluation of Human Response to Vibration of Buildings'. The piling works shall thereafter be carried out in accordance with the approved method statement until they are completed on site.

22. No development shall take place on site until such a time as a noise impact assessment has been submitted to and approved in writing by the Planning Authority. This noise impact assessment shall include an assessment of the potential for occupants of the development to experience noise nuisance arising from all nearby industrial/commercial/recreational grounds. Where a potential for noise disturbance is identified, proposals for the attenuation of that noise shall be submitted to and approved in writing by the Planning Authority. The noise impact assessment and any recommendations in respect of attenuation measures shall be prepared by a suitably qualified person. Any such approved noise attenuation scheme shall be implemented prior to the development being brought into use and shall thereafter be retained in accordance with the approved scheme.
23. Further to condition 22 above and prior to the commencement of any development on site, a report (prepared by an independent suitably qualified person/consultant) which demonstrates compliance and validation of the measures outlined in the approved noise impact assessment shall be submitted to and approved in writing by the Planning Authority. In particular, this report shall demonstrate that any projections detailed within the noise attenuation scheme as part of the noise impact assessment are reliable and accurate. The measurements shall be carried out in accordance with

BS4142:2014: "Methods of rating and assessing industrial and commercial sound" (with respect to current best practice).

24. No development shall take place on site until such a time as a lighting impact assessment has been submitted to and approved in writing by the Planning Authority. This assessment shall take cognisance of and assess the impact of nearby sports pitches and their associated floodlights to the south of the site as well as the commercial and retail units to the north of the site. The lighting impact assessment shall include the following:

- Assessment and consideration of the potential for obstructive lighting affecting properties within the development site.
- Details of mitigation measures to negate any nuisance from the lights on the recreational grounds/sports pitches in particular.

Any such mitigation measures proposed shall thereafter be implemented prior to the occupation of the applicable housing unit/property to which they apply/relate and shall be maintained as such thereafter unless otherwise agreed in writing by the Planning Authority.

25. Unless otherwise approved in writing by the Planning Authority, no development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing by the Planning Authority. The scheme shall identify likely sources of dust arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.
26. No development shall commence on site until details for the storage and the collection of waste arising from the development shall be submitted to and approved in writing by the Planning Authority. The agreed details shall be in place prior the occupation of the first housing unit/property within the site and thereafter maintained for the lifetime of the development.
27. During the period of construction, all external works including piling and ancillary operations shall be carried out between the following hours and at no other time, unless otherwise agreed in writing by the Planning Authority:
- Mondays to Fridays : 0800 – 1800
 - Saturdays: 0800 – 1300
 - Sundays and public holidays: No Working
28. Prior to the commencement of development on site, full details of all soft and water landscaping works shall be submitted to and approved in writing by the Planning Authority. These details must comply with Advice Note 3 'Potential Bird Hazards from Amenity Landscaping & Building Design'. These details shall include:

- The species, number and spacing of trees and shrubs.
- Drainage details including SUDS.

The details shall be implemented as approved at a timescale to be agreed with the Planning Authority. It shall remain in force for the life of the development and no subsequent alterations to the plan shall take place unless firstly approved in writing by the Planning Authority in consultation with Glasgow Airport.

29. Prior to the occupation of the first housing unit/property within the site, the developer shall install the necessary infrastructure to enable the full development and all associated properties to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.
30. Unless otherwise agreed in writing, no development shall commence on site until a Travel Plan which includes details promoting sustainable travel to and from the site and appropriate measures and actions to reduce car dependence for the development is submitted for the written approval of the Planning Authority. The Travel Plan shall include details of all of the measures and proposals as set out within approved document 'Sustainable Transport, Active Travel and Parking Technical Report' (Dated December 2019) and shall also comply with Planning Advice Note 75 (PAN75). The approved Travel Plan and the associated measures and actions shall be in place and implemented in full prior to the occupation of the 50th housing unit/property associated with the development and maintained as such unless otherwise agreed in writing by the Planning Authority.
31. Prior to the occupation of the first housing unit/property within the site, the works required to form the emergency vehicular access for the development shall be implemented and constructed in full in accordance with the approved details. Thereafter and once constructed, the emergency vehicular access shall be made available for unrestricted use at all times and shall be maintained as such within the site for the lifetime of the development.

PLANNING COMMITTEE

NOTE OF VISITATIONS – 21 JANUARY 2020

Present: Councillors Karen Conaghan and Marie McNair.

Attending: Pamela Clifford, Planning, Building Standards and Environmental Health Manager; Antony McGuinness, Team Leader – Forward Planning; Erin Goldie, Team Leader – Development Management and Ross Lee, Lead Planning Officer.

SITE VISITS

Site visits were undertaken in connection with the undernoted planning applications:-

(1) **Farm Road, Duntocher, Clydebank**

DC19/203 – Erection of residential development by Barratt Homes West Scotland.

(2) **Dillichip Loan, Bonhill**

DC19/169 – Formation of a plot for a single house with associated access, parking and garden ground at land by Mr John Burleigh.

(3) **Stanford Street, Clydebank**

DC19/186 – Residential development comprising 70, two storey terraced, semi-detached and detached houses with 65 two bedroom flats over three and four storeys with associated roads, parking and landscaping by Turnberry Homes.

WEST DUNBARTONSHIRE COUNCIL

Report by Strategic Lead - Regulatory

Planning Committee: 12th February 2020

**DC19/235: Change of use from beauty parlour to café (retrospective) at
129 Main Street, Renton by Mr Duncan Wrethman.**

1. REASON FOR REPORT

- 1.1** The application is subject to representations and raises issues of local significance. Under the terms of the approved Scheme of Delegation, it therefore requires to be determined by the Planning Committee.

2. RECOMMENDATION

- 2.1** **Grant** planning permission for the reasons set out in Section 9.

3. DEVELOPMENT DETAILS

- 3.1** The application site is a ground floor commercial premises located within a traditional 2 ½ storey terraced building located on the west side of Main Street in Renton. It is one of four commercial premises in a row accessed from Main Street including a public house, hairdressers and retail unit. There is a pend opening at the north end of the building that provides access from Main Street to the rear of the building where there are gardens and a staircase that leads to the residential properties above the commercial premises.
- 3.2** This is a retrospective application that seeks permission for a café. The last recorded use of the premises was as a Beauty Parlour granted planning permission in 1996. The current owner/tenant has been operating a café from the premises since May 2018. It is understood that the premises previously was used as a café/ snack shop under separate ownership although there is no planning record of this use. The café is currently opened 7 days a week from 07:00am to 14:00pm Monday – Saturday and 08:00 am to 1:00pm on a Sunday and offers a variety of fried foods including full breakfasts, chicken nuggets, chips, as well as other hot food items such as curry. The café also provides an external catering service. It has 3 tables and one bench and seating for 13 people. There is a service and display counter, food preparation area and staff toilet facilities.
- 3.3** An application for planning permission (DC19/030) for the same use was refused in November 2019 due to the absence of details of an adequate ventilation and extraction system to address odours associated with the use. Following the refusal of the application an Enforcement Notice was served and this has been appealed to the Department of Planning and Environmental Appeals (DPEA) by the applicant. This revised application has

now submitted separately to the enforcement process and seeks retrospective consent for the same use but with an amended ventilation system and details provided.

4. CONSULTATIONS

- 4.1** West Dunbartonshire Council Roads Service has no objection to the proposed use.
- 4.2** West Dunbartonshire Council Environmental Health Service has no objection to the amended ventilation details and have recommended a number of conditions.

5. REPRESENTATIONS

- 5.1** Two representations have been received from neighbouring residential properties located above the café. Their reasons for objection predominantly focuses upon the adverse cooking odours and health and safety concerns regarding the use and activity associated with the premises. The following matters have been raised as part of the representations received:

- Inability to confirm that the new ventilation equipment will satisfactory address ongoing odour issue.
- Doubt regarding the validity of the ventilation equipment and the assessment undertaken.
- Concern as to when the ventilation will be installed and when it will become operational noting the ongoing odour and amenity issues.
- Concern regarding how the proposed ventilation will operate, when it will operate and how it will be maintained/cleaned.
- Lack of cooperation from applicant to reduce/change menu offering.
- Issues with vehicular and pedestrian safety due to traffic generation and inappropriate parking associated with the use and premises.

6. ASSESSMENT AGAINST THE DEVELOPMENT PLAN

West Dunbartonshire Local Plan 2010

- 6.1** The application site is located within a Local Centre and Policy RET7 seeks to protect and enhance vitality and viability of commercial units and supports the establishment of new uses where appropriate. Applications for any change of use from retail to another use will be supported where it can be satisfactorily demonstrated that such a change would enhance the centre and would not have a detrimental impact on adjacent properties by way of noise, disturbance and odour, and would not adversely affect the general character and amenity of the area. Policy GD1 relates to all new development and sets an expectation that they respect the character and amenity of the area in which it is located. The principle of the use of the premises as a café is supported at this location as it enhances the commercial offering and adds vitality and activity to this location. The proposal is also compliant with Policy RET7 and

GD1 as the proposed ventilation and extraction system is now considered adequate to control odours associated with the type of cooking at the café. This is discussed in detail in Section 7.

7. ASSESSMENT AGAINST MATERIAL CONSIDERATIONS

West Dunbartonshire Local Development Plan (LDP1) Proposed Plan (2016)

- 7.1** On 27 April 2016, the Planning Committee took a final decision not to accept the Local Development Plan Examination Report as it recommended modification in respect of including the Duntiglennan Fields site in Clydebank as a housing development opportunity. Therefore, as a result of the Scottish Ministers' Direction, the Local Development Plan has remained unadopted but continues to be a material consideration in the determination of planning applications.

- 7.2** The application site is located within an area identified as an Existing Neighbourhood. Policy BC4 states development that would significantly harm the residential amenity, character or appearance of existing neighbourhoods will not be permitted. Policy DS1 states that all development should avoid unacceptable impacts from adjoining uses including noise, smell, vibration, dust, air quality, invasion of privacy, and overshadowing. The development is considered to be compliant with the applicable policies and its compatibility is discussed in detail further in Section 7.

West Dunbartonshire Local Development Plan (LDP2) Proposed Plan (2018)

- 7.3** On 19th September 2018, Planning Committee approved Local Development Plan 2: Proposed Plan for consultation. It is therefore the Council's most up to date policy position and it is a material consideration in the assessment of planning applications. The Plan is currently at Examination stage, which commenced on 20th August 2019.
- 7.4** The application site is within an area defined as an existing residential area where Policy H4 applies. Similar to GD1 and RT7 of the adopted plan and DS1 and BC4 of the Proposed Plan (2016), Policy H4 states that there will be general presumption against non-residential uses, which potentially have detrimental effects on local amenity. The ventilation and extraction system proposed is considered to be appropriately, allowing the development to be compliant with the policies which protect amenity.

Background

- 7.5** The current use of the premises as a café was brought to the attention of the Planning Authority following complaints of cooking odours received from the residential properties located above. The premises was subsequently visited and the use investigated by the Planning Compliance Officer. The food offering/menu and associated cooking equipment, including deep fat fryers were noted and advice sought from the Council's Environmental Health Service. The owner/tenant was subsequently advised that a retrospective application for planning permission should be submitted to regularise the change of use and should be accompanied by proposals for a ventilation system that can control cooking odours and address concerns about smell

and odour. Although permission was granted for a beauty parlour in 1996, sometime later the premises was used as a café/snack shop. When the premises was taken over by the applicant in May 2018, larger fryers were installed and the menu was extended and intensified. Regular complaints followed from the residents living above the premises with the use of the premises now requiring planning permission.

- 7.6** Following the above discussions, a retrospective planning application (DC19/030) was subsequently submitted by the applicant in February 2019. This application was refused at Planning Committee in September 2019 due to inadequate ventilation details to address odour issues. Following refusal, the Council required to undertake enforcement action as a means to try to regularise the breach of planning control (unauthorised use of premises as a café) whilst at the same time safeguard amenity. An enforcement notice was issued and this matter remains ongoing at this time separate from the new planning application submitted.

Principle of Use

- 7.7** The premises is within a designated local centre in the Adopted Plan and within the Proposed Plans as an existing neighbourhood/residential designation. The principle of the use as a café is supported by both the Adopted and Proposed Plans provided there will be no detrimental impact on neighbouring residential amenity as a result of the use. The Council's Environmental Health Service have maintained from the outset of their involvement that the lack of ventilation within the premises is unacceptable for the type of cooking/menu offering at the café and on this basis have continually requested details of a suitable ventilation and extraction system as part of this process. To date, the absence of any such appropriate ventilation system which demonstrates an ability to address the odours generated has prevented this particular use from being considered acceptable at this location.
- 7.8** This revised planning application has included a report from the applicant's ventilation consultant which details the proposals for a new ventilation and extraction system to be installed within the premises. The report sets out and specifies all of the equipment proposed and details how it will be cleaned and maintained once installed and operational. It also provides an assessment as to how the system will combat odour generated from the cooking activities associated as part of the use whilst at the same time demonstrates how it will be able to function in a manner which does not create noise/vibration issues from within the premises. This has all been reviewed and assessed in detail and it is now considered that the proposals provided do demonstrate that the intended system can safeguard neighbouring residents from experiencing adverse odours associated with the type of cooking used. Subject to conditions, the acceptability of the ventilation system now allows the use to be supported and the proposal is now considered to comply with the policies of both the Adopted and Proposed Plans.

Residential Amenity

- 7.9** Two representations have been submitted in response to this application, however historically there has been a number of complaints to the Council regarding odours experienced in the residential properties located above the café. Despite the Council's Environmental Health officers having visited these properties, a statutory odour nuisance has not been established and this remains the case. The residents have been advised to contact Environmental Health directly at the time of experiencing odours and this remains the protocol.
- 7.10** The proposals submitted for the ventilation and extraction system have been assessed and are considered sufficient to address residential amenity issues. A specialist and professional ventilation contractor has been employed by the applicant and has confirmed that the proposed system is suitable for the type and method of cooking at the café and the ventilation arrangement can effectively safeguard neighbouring amenity and residents from odour nuisance. Equally, the report confirms that the equipment due to its design and arrangement will not cause secondary noise or vibration issues for adjoining residential properties. It is now considered to be sufficient to allow the equipment to be accepted and deemed fit for purpose to address the cooking activity and prevent it from adversely impacting amenity in this regard.
- 7.11** The applicant has indicated that the café is currently opened 7 days a week from 07:00am to 14:00pm Monday – Saturday and 08:00 am to 1:00pm on a Sunday. However they have indicated that during summer months they would like to extend the opening hours from 07:00am to 16:00 pm Monday-Saturday and 08:00am to 16:00 pm on a Sunday. This is considered to be acceptable in the circumstances as it only involves a small increase in the opening hours but the café will still operate within normal day time hours.

Technical Matters

- 7.12** The Council's Environmental Health Service has confirmed that based on the information provided that they are accepting that the arrangement as shown represents a suitable ventilation and extraction system which does demonstrate a capability to control odours associated with the type of cooking on offer through the existing café menu.
- 7.13** The option proposed is considered to be feasible and relates to the installation of an odour filtration system internally within the café which does not require external treatment including the need for a flue to run up the outside of the building. As previously outlined, a specialist ventilation contractor has been brought in and assessed the cooking/menu at the café and advised on the most suitable ventilation option to eliminate potential odour nuisance. The exact make up of the ventilation equipment is set out in the report provided as part of the application and its installation can be addressed through appropriately worded planning conditions to ensure it is in place in a timely manner noting the retrospective nature of the use and the ongoing amenity concerns. A requirement for it to be cleaned and regulated in accordance with the proposed maintenance programme will also form part of a condition

to ensure that the equipment is managed appropriately and to make sure remains effective in combatting odours generated from the use going forward.

- 7.14** Whilst separate points have been made in the representations in relation to the use and its impact and influence on traffic, parking and vehicular safety, the Council's Road Service have assessed the use and have no objections to the proposals. For clarify, illegal parking (a matter raised in the representation received) is not a material planning consideration or indeed a roads matter. This would be a matter for Police Scotland when these issues arise.

8. CONCLUSION

- 8.1** The principle of the use of the premises as a Class 3 café is acceptable and a suitable ventilation and extraction system has been proposed in order to allow the use to be considered acceptable. The new ventilation system proposed is considered satisfactory to support the type of cooking/menu offer at the café and its installation will allow the use to operate without adversely impacting upon amenity of nearby residential properties. On this basis, the proposal complies with the relevant policies of the Adopted and Proposed Plans.

9. CONDITIONS

1. Within 8 weeks of the date of this permission and further to approved report 'Village Café Commercial Kitchen Design and Installation Ventilation Strategy' (10th January 2020) the ventilation and extraction system and equipment as scheduled on Page 4 of this approved report shall be installed in full in line with manufacturers guidelines within the premises. Once installed, the approved ventilation and extraction system shall thereafter be retained within the premises, functioning and operating to the approved manufacturer's standards and requirements for the lifetime of the use of the premises as approved unless otherwise agreed in writing by the Planning Authority.
2. Further to Condition 1 above, the approved ventilation and extraction system once installed and operational shall be maintained in working order and cleaned in strict accordance with the system maintenance programme as set out in Page 5 of approved report 'Village Café Commercial Kitchen Design and Ventilation Strategy' (10th January 2020) for the lifetime of the use of the premises as approved unless otherwise agreed in writing by the Planning Authority.
3. Within 4 weeks of the date of this permission, details of an adequate size grease trap shall be submitted to and approved by the Planning Authority. Thereafter, the grease trap unit shall be installed concurrently with the ventilation equipment as required under condition 1 and maintained as such for the lifetime of the use of the premises as approved unless otherwise agreed in writing by the Planning Authority

4. The business and use hereby approved shall operate only between the hours of 0700hrs to 1600hrs Monday to Saturday and 0800hrs to 1600hrs on a Sunday unless otherwise agreed in writing by the Planning Authority.

Peter Hissett
Strategic Lead - Regulatory
Date: 12th February 2020

Person to Contact: Pamela Clifford, Planning, Building Standards and Environmental Health Manager
Email: Pamela.Clifford@west-dunbarton.gov.uk

Appendix: None

Background Papers:

1. Application documents and plans
2. West Dunbartonshire Local Plan 2010
3. West Dunbartonshire LDP - Proposed Plan
4. West Dunbartonshire LDP - Proposed Plan 2
5. Consultation responses
6. Representations

Wards affected: Ward 2 (Leven)

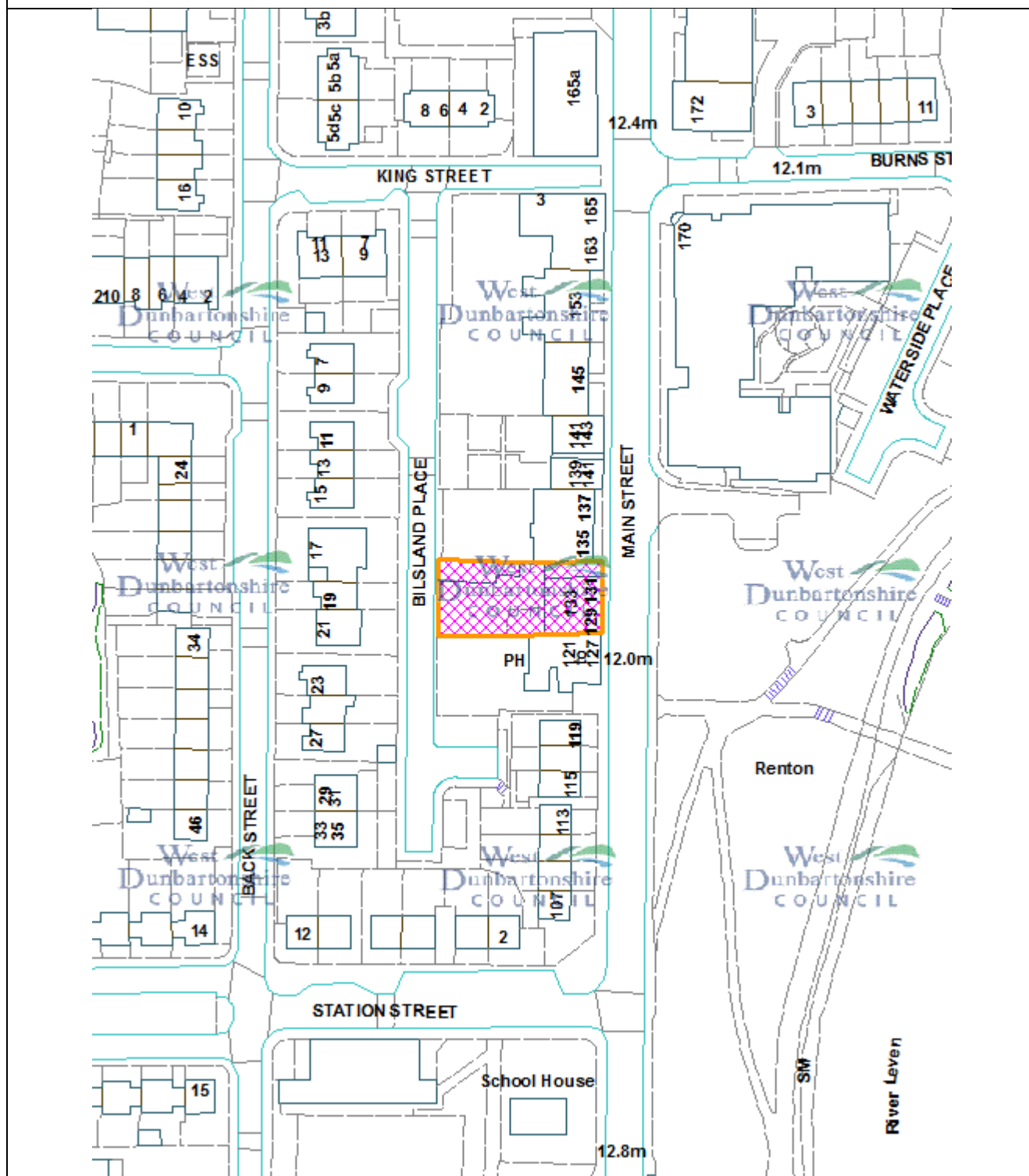
Map Register No: HQ635

Date: 29 January 2020

DC19/235

Change of use from beauty
parlour to café
(retrospective)

129 Main Street
Renton
Dumbarton
G82 4NL



WEST DUNBARTONSHIRE COUNCIL**Report by Strategic Lead- Regulatory****Planning Committee: 12th February 2020**

Subject: Design Codes - Queens Quay, Clydebank.**1. Purpose**

- 1.1 To seek approval of the draft Queens' Quay Design Codes.

2. Recommendations

- 2.1 It is recommended that the Committee approves the Queens' Quay Design Codes document set out in Appendix 1 for consultation.

3. Background

- 3.1 The Queens Quay site is one of the most significant regeneration opportunities in West Dunbartonshire and it is seen as a key catalyst in the transformation of Clydebank. A Design Framework, including masterplan has been approved for the central 23 hectares of the site and Planning Permission in Principle has been granted for up to 1,000 new houses, commercial, health and leisure uses, public realm and road infrastructure.
- 3.2 There has already been considerable investment in the areas surrounding the site with West College Scotland, the Titan Enterprise and Aurora House offices and the Clydebank Leisure Centre. Work is well underway on a District Heating Energy Centre and the central road infrastructure is now complete on the site. The new Clydebank Care Home is under construction and this will form part of a 'health quarter' alongside a health and care centre which will begin construction this year together with the construction of 146 affordable homes. The Queens Quay site will also be linked with the town centre and the railway and bus station by the Connecting Clydebank A814 Corridor project which is to provide a high quality streetscape.
- 3.3 The vision for Queens Quay is as a vibrant mixed use community that links the town centre with the waterfront. It is intended that Queens Quay will be a place for people to live and work which will allow the Clydebank community access to the river, as well as, high

quality public spaces at the waterfront to attract visitors and new residents from outwith Clydebank. The approved development on Queens Quay is of very high quality and it is intended that this will continue through the remaining plots. The approved masterplan identifies twelve development parcels with two of these parcels now well advanced as described above.

- 3.4** An accredited design consultant team have been commissioned to provide Design Codes to support the delivery of the remainder of the development parcels. It is intended that the Design Codes will provide clarity and certainty to decision makers and to prospective developers about the expected outcomes with regard to key aspects of the street layout, development pattern, massing, density architectural treatment and materials. The Design Codes will assist in providing clear guidance at an early stage of the development process and provide a structured approach for the assessment of future planning applications in the Development Management process. It will help to facilitate the delivery of high quality development for the remaining development plots. The guidance builds upon the approved Design Framework and will be used by developers, planning officers and decision makers to inform design requirements and place making ambitions at an early stage of the development process with the aim of creating a high quality place.

4. Main Issues

General Design Codes

- 4.1** A Design Code is a form of detailed guidance often used in respect of a new neighbourhood. Typically they are developed in support of a Masterplan and comprise of a series of written and graphic rules that set out the two and three dimensional design elements of the development. Design Codes are a positive statement recognising the particular qualities of a place, focusing on design characteristics that are important to achieve and they establish and firmly fix the 'must have' design elements. In doing so, Design Codes help to provide continuity in quality and consistency over time. They offer certainty to developers and communities around the type of development that may be built next to their own development ensuring that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision. Design Codes are most successful when they are developed to 'get the fundamentals right' and are not so prescriptive that they give too little scope for distinctive and innovative architectural expression.

Queens Quay Design Codes Document

- 4.2** The Queens Quay Design Codes, which is contained in Appendix A, set out the parameters for how development should be taken forward for the remaining development plots on Queens Quay which will be principally developed for private housing. It is likely that the plots will be developed over the next 5-10 years. The Design Codes are formatted to provide a higher degree of detail around the design aspirations which are detailed in the approved Design Framework for the site and they deliberately stop short of being overly prescriptive. They are not 'rules' that encourage uniformity or that stifle innovation or creativity but are furnished with examples of well designed places elsewhere to ensure the basics are right in evolving the physical fabric of the area.

Structure of Document

- 4.3** The Design Code document sets out the context through an appraisal of the site, its location within the town and in relation to surrounding areas and the history which is a big part of what makes Queens Quay a special place. Two dimensional 'regulating plans' are provided in the first section which allow developers and planning officers to easily locate where the provisions of the codes will apply and to express how the codes relate to plots and different character areas that have been identified across the site. Each additional section of the document expands on this by providing more detail around the expectations for the site, offering precedents images of building and landscape materials suitable for a waterfront environment. The images show examples of roofing materials and edging, windows, entrances and street and landscape materials for roads and pavements, boundary treatments, gates and planting that demonstrate examples of what is preferred and not preferred or what is deemed as being successful and what is seen to be less successful.
- 4.4** The document has been formatted to reflect the order in which a designer would be expected to approach the design development of a plot and will also assist Planning Officers in the assessment of future applications. The format of the Design Code document is as follows:
- The 'Context' section sets the scene in terms of historical significance, existing townscape, built form and public buildings, public transport access, the cycling network and the green network as an overview and it directs the code user to significant policy documents such as the Masterplan and the relevant planning consents already granted.
 - The Regulating Plans set out the site wide urban design considerations for the site. These diagrams are an easily relatable set of guidelines that demonstrate the key, strategic moves that

should be adhered to when a developer begins to design a plot or plots. They provide parcel subdivision, a transport and access diagram, density standards, and the hierarchy of the road and street networks, including acceptable parking standards/locations and a waste and recycling strategy.

The regulating plans are supported by a more detailed study which identifies 3 character zones:

- a Parkland character zone that runs parallel and adjacent to the linear park on the waterfront;
- an urban character zone that sits off the main vehicular route through the site;
- a Mews character zone that has its own character but sits comfortably between the Urban and Parkland zones.

- 4.5** The regulating plans go on to explain the locations of the different building typologies and the height of buildings which is linked to views/ vistas, landscape character and neighbourhood nodes. The section on 'Materials' describes the aspirations for quality materials across the buildings and the landscape which would be suitable to the marine environment on the Clyde, noting the link to heritage and the past. It advocates the importance of a simple and robust material palette using good architectural detailing to avoid staining and weathering issues. The materials section is extended to cover streetscape and materials that would be successful in the various character areas identified above.
- 4.6** The 'Character' zones are further expanded by identifying general development principles, carefully describing a typical street where the landscape and streetscape treatment will vary from a more urban approach beside the main vehicular route through the site to a parkland character beside the river. Every different type of space is considered; there are details of how the 'node' spaces (which are public spaces linking developments parcels together) should be treated in order to ensure their success as important hubs for community or social activities and of the preferred landscape character and treatment of back courts and parking courts that reduce the dominance of the car and put pedestrians and amenity first.
- 4.7** The document finishes with a series of 'Case Studies' illustrating successful examples of good design and places elsewhere with the intention to be used flexibly but to stimulate excellence and innovation.

Elected member workshop

- 4.8** An Elected Members workshop was held in December 2019 for the Design Codes. Member's comments included encouraging the use of colour and interest on the waterfront and alternative waste collection strategies.

Place and Design Panel

- 4.9** In January 2020 the Design Codes document was presented to the Place And Design Panel and the Panel were impressed by the quality of the Design Codes and felt that the deep thinking that had gone into the document really helped with the understanding of the site and what was appropriate for development there. The Panel agreed that the document would be a very useful resource that was straight forward to understand and use and it had the right balance of prescriptive design within them but would still allow for innovation. The Panel made further suggestions around the nature of the codes in relation to the mixed use areas of the masterplan and also suggested ways in which to ensure the landscaping is successful particularly in relation to the waterfront and the park that the consultants will take cognisance of in the development of the Codes.

Next Steps

- 4.10** The draft Design Codes will be published for consultation for a period of 6 weeks. An advert will be placed in the local press and the draft document will be made available on the internet and in libraries and to relevant organisations such as community councils, housebuilders, housing associations and relevant Council services. Following this, the consultation responses will be reported back to the Committee along with a finalised version of the Design Codes.
- 4.11** As part of the consultation, a workshop will be held whereby the usability of the Design Codes will be tested by both designers and planning officers.
- 4.12** The method of carbon reduction on the building fabric of future housing on Queens Quay is being considered. This would offer clarity over expected minimum building fabric performance if a development is connected to the District Heating network. This would form an annex to the Design Codes and would be used with the Design Codes by future housebuilders on the site.

5. People Implications

- 5.1** There are no personnel issues associated with this report as the work will be carried out within current workload of the Place and Design Officer.

6. Financial and Procurement Implications

- 6.1** There are no financial or procurement implications associated with this report as the consultants were appointed after a complaint tendering exercise and the budget has been taken from the existing Place and Design Panel budget.

7. Risk Analysis

- 7.1** The Design Codes will provide clear guidance to future developers of the site and will assist with the assessment of any future planning applications on the respective plots. This is intended to lead to a smoother and quicker application process resulting in quality development.

8. Equalities Impact Assessment (EIA)

- 8.1** An Equalities Impact Assessment is required to assess the impacts of the Design Codes on the existing and new populations in West Dunbartonshire and visitors who will use Queens Quay.
- 8.2** The Impact assessment work carried out during the development of this proposal has identified a range of positive impacts, especially for women, BME groups, disabled people and older people as well as children.

9 Environmental Sustainability

- 9.1** A Strategic Environmental Assessment is not required

10. Consultation

- 10.1** In early 2019 officers from the Services of Regeneration, Roads, Asset Management, Greenspace, Waste, Housing, Capital investment, Consultancy Services, Energy and Compliance, Environmental Health, The Access Panel, Scottish Green Network, Performance and Strategy attended a stakeholder's scoping workshop to discuss the main issues that the codes should address.

11. Strategic Assessment

- 11.1** The Design Codes document is considered to strongly align with all the strategic priorities; in particular improving the strength of and growing the local economy and investing in regeneration which takes account of the environment, sustainability and health and well being.

Peter Hessett
Strategic Lead - Regulatory
Date: 12th February 2020

Person to Contact: Pamela Clifford, Planning, Building Standards and Environmental Health Manager,
pamela.clifford@west-dunbarton.gov.uk
0141 951 7938

Ashley Mullen, Place and Design Officer
ashley.mullen@west-dunbarton.gov.uk
01389 737775

Appendices: Appendix 1 – Queens Quay Design Codes Draft (2nd December 2019)

Background Papers: Equality Impact Assessment

Wards Affected: Ward 6 Clydebank Waterfront



QUEENS QUAY DESIGN CODES

Draft 02.12.2019

INTRODUCTION

These Design Codes set out structured guidance for designers and developers. They provide a structured approach for West Dunbartonshire Council to assess Planning Applications.

The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision.

CONTEXT

CLYDEBANK CONTEXT

The site is in a prominent location on the northern bank of the River Clyde and is strategically important for Clydebank's ongoing regeneration.

The new masterplan for Queen's Quay, will see the redevelopment of a significant area of Clydebank's townscape on the site of what was originally John Brown shipyards. This is an opportunity to change the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry.



Queens Quay Masterplan Site



At its peak employing over 10,000 people, John Brown Shipyards was one of the most commercially successful shipyards in the world, becoming famous internationally for its shipbuilding quality.

Following the yard's closure in 2001, only a few features remain. Dominating the site is the Titan Crane, now an icon for the town's future regeneration.

There is also an opportunity to change the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry

Existing Townscape

The once intact townscape of Clydebank has been badly eroded through damage during the Blitz and inappropriate demolition and redevelopment during the period 1960–2000, leaving few key landmarks remaining.

The result is a lack of defined 'street edge', large areas of 'waste-land', no 'celebration of corners' and a lack of height eroding the urban townscape, all resulting in a lack of a sense of place.

Although there are some housing areas adjacent to the site, these are relatively isolated and the new development offers an opportunity to knit these in to a new urban fabric.

Existing Public Buildings

The development site is well -located for pedestrian access to a wide range of public facilities at Clydebank Town Centre.

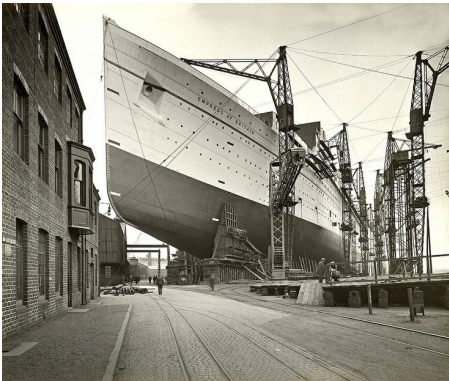
- **Within 5 minute (400metre) walking distance:** Leisure centre, Town Hall, Clydebank College, plus the proposed Clydebank transport Hub which includes improvements to the existing train station
- **Within 10 minute walking distance:** Golden Jubilee National Hospital, Shopping Centre. Local Primary Schools, Rothesay Dock industrial area

Public Transport and Cycling

Clydebank Train station and Chalmers Street Bus Station sit within 5 minutes walking distance of the site sits. There are bus stops along Glasgow/ Dumbarton Road.

Existing Green Network

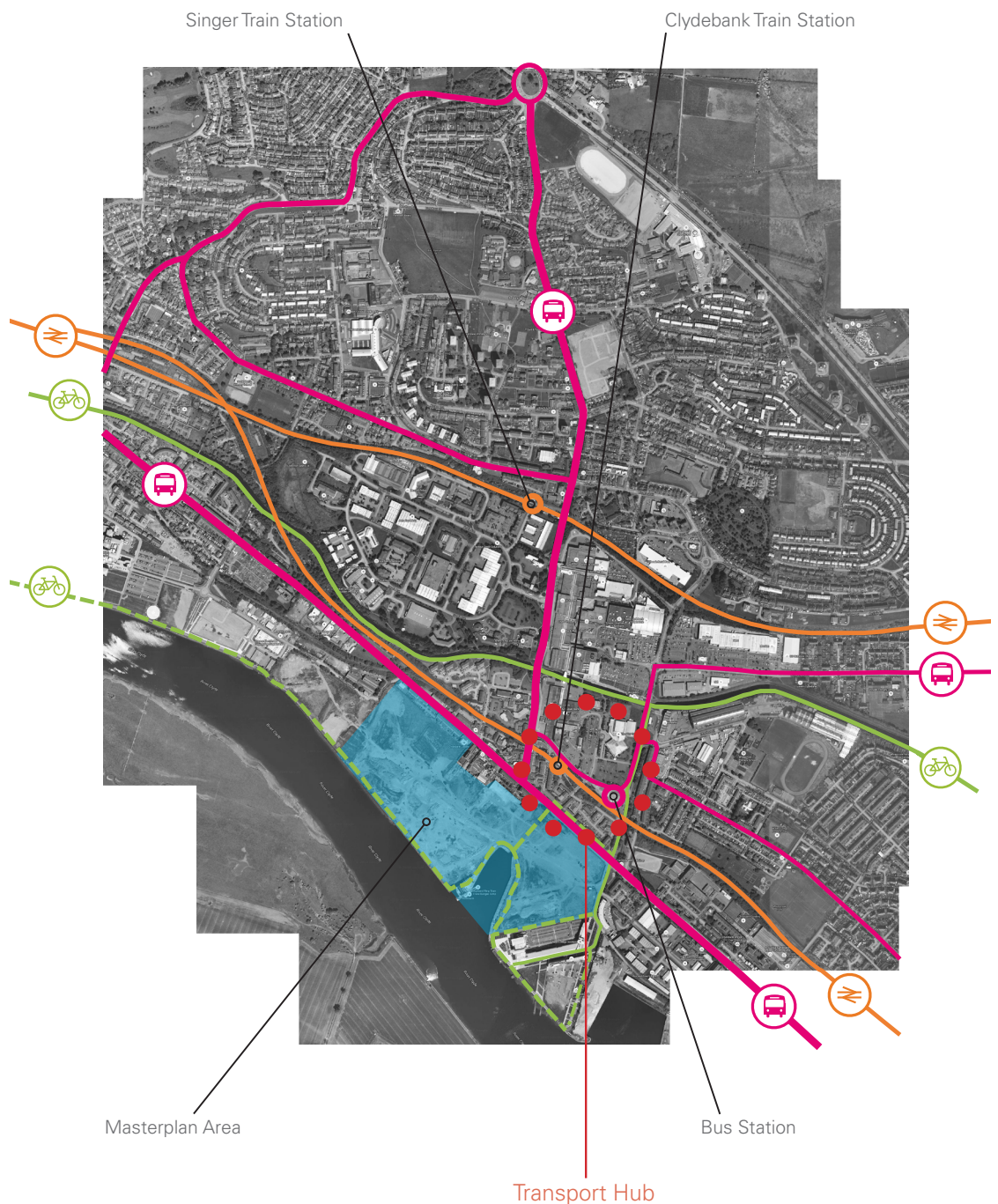
Greenspaces within Clydebank are isolated and the town centre lacks open greenspace of a significant useable scale. The riverside walkway west of the site is not accessible to the public, but the aim in the long term is to link it to the Queens Quay area to form a continuous riverside walkway.



Public Transport Hub

Connecting Clydebank is a transformational project which seeks to revitalise the civic core of Clydebank; forming essential links with the ongoing Queen's Quay development, the present town centre and the many active and public transport routes into the area. It aims to link different strands of investment to reinforce Clydebank as a destination.

The project originated from a successful Charrette process in 2015 (refer Clydebank Charrette Action Plan) and has been progressing in consultation with funding partner Sustrans following the successful funding award from the Sustrans Community Links Programme in 2016.



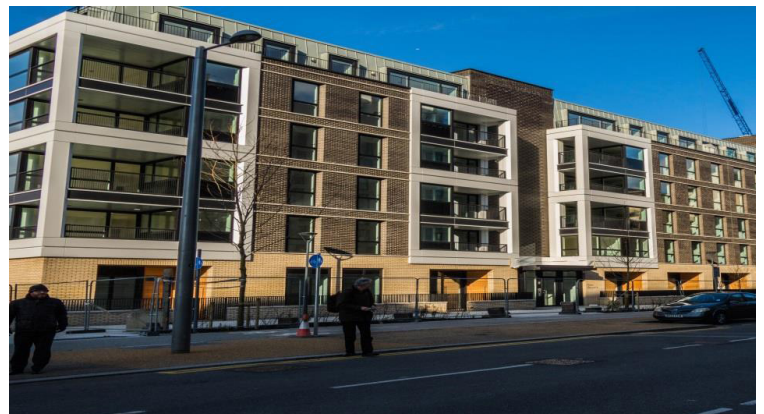
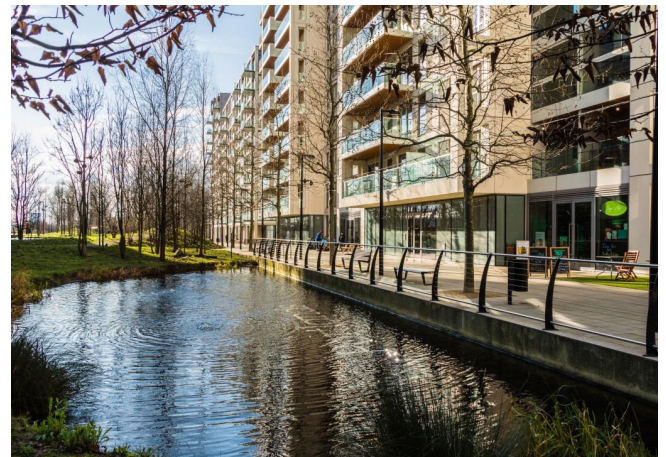
OBJECTIVES AND ASPIRATIONS

Clydebank has a rich and colourful past as an important industrial heartland. This history is a big part of what makes it a special place. In recent years Queens Quay has been an industrial wasteland, however it is now being transformed into an exciting new neighbourhood.

West Dunbartonshire Council's Local Development Plan and supporting documentation highlight Queen's Quay as a location where high quality design and attention to place making is an essential component of new development in order that Queen's Quay becomes a destination neighbourhood.

Following visits to Kings Cross and Stratford with Elected Members, the Planning Authority have set out an Aspirational Vision for Queens Quay based upon:

- Using remaining elements of the shipyard to help define the place
- Using materials that complement the industrial character but can also integrate new development.
- Minimal change to the robust character of the existing dockyard quay structures will be subject to minimal change.
- Massing, scale and detailing which complement the historic, large scale structures on the site and its previous industrial character



West Dunbartonshire Council planning and regeneration officers visited recent exemplar developments to identify precedent projects that demonstrate high design quality

THE MASTERPLAN

A masterplan has been prepared for the development area by Clydeside Regeneration and Dawn Group in partnership with West Dunbartonshire Council. The masterplan's objectives are:

- **Creating a quality public realm:** An enhanced urban (rather than suburban) environment with safe pedestrian connections to the town centre
- **Improving connections:** Linking to other development sites along the water's edge and creating better pedestrian and cycle routes to the Titan Crane and Riverside Park.
- **Creating a special place:** Developing the site as a key destination with opportunities to enhance cultural offerings in the area while creating a sustainable community.
- **Regeneration:** Changing the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry.

The current masterplan and associated planning consents include detailed proposals for infrastructure, together with some areas of public realm and greenspace:



The Fitting-Out Basin: The existing concrete deck structure will be made safe for day to day public use as well as events; it will be the location for a seating areas and information display as well as HMS Hood Park - a local play park and garden.



Riverside Park: West of the basin, a greener approach is proposed and new homes will look out over the river towards countryside. The river edge will provide a wildlife habitat; behind this the Clyde Path (a continuous 4 metre wide shared path/cycleway) sits within a pattern of subdivisions derived from the layout of the berths and buildings that previously occupied the site. This park subtly accommodates slopes to raise the development plots up above flood level. All residential streets will have path connections to this area which will be lit to enable safe use at night, as part of the cycle network.



Main Street: The proposed Main Street takes the line of the previous plate gantry, which ran between the platers shed and the fitting-out basin. A strongly defined tree-lined street is the main traffic route, with generous verge zones behind the footways providing space for an avenue of street trees

To calm traffic speed, the six metre carriageway of the Main Street is broken into sections corresponding to future junctions with residential side-streets. In-carriageway bus stops with raised kerbs are provided at intervals on either side of the street.

There are two greenspaces within the main street corridor. A Pocket Park contains play, lawn and garden seating spaces, and at Cable Depot Road a Community Orchard has a selection of fruit trees significant to the region, planted on a five metre grid amongst meadow with seating and mown grass paths.

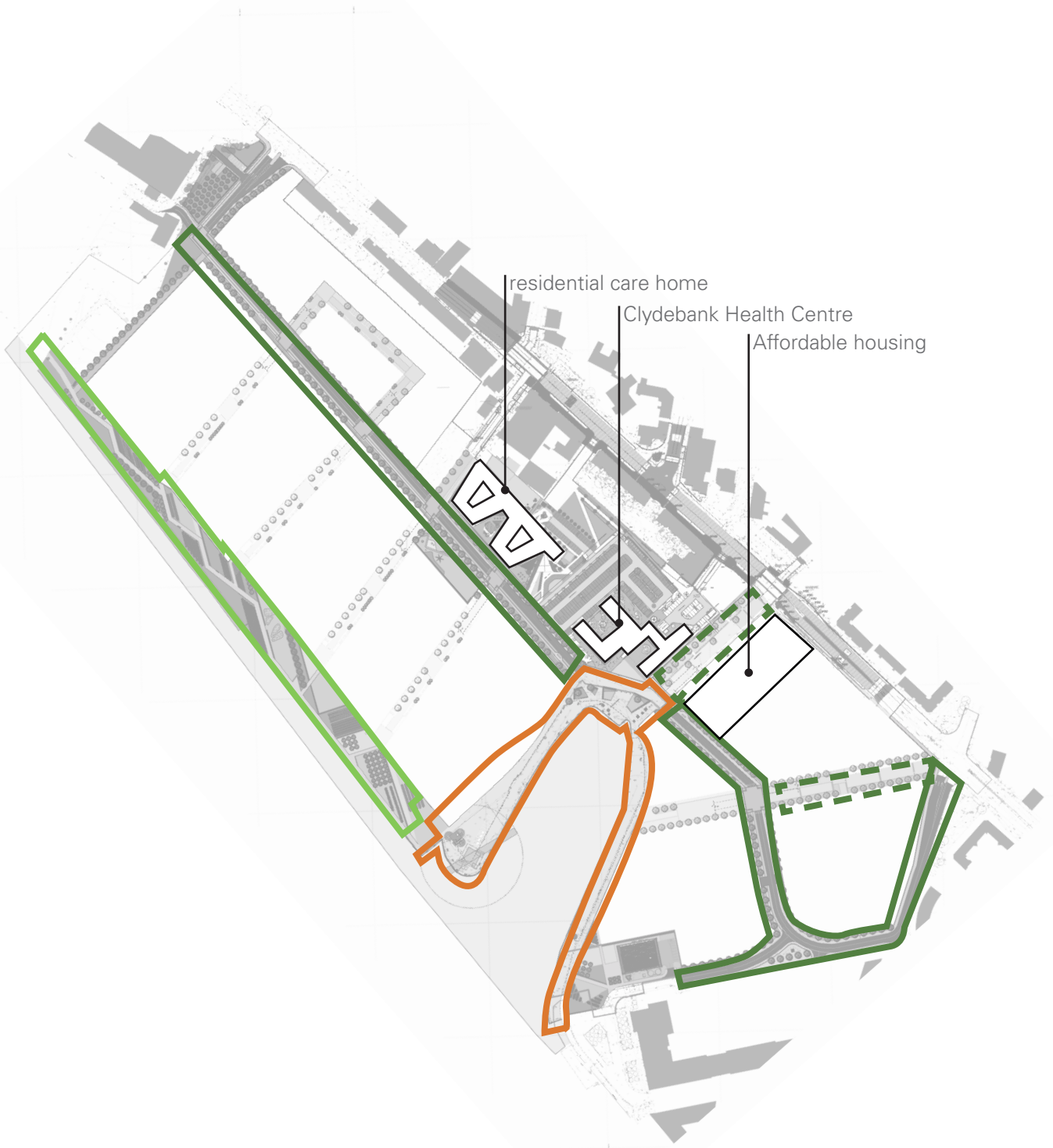


Boulevards: Spatial connection between the basin and Dumbarton Road is provided by two broad avenues which intersect at the basin. They are aligned axially with the Titan Crane and provide strong visual and spatial connections to and from Dumbarton Road, bridging between the town and its reclaimed, revitalised waterfront.

These linear spaces are intended to be busy and dynamic, with strong built frontages and flexible shared space to enable commercial life. Trees line the axial view to emphasize a green connection to the river. Materials will co-ordinate with the current Council proposals for public realm on Dumbarton Road.

Neither will connect onto Dumbarton Road for vehicles, but instead will provide pedestrian and cycle connections with occasional use by emergency vehicles.

DIAGRAM: Masterplan components



West Dunbartonshire Council Planning Policy and Guidance

West Dunbartonshire Council's Local Development Plan and supporting documentation highlight Queen's Quay as a location where high quality design and attention to place making is an essential component of new development in order that Queen's Quay becomes a destination neighbourhood.

Following visits to Kings Cross and Stratford in London with Elected Members, the Planning Authority have set out an Aspirational Vision for Queens Quay:

- Using remaining elements of the shipyard to help define the place, such as service gallery and mooring bollards
- Using materials that complement the industrial character but can also integrate new development.
- The robust character of the existing dockyard quay structures will be retained, with minimal change.
- Providing active travel routes which connect Queens Quay to the town centre
- Engaging with existing streets and creating a new urban street pattern
- A new health centre will be a busy focus
- An area wide heating system
- Improved green connections with boulevards and linear greenspace, pocket parks and edible landscapes.

The Queens Quay design codes build on these aims and objectives, setting out design parameters for development of the remaining plots.

Relevant planning consents

The following planning approvals have been granted for the Queens Quay site, to date:

- A - DC16/240 Spine Road: formation of new spine road (including junction alterations at Cart Street, Alisa Road and Cable Depot Road) formation of public realm around the basin and river edge (including hard and soft landscaping and linear park) and landscaping strategy for full masterplan site . Infrastructure is ongoing
- C - DC16/244 Care Home: a two /three storey home for life and a day care centre
- F - DC17/231 Energy Centre: the largest ambitious district heating system in Scotland powered by heat pumps which will extract water from the River Clyde
- G - DC18/033 Connecting Clydebank: new controlled and uncontrolled crossings, improved footpaths, better cycle access, public realm and road works to Dumbarton Road, Glasgow Road and Hall Street in order to improve the public realm connecting the main shopping area to the north of the site and the new Queens Quay development to the south.
- B - DC18/057 Clydebank Health Centre: this will act as a catalyst for significant change, bringing a wide range of health services together - a focal point and landmark feature.
- D - DC18/272 Mixed Use Development on Titan Boulevard (part of plot 5 which is excluded from the scope of the design codes).
- E - DC18/275 Titan Boulevard: new street, public space, landscaping and associated street furniture.

THE DESIGN CODES

THE DESIGN CODES

Scope

The Queens Quay design codes build upon West Dunbartonshire Council's Aspirational Vision, setting out the parameters for development of the remaining plots.

These Design Codes set out structured guidance for designers and developers. They provide a structured approach for West Dunbartonshire Council to assess Planning Applications.





Although design codes guide the development in three dimensions, a two dimensional Regulating Plans are included with them, to enable code users to locate where the provisions of the code will apply, and to express how the codes relates to plots and different character areas.

The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision. Some design components are already described in detail in the masterplan, or consents have already been granted for development. These are described in more detail in the following pages.

It is anticipated that development at Queens Quay will be phased with development ongoing for 5 to 10 years. As a consequence, the overall development site will be parcelled into smaller development plots (*see following page for details*). In some locations, each plot plus an adjacent residential road will be parcelled together.

Site	Residential road	Greenspace	Use
1	no	no	mixed
2	no	no	mixed
3	no	no	leisure
4&5	no	no	mixed
7	yes	no	residential
8	no	yes	residential
9	yes	yes	residential
10	yes	yes	residential
11	yes	yes	residential
12	yes	yes	mixed

Parcel subdivisions

-  plot boundary - housing
-  plot boundary - mixed use
-  greenspace associated with each plot
-  residential access associated with each plot



Transport and Access Hierarchy

Access hierarchy outwith Design Code areas



Dumbarton Road



Masterplanned vehicular access routes - currently under construction



Pedestrian and cycle route with occasional access for emergency vehicles



Pedestrian and cycle route only

Access hierarchy within Design Code areas



Residential access road - vehicles, pedestrians and cyclists



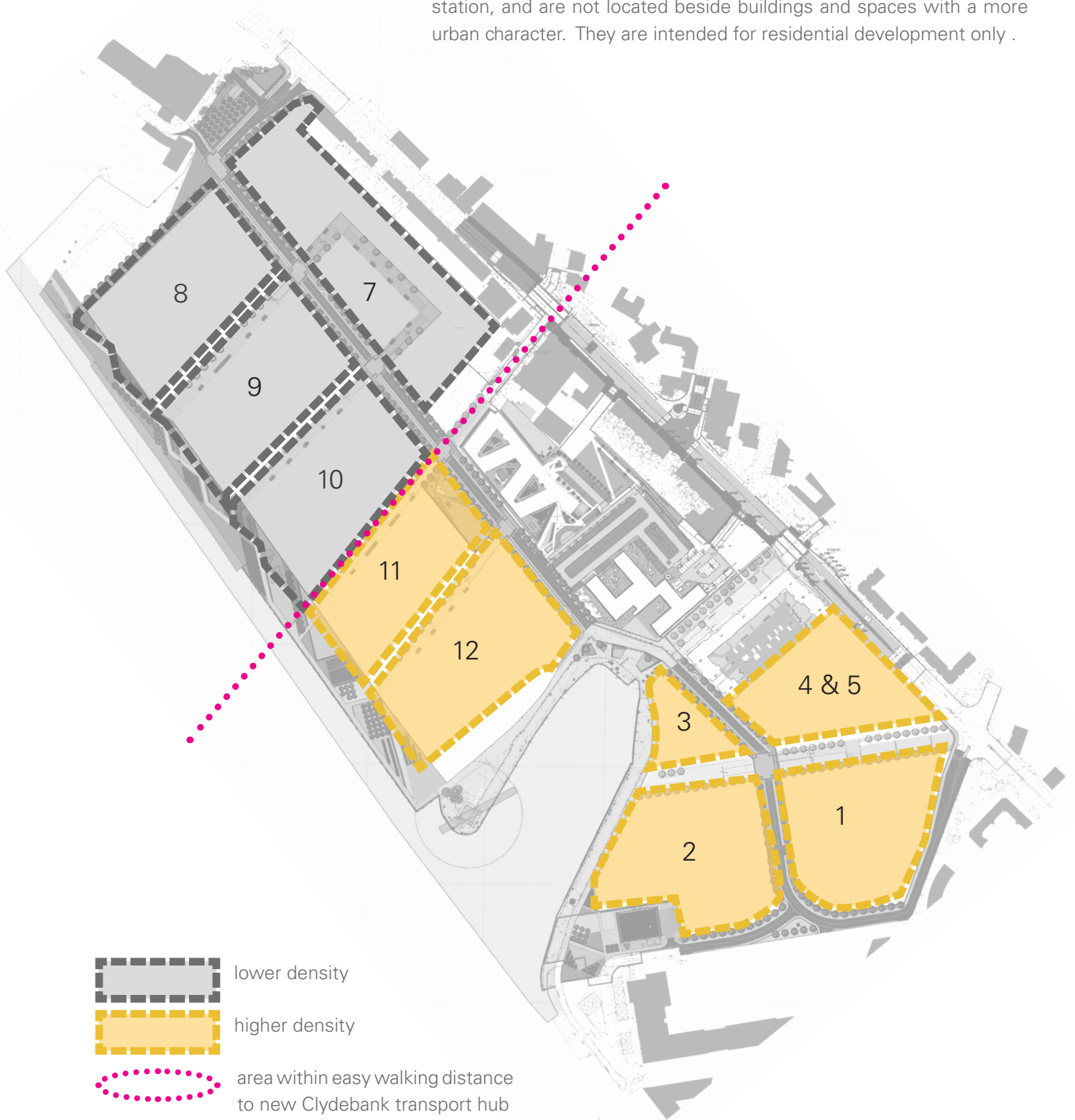
Residential pedestrian and cycle links with occasional access for emergency vehicles



Density

There is a natural division within Queens Quay. The plots shaded in yellow are closer to the train station and public transport hubs. They are also located in an area where a higher density is more appropriate because of the scale of development beside them and the proposed mix of uses.

The plots shaded in grey are further from the transport hubs and train station, and are not located beside buildings and spaces with a more urban character. They are intended for residential development only.



Road hierarchy & street network

In addition to compliance with local and national design guidance, the Design Codes define a user hierarchy for different types of streets - based on their significance in terms of both place and movement for pedestrians, cycles and vehicles. Applying the hierarchy will help to create a series of attractive, sociable urban spaces as well as controlling traffic and promoting the attractiveness of walking, cycling and the use of public transport. The principles of the street hierarchy are mandatory, although their exact alignment and design will be fixed through further planning consents, on a plot by plot basis.

New residential roads within Design Codes areas link the Main Street to development plots; these form a route to off-street parking areas and are also the location for on-street parking.

Routes running through the centre of the riverside plots are envisaged as fully pedestrianised routes primarily for the residents of Queens Quay. Other routes are predominantly for pedestrians and cyclists, and will be only occasionally used for emergency traffic, delivery and maintenance.

Table 2; street design and geometries (design code areas only)

	Residential access roads	Residential pedestrian & cycle links	Residential pedestrian & cycle links + emergency access
Design speed			
Target speed for traffic	20 mph	Not applicable	10 mph
Street dimensions			
Minimum carriageway width	4.8 metres	2.5 metres	3.8 metres
On street parking	Yes either or both sides, 2.6m width	Not applicable	Not applicable
Footway	1.8m to either side	Not applicable	Not applicable
Verge	Yes – same width as on street parking zone	Not applicable	Not applicable
Direct plot access for traffic	No	Not applicable	Not applicable
Vehicular access to parking courts	Yes	Not applicable	Not applicable
Public transport			
Pedestrian access to transport hub	Within 400 metres walking distance	Within 400 metres walking distance	Not applicable
Bus access	No (Main Street only)	No (Main Street only)	Not applicable
Street design details			
Traffic calming	Yes (linear route)	No	
Vehicle swept paths	Yes – all vehicles	No	Yes – emergency vehicles only
Junction sightlines	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction spacing	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction radius	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only

Parking

Queens Quay is very close to the improved Clydebank Transport Hub, so there is an opportunity to create a new urban area which prioritises movement on foot and by cycle, rather than by car. The Design Code therefore stipulates maximum parking numbers for individual development plots.

There is a natural division within Queens Quay where parking density changes. Plots which are closer to the train station and public transport hub also contain elements of primary building frontage to Dumbarton Road and the Basin. This is an area where proximity to the new transport hub, plus a higher dwelling density will support a more urban character. This can potentially be undermined by larger areas of parking so a maximum parking ratio of 70% is to be provided in these areas.

It is likely that car ownership will be higher for plots which are further from the transport hubs and train station. In these locations the density is lower. Because of this, they can achieve higher parking levels. A maximum parking ratio of 100% is to be provided in these areas. (Parking allowances should include garage spaces).

The Building Standards require 5% of parking spaces to be 'accessible'. These spaces require a wider parking bay and must be close to the building entrance.

Car club spaces should be provided to offset lower numbers of parking spaces. (WDC to confirm requirements)

Electric charging points should be provided (WDC to confirm requirements)

Visitor parking for cars, small vans and motorcycles should generally use shared, public on-street parking.

Cycle storage should comply with Cycling by Design by Transport Scotland which sets standards for cycle parking

Parking ratios



70% parking



area within easy walking distance to new Clydebank transport hub

Plot No	Parking ratio PER RESIDENTIAL UNIT	Cycle storage PER RESIDENTIAL UNIT
1	70%	2 spaces
2	70%	2 spaces
3	70%	2 spaces
4&5	70%	2 spaces
7	100%	1 space
8	100%	1 space
9	100%	1 space
10	100%	1 space
11	70%	2 spaces
12	70%	2 spaces



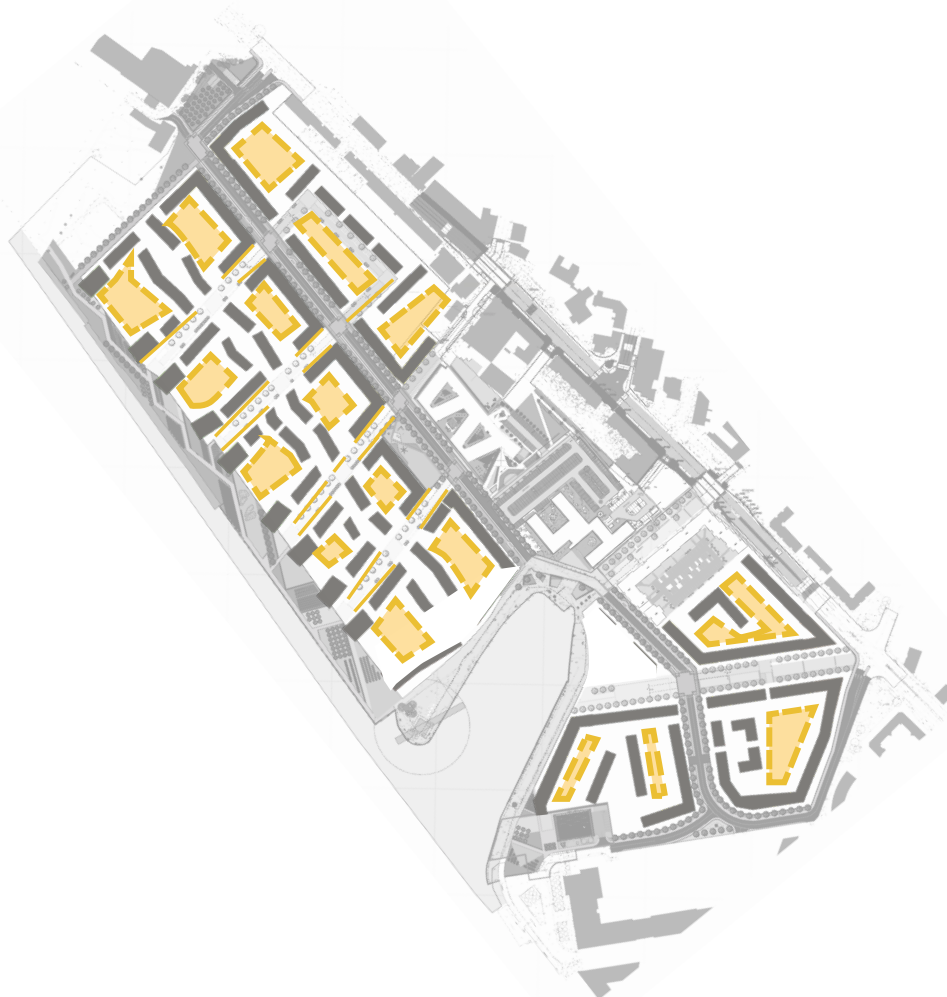
Parking character



parking court



on street parking



Waste and recycling

The preference is for a circular route for cleansing vehicles through rear courts; this will entail a coordinated approach between developers, which should be included as a condition for planning consents. This approach will allow bins to be wheeled to the rear of properties.

There should be no more than 7m of a 'pull distance' for the bins from the location of the bin stores to the vehicle. Each household will require 3no. 240 litre bins (grey/brown/blue) emptied on a fortnightly cycle.

At flats, this figure is translated into 1100litre bins on a ratio of 3/1 waste/recycling bins. For example, 13 flats would have 4 no. 1100 litre bins (3 waste, 1 recycling). These should be stored in a communal collection area.

Where properties are higher rise (for example 7 stories) an external bin store should be provided rather than an internal space. Where blocks are mixed use, this store should provide separate space for commercial uses.

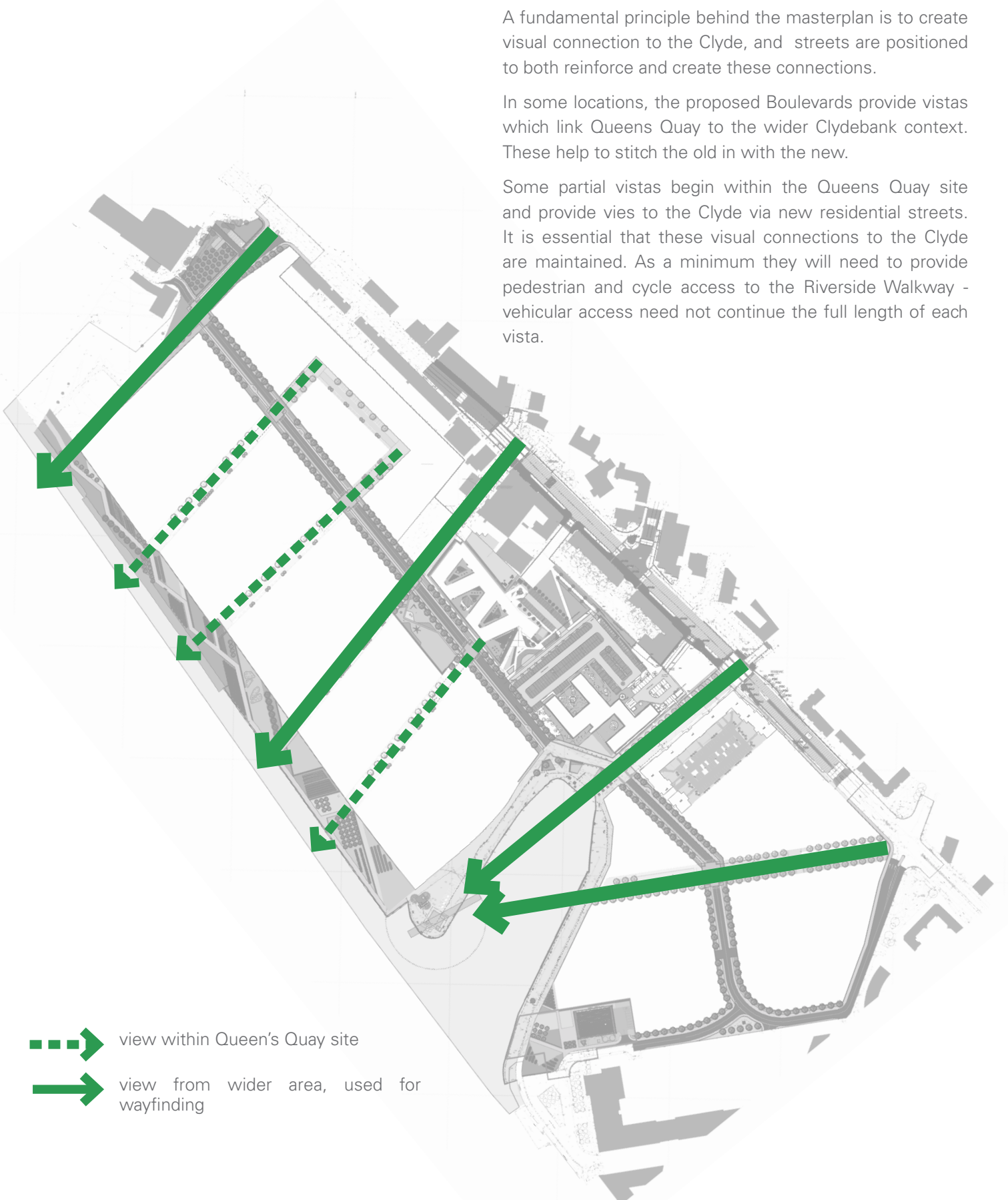
Cleansing department vehicle routes through parking courtyards

Views and vistas

A fundamental principle behind the masterplan is to create visual connection to the Clyde, and streets are positioned to both reinforce and create these connections.








In some locations, the proposed Boulevards provide vistas which link Queens Quay to the wider Clydebanks context. These help to stitch the old in with the new.

Some partial vistas begin within the Queens Quay site and provide views to the Clyde via new residential streets. It is essential that these visual connections to the Clyde are maintained. As a minimum they will need to provide pedestrian and cycle access to the Riverside Walkway - vehicular access need not continue the full length of each vista.

- 
- The map illustrates the Queens Quay site with various streets and buildings. Green arrows indicate visual connections. Dashed green arrows represent views within the site, while solid green arrows represent views from the wider area used for wayfinding. The arrows point towards the Clyde, reinforcing the visual connection mentioned in the text.
- view within Queen's Quay site
 - view from wider area, used for wayfinding

Landscape Character Zone plan

The site has been divided into 8 landscape character areas/ typologies. The landscape requirements for each character area are set out in the design codes; these include key information on relationships and layout, precedents, furniture, materials and planting.

-  **Streets - Urban:** a formal civic character relating to the scale of the wider street and town houses on the main route through Queens Quay
-  **Streets - Parkland:** residential areas that reflect the soft green landscape of the riverside park
-  **Nodes:** areas of enhanced public realm and landscaping
-  **Mews:** intimate, smaller scale housing along a pedestrian route running east west through residential blocks
-  **Parking Courts:** informal but functional back of house spaces: communal parking, servicing and bin storage areas
-  **Backcourts:** less dense parking courts incorporating communal parking, servicing and bin storage areas set within amenity and green space
-  **Park Interface area:** where new development links to and borders the expansive riverside park

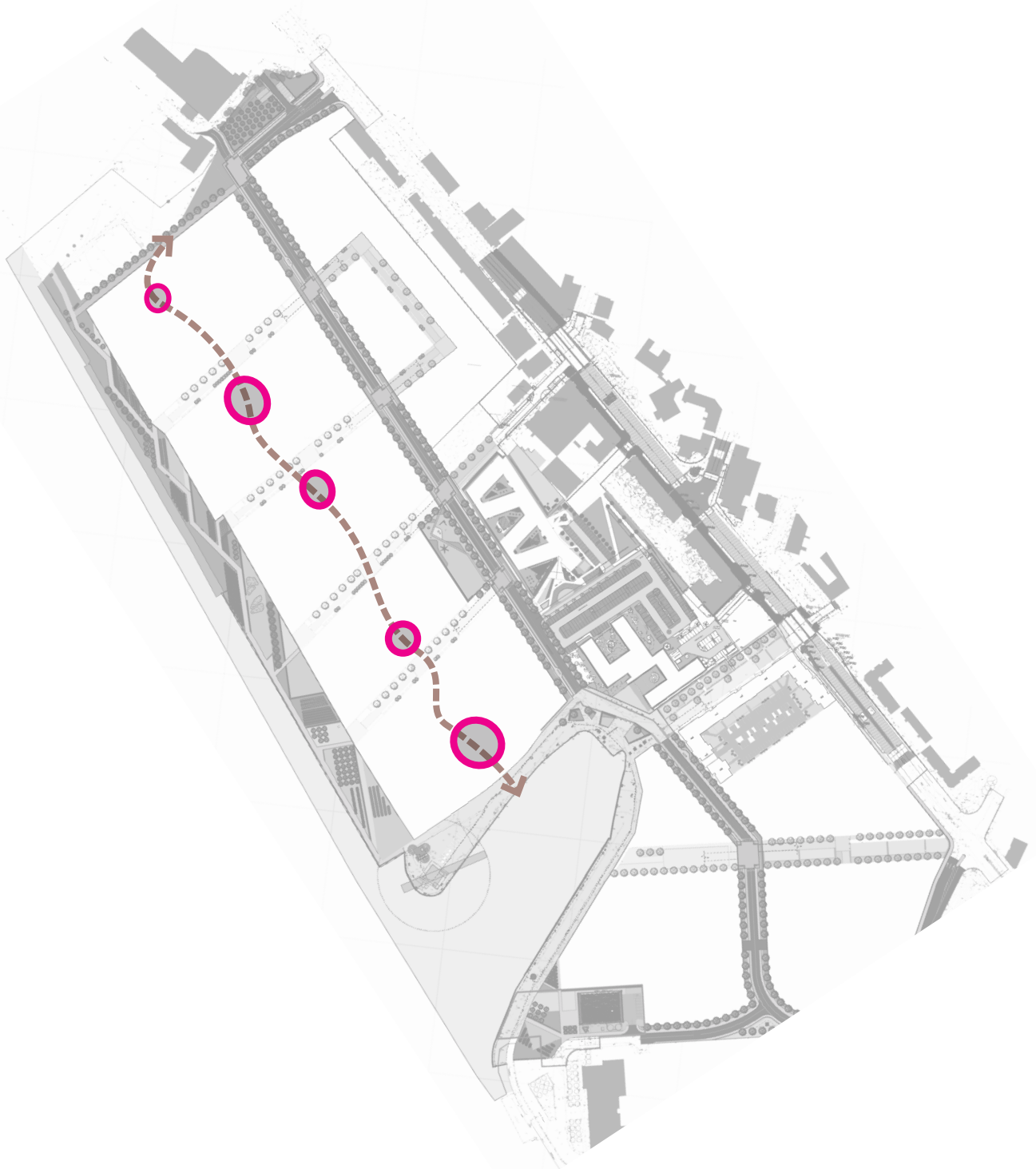


Hierarchy of Nodes

Along the route running through the centre of the riverside plots, it is proposed to create neighbourhood nodes. At these spaces the street should open up to create an area of public space that is sheltered from the prevailing winds and weather.

Each node should be activated by building frontage and soft landscaping. The pedestrian should be dominant in these spaces.

These nodes increase in significance, not in size, as they move towards the basin. Any proposed developments should take cognisance of this and demonstrate to the Planning Authority how each node relates to the other.



Aggregated Diagrams

This drawing collates and displays all of the previous diagrams. The attributes of each diagram come together to work as a whole, creating a mesh of urban design parameters for Queens Quay.

It is essential that these parameters are met by each development plot to ensure conceptual continuity through the Queens Quay area. This will then establish Queens Quay as a valued and desirable location to live, work and socialise.



Location of building typologies

- Houses
- Mews
- 3 storey flats
- 4 storey flats
- 4 storey flats (above commercial units)
- 6 storey flats
- 8 storey flats
- mixed use (landmark development)



Typologies	Building height	Frontage continuity	Privacy distances* from frontage to footpath	Commercial/retail use at ground floor
Development facing on to Fitting Out Basin	4 to 8 stories	Yes	Not required where there is a retail/commercial use at ground floor, otherwise minimum 1.5 m	Preferred
Boulevards with some commercial uses (Mixed use typologies)	4 or 6 stories	Yes	Not required where there is a retail/commercial use at ground floor, otherwise minimum 1.5 m	Preferred
Main Street	2 to 3 stories	Yes	Minimum 1.5 metres	No
River frontage	3 - 8 stories	No – pavilion type blocks to allow views to river	Minimum 1.5 metres	No
Residential streets	2-3 stories	Preferred	Minimum 1.5 metres	No
Mews houses facing onto residential pedestrian and cycle links	2 stories	Preferred	Minimum 1.5 metres	No

MATERIALITY

History + Community

The redevelopment of Queens Quay will contribute to the wider regeneration of Clydebank. As such it is important to be sensitive to the cultural significance of the sites history.

The Queens Quay site was formerly the John Brown Shipyards. Before that it was owned by the Thomson brothers. Prior to that it was agricultural land owned by the Cochno's and was miles from Glasgow's urban borders. The rapid development of Clydebank is directly related to the ship building activities. People moved to the area in droves for work in the yards. As such it is important that this part of the history of Clydebank is referenced in the new buildings of Queens Quay.

Likewise it is to be recognised that the shipyards are also a substantial source of ill health in the community. Legacy issues around exposure to dust and chemicals including asbestosis still prevail from the shipyards labour force, who still reside in the area. The decline of the shipyards is also linked to the social deprivation in the area.

As such the proposals should have a **nod to the past** but must take cognisance of the circumstances this caused, and so **look to the future**. Consequently literal interpretations or representations of industry are not felt to be appropriate. New designs should be aspirational and point towards a healthier Clydebank, with a new urban heart.



Historical Site Remnants

Based on a 1918 Map of John Brown Shipyards the original workshops have been shown dashed on the adjacent diagram. Entrance Gates to the shipyard sat at the ends of Hall st and Wallace street. What remains today of historical significance includes the Titan Crane, the quay wall, the Town Hall, Library and the Baths. While the culturally important buildings are sandstone, the majority of buildings in the shipyard were brick. As such it is felt that a predominantly brick character is important for Queens Quay.

Beyond brick, it is clear that metal is of core significance to the site. Primarily steel, as this was the main product from the foundry. This was used to form the ships hulls, mechanics and structures. The shipyard and possibly even Clydebank, would not have existed without the foundry.

Secondary in importance to steel are copper, zinc (galvanising) and brass.

This historical and contextual analysis provides the basis for a robust material selection for the site. These materials are in wide use today, however when applied as a material palette they contain an embedded link to the social history of Clydebank.

The future designers of Queens Quay must take care to apply these materials sensitively, so that the buildings and streets exhibit a nod to the past but demonstrate a clear view on an aspirational future.



Clydebank Town Hall



Facade of Old Baths



Titan Crane and
Queens Quay Basin



Robustness

To ensure the longevity of Queens Quay, a detailed analysis was carried out to identify materials used in the locality and how they had performed over time. It is known that the weather conditions on the banks of the Clyde differ to inner city locations, through prevailing winds and a greater exposure.

Following the analysis the materials used were categorised into two broad categories, successful materials and less successful materials.

The images below describe materials that are felt to be less successful in this climate.

Smooth panels, particularly in large format had not weathered well. Smooth tiling, brick or terracotta in a stack bond pattern do not weather well. Timber cladding weathers unevenly depending on prevailing winds. Laminated panels were noted to exhibit signs of delaminating.

As such designers should not propose this type of material or detailing at Queens Quay.

Less Successful



Robustness

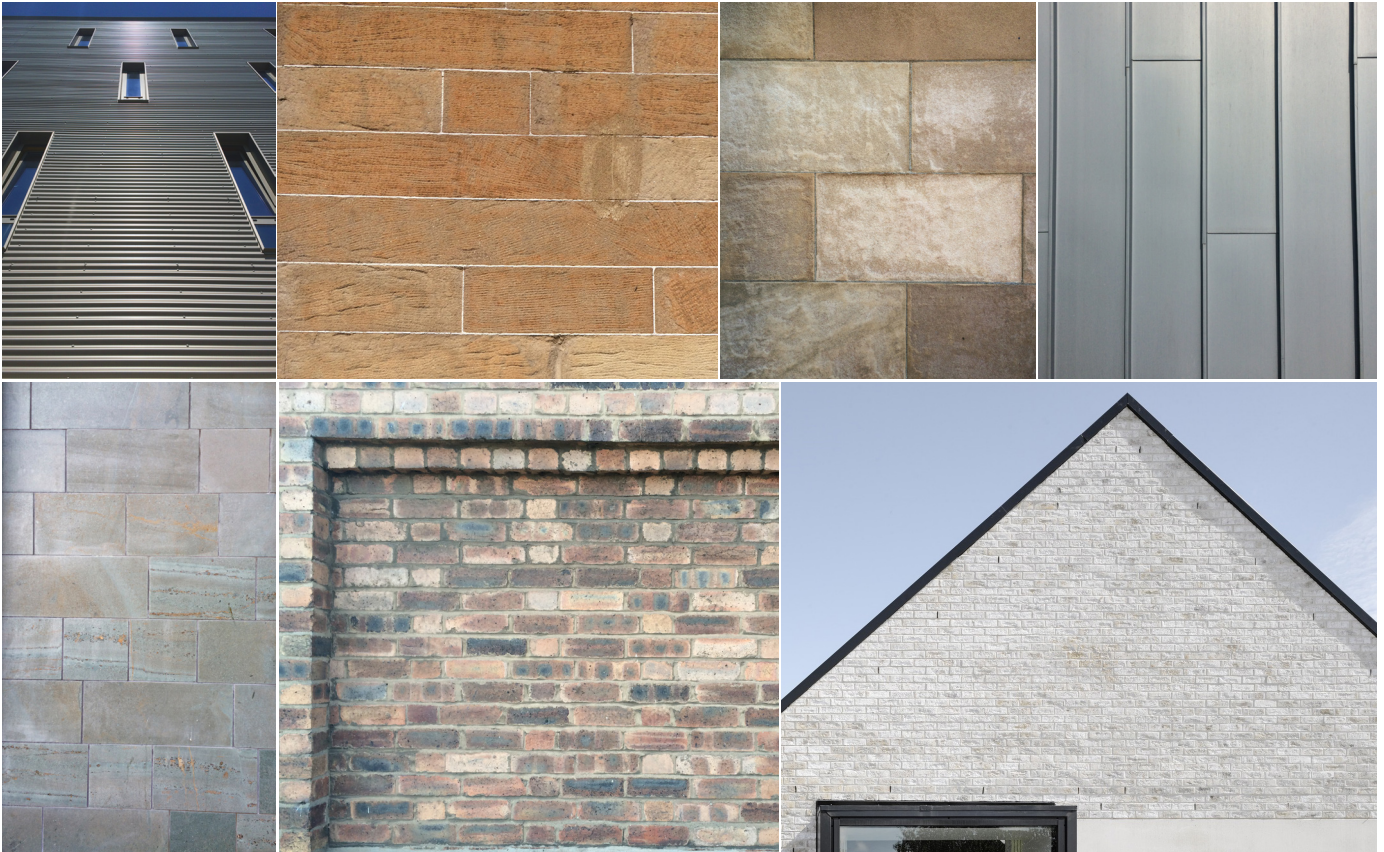
Materials that were successful exhibited a number of common traits. These should be utilised in the material selection and detailing at Queens Quay.

Surface texture is an important feature, traditional style brick laid in stretcher bond, bricks multi with a variety of colour and depth of tone, traditional stonework and ribbed metal, or standing seam panels all aged well in Clydebank.

Cropped eaves lines are also important to achieve a consistent weathering appearance across the facade. The successful examples were evenly exposed to the elements and so were able to weather naturally.



Material Successes



Materials Palette for Queens Quay

Simplicity

For a clean and consistent aesthetic across the site, no more than two cladding materials should be used on a building. Given the scale of the Queens Quay development some variety is encouraged within a pre-determined palette of materials. It is envisaged that the buildings may be articulated differently and may vary in colouration between sites, provided they draw from the material palette noted below.

Planned Weathering

The local climate at Queens Quay can be severe, with strong prevailing winds and driving rain. Detailing on all buildings must be robust to ensure that rainwater is cast off eaves, copes and flashings.

Consideration should be given to the intended long term aesthetic of materials. For example, oil canning (wrinkling of the otherwise flat panel caused by heat expansion) of standing seam metal cladding must be considered and shown to be part of a planned aesthetic for the building. Brick detailing, such as corbelling, can provide features or areas of interest, however consideration needs to be given to the possibility of staining where water runs off or is directed.

A Materials Palette for Queens Quay

For the reasons stated previously, a palette of materials is provided below. These are deemed to be favourable by the Planning Authority at Queens Quay. The proposed materials of any building submitted to the Planning Authority in relation to Queens Quay should demonstrate compliance with this Materials Palette.

The Materials

Preferred materials are listed on the following pages.

Built Form - Materials Palette

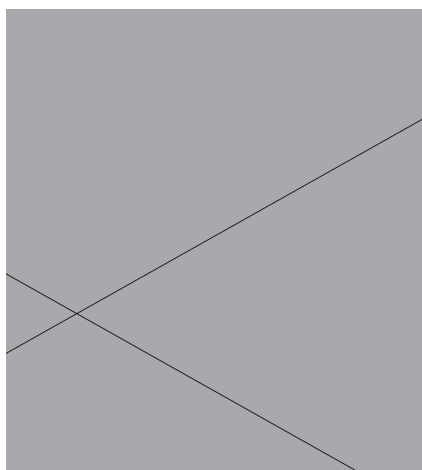
Cladding Material 01 - Brick

Traditional style clay stock brick, laid in stretcher bond, specified as a multi for depth of tone and variety of colouration. Colour matched mortar is preferred. Technical specification of the selected bricks should be appropriate to the local climate. Performance of moisture absorption, durability and active soluble salts must be considered.

Buff / Cream brick is favoured. All proposed bricks should tone with one on another on a single building. A contrasting Grey / Black brick could be considered to highlight key features. All bricks should tone with one another between sites.



Preferred
Buff / Cream
Traditional
Multi Clay
Stock Brick



Preferred
Buff / Cream
Traditional
Multi Clay
Stock Brick

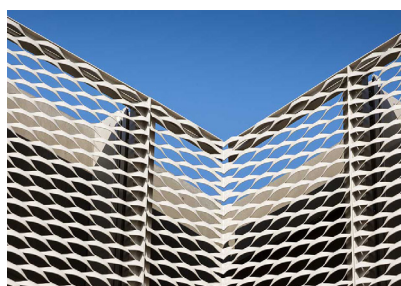


Not Preferred
Red / Brown
/ Orange /
Yellow / Blue
Bricks
Wirecut and
or Extruded

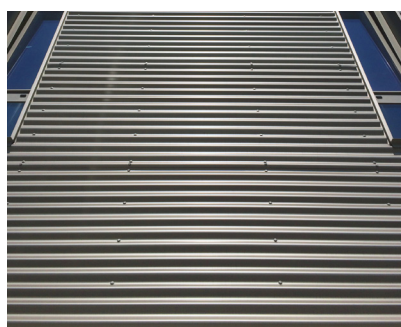
Cladding Material 02 - Metal

Steel - Corten is favoured due to its suitability to the environmental conditions. Care should be exercised to ensure that the Corten is pre-weathered off site, or detailed to weather in-situ without staining adjacent surfaces.

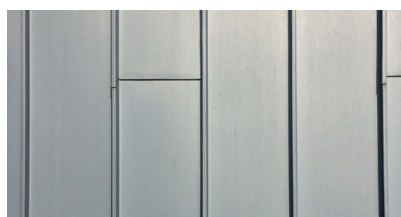
Copper, zinc and brass can be used however they should be considered in the context of surrounding buildings and streetscape. Smooth jointing is not to be used, standing seam or ribbed panels are preferred.



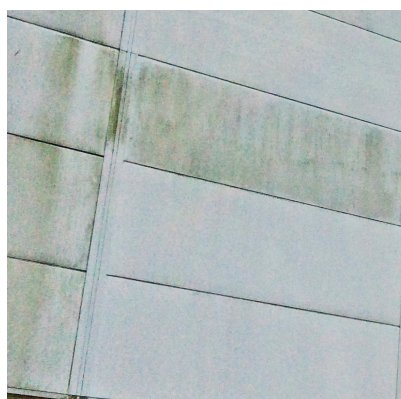
Preferred
Expanded Metal
Mesh
Anodised
Aluminium or a
Metallic Finish



Preferred
Profiled Anodised
Metal Cladding
Anodised Aluminium
or a Metallic Finish
Powder Coat.



Preferred
Profiled Anodised
Metal Cladding - Zinc,
Copper or Brass.

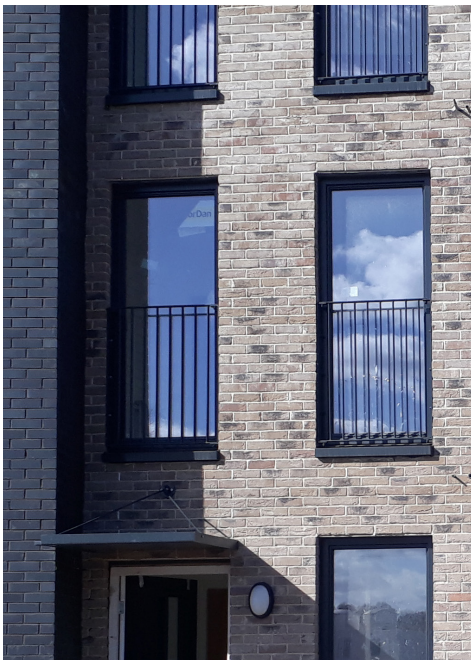


Not Preferred
Flat Smooth
Preformed Panels

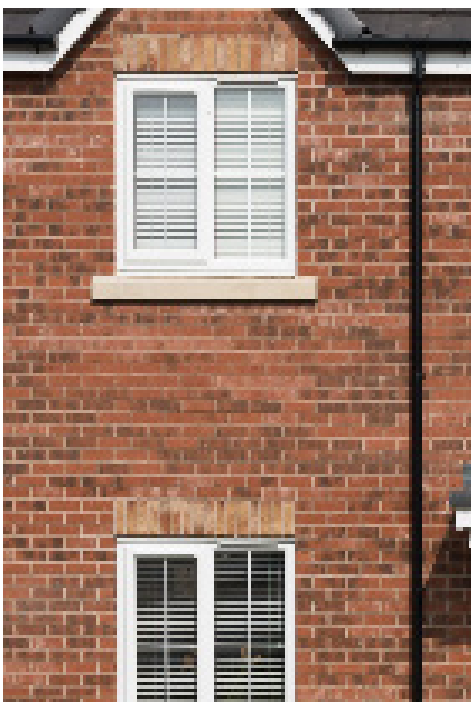
Built Form Materials Palette

Windows

ll windows should be provided in a dark grey finish, preferably polyester powder coated aluminium externally. Cills and balustrades should complement the window, window colour, cladding and aperture.



Preferred
Dark Grey PPC
Aluminium
Windows
Could be
aluminium clad
timber or all
metal window
frames.
Colour
galvanised
balustrades to
match window.
Colour matched
precast or
aluminium cills.



Not Preferred
White PVC
Windows

Roofs

The roofs on all buildings should be specified to be in keeping with the material palette noted previously. Where roofs are tiled consideration should be given to the eaves and verge details.

Verge Details - The clean lines of a polyester powder coated aluminium flashings are preferred to proprietary dry verge systems. A skew wall detail, a parapet detail, or clipped eaves with a fibre cement slate roof finish could achieve this.

Eaves Detail - Raked box eaves are preferred to traditional boxed eaves details. Clipped eaves are preferred to projecting eaves, unless a clear case is made for the aesthetic approach.



Preferred
Skew Wall



Preferred
Parapet



Not Preferred
Traditional
Box Eaves



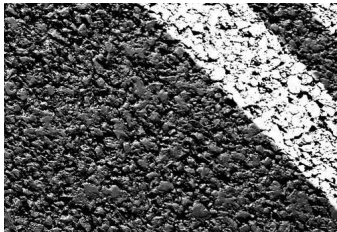
Not Preferred
Proprietary
Dry Verge

Streets - Urban landscape and streetscape materials

Furniture & Materiality

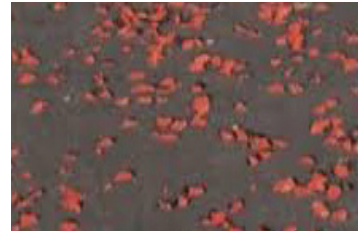
Hard surfaces in the Urban character area should be of same quality and finish as the materials delivered on the Main Avenue as part of the Queens Quay masterplan.

Road carriageways and pavements - hot rolled asphalt finish (no aggregate chippings preferred)



Preferred

hot rolled
asphalt finish



Not preferred

coloured
chippings

Boundary treatments - 1,200mm high brick walls to match buildings; 'brick on end' copes (noting requirement for specials/ resin bonded bricks with overhangs for protection)



Preferred

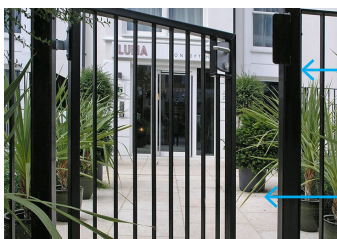
brick copes
brick type
& detailing
to match
building



Not preferred

railings or
fences that
create visually
permeable
boundary
treatments

Gates to entrances - 1,200mm high steel gates - powder coated steel gates in dark grey to match building windows and metalwork; vertical bar style, no adornment



Preferred

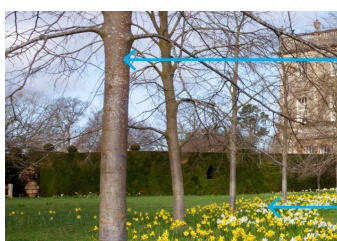
colour to match
metal work on
facades(grey);
vertical flat bar/
simple design



Not preferred

no adornment

Planting; Street trees - Lime (Tilia) Avenue tree planting to match the boulevard - (Tillia cordata 'Greenspire' preferred - trees to be planted in tree pits with load bearing tree soil, at max 12m



Preferred

avenue trees
(max 12m
spacings)

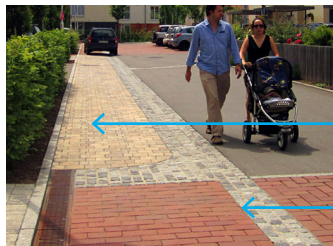
emergent bulbs
in grass verges

Streets - Parkland landscape and streetscape materials

Furniture & Materiality

The south end of the residential streets (beyond the node spaces) relate most strongly to the waterside park should be much greener and open in character - with a focus on the pedestrian and public spaces instead of the vehicle servicing and access.

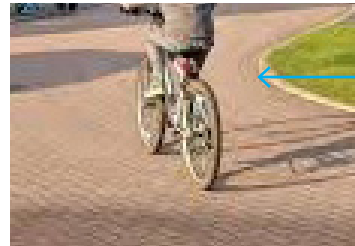
Road carriageways and pavements - vehicular carriageways should be in bitmac with parking bays and pavements in 'textured' precast concrete setts in silver-grey tones - creating safe spaces for pedestrians and a softer feel. If appropriate to the drainage strategy these could also be permeable areas.



Preferred

parking bays and pedestrian areas in textured setts

different textures define routes and spaces



Not preferred

single surface type for all road / pavement / parking areas

no definition of routes and spaces

Boundaries must be defined - but done so with hedges and planting (minimum 900mm high) - not by wall or fences. Boundaries should not be left undefined or left open



Preferred

boundaries defined by hedges and planting



Not preferred

open boundaries to front gardens

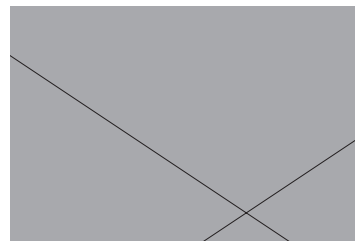
large areas of lawn

The general feel should be soft and leafy as if the housing is in the park



Preferred

trees planted in groups with shrubs/ground cover below
visitor parking located within streetscape in blocks of 3 spaces



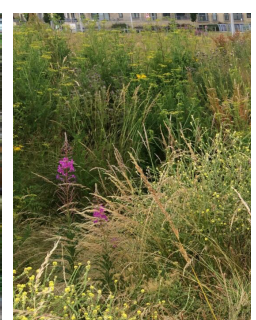
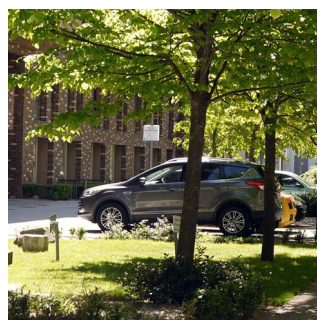
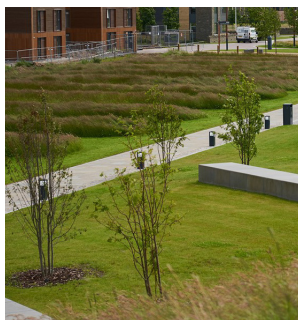
Not preferred

incutillage parking spaces

Planting

The planting to the Parkland character area should include:

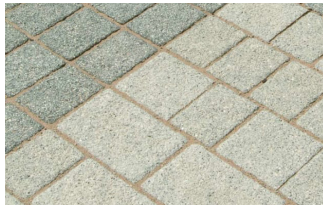
- blocks of native shrubs and grasses taking inspiration from the planting palette and overall structure as the park
- informal clusters of native trees (groups of 3-7)
- front gardens to include grass and a native tree in each front garden
- a native mixed species hedge (or beech) to the boundary (coastal hardy varieties)



Mews - landscape and streetscape materials

Furniture & Materiality The furniture and materiality to the pedestrian 'mews' must reflect a more tactile and human scale environment, with playful elements and textures used to define public/semi public/private zones.

Pedestrian environment - The mews should be detailed in small format 'textured', exposed aggregate, pre-cast concrete setts in silver-grey tones.



Preferred

'textured' setts in silver grey



Not preferred

red mono block laid herringbone

Boundary treatments - there should be no boundary to front gardens - instead buffer planting strips created with opportunities for seating / personalisation (minimum 2m wide)



Preferred

planting forms buffer to windows & area to personalise seating opportunities
no boundary



Not preferred

too narrow to buffer windows or provide opportunities for personalisation

Furniture within the social spaces along the mews should be made from timber and precast concrete and integrate with the design of the greenspace.



Preferred

robust materials

integrated with space design/landform



Not preferred

generic design

free standing bench - not integrated with planting

Incidental spaces for play (not play areas) should be incorporated within the street greenspace



Preferred

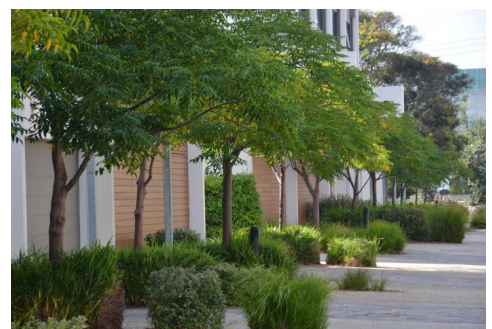
objects for play integrated into the streetscape



Not preferred

standard catalogue play equipment, rubber play surfacing, fenced off areas

Preferred planting - Clusters/ groups of small native street trees - to hold in spaces and edges. Appropriate species would be Rowan, Birch or Cherry.



Nodes - landscape and streetscape materials

Nodes should take on the furniture and materials of the area within which they are located. The general feel should be civic and like a small plaza or pocket park rather than a street junction. Feature areas (at building thresholds, around seats, under trees, furniture and greenspaces) should be surfaced in a smooth, porous alternative material such as a resin bound gravel.



Preferred

feature seating areas and routes defined by greenspace and planting



Not preferred

wide open spaces

seating not located in spaces



Preferred

brick boundary walls to use same brick type as adjacent buildings to enclose back gardens on corners (walls to be 1800mm height)



Not preferred

timber fences as boundaries on residential streets



Preferred

seating arranged around edge of space
greenspace and trees



Not preferred

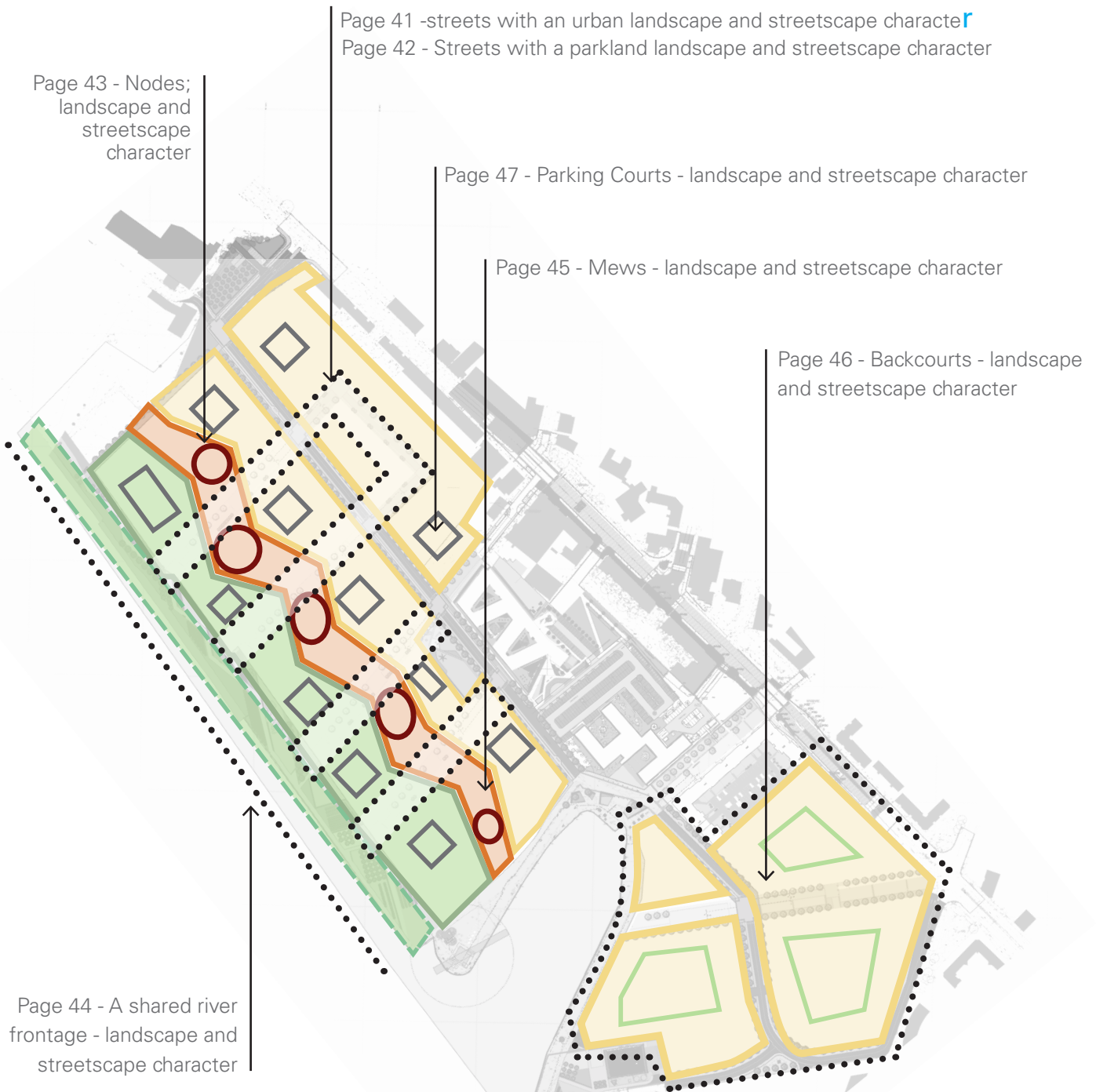
standard road junction design

no usable greenspace or trees

CHARACTER

Character Zone Overview

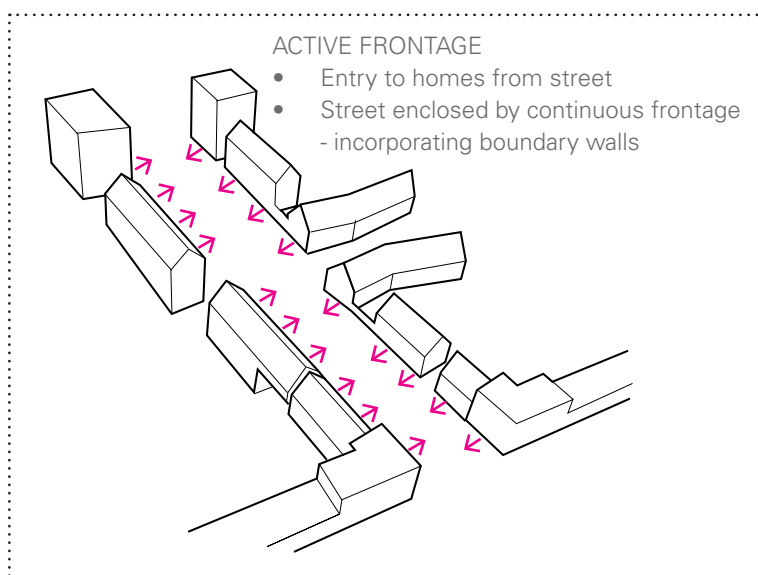
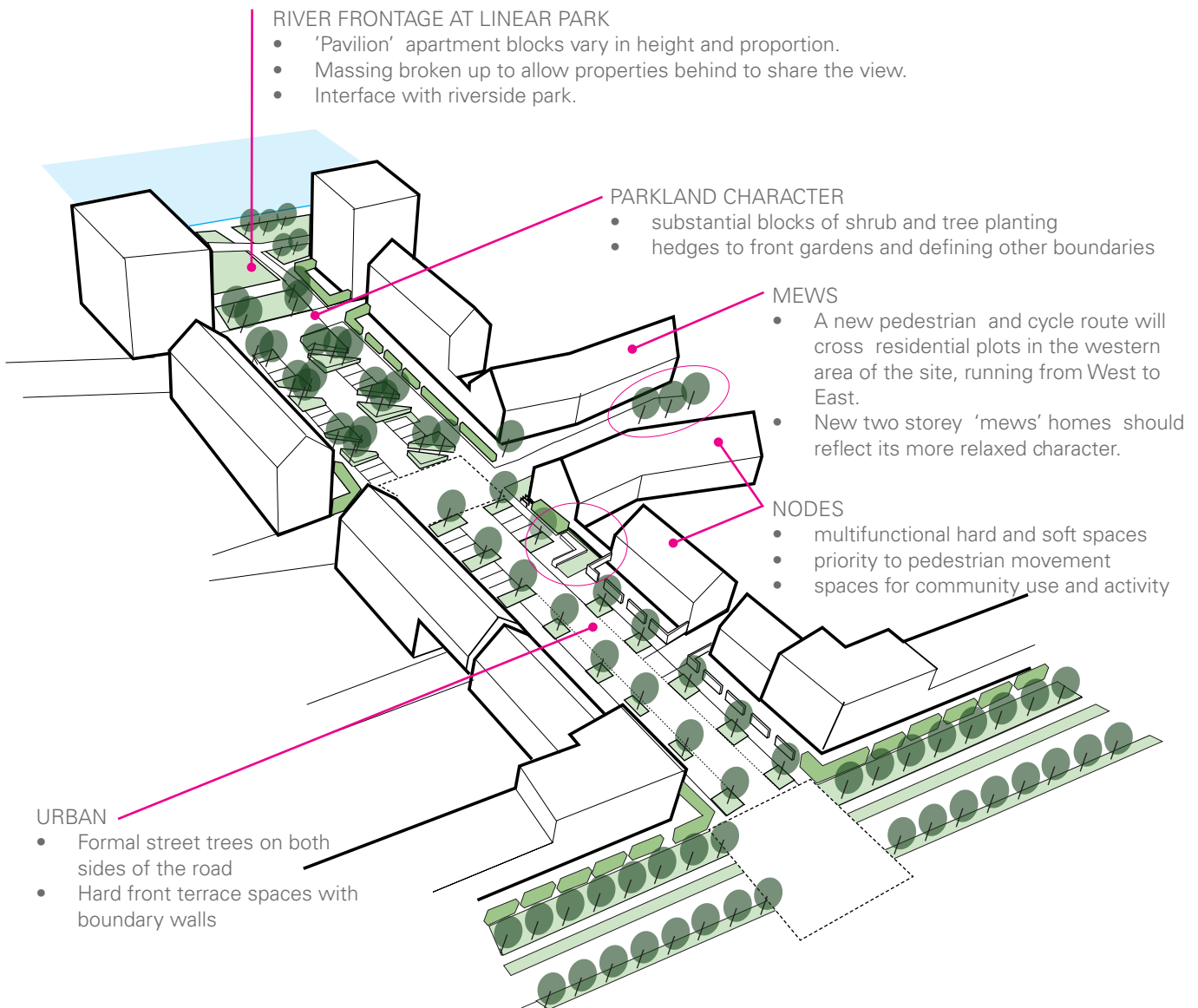
The site has been divided into 8 landscape character areas/ typologies. The landscape requirements for each character area are set out in the design codes; the following pages illustrate these in more detail and explain the interfaces between the character areas as shown below



- Streets - Urban:** a formal civic character relating to the scale of the wider street and town houses on the main route through Queens Quay
- Streets - Parkland:** residential areas that reflect the soft green landscape of the riverside park
- Nodes:** areas of enhanced public realm and landscaping
- Mews:** intimate, smaller scale housing along a pedestrian route running east west through residential blocks
- Parking Courts:** informal but functional back of house spaces: communal parking, servicing and bin storage areas
- Backcourts:** less dense parking courts incorporating communal parking, servicing and bin storage areas set within amenity and green space
- Park Interface area:** where new development links to and borders the expansive riverside park

General development principles - overview

The diagram below shows a typical street where the landscape and streetscape treatment will vary from a more urban approach beside Main Street to a parkland character beside the river. Refer to the diagram on Page 22 for the locations of each landscape/streetscape character type

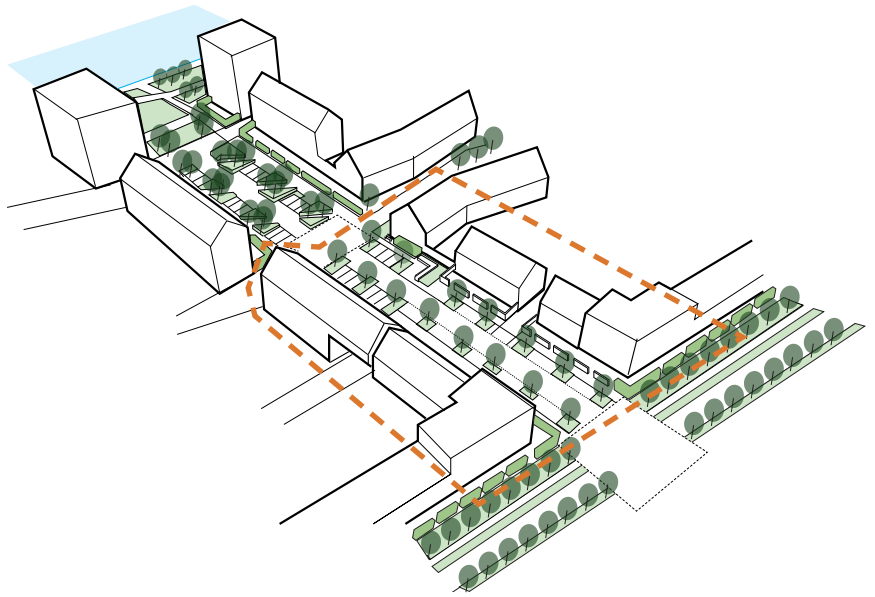


Streets with an urban landscape and streetscape character

Refer to diagram on page 22 for the location of streets with urban landscape character.

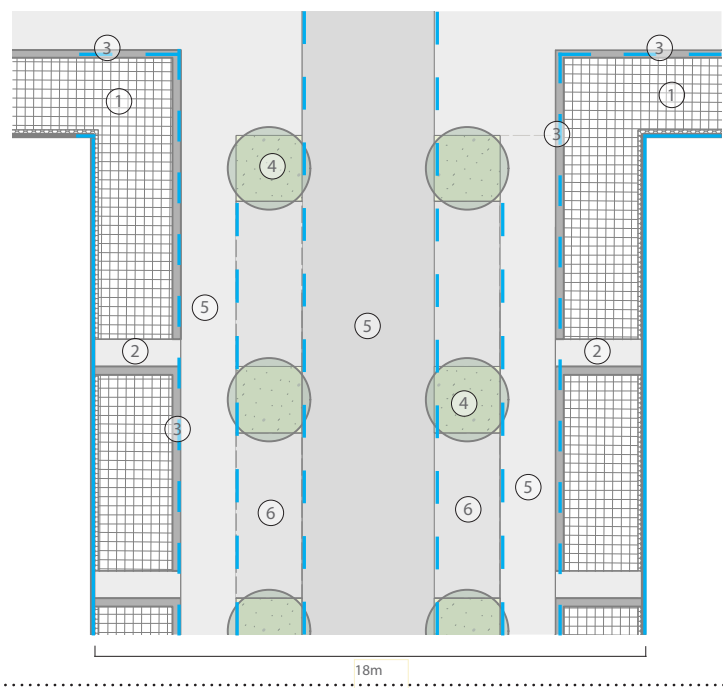
These areas have a formal civic character which relates to the scale of the wider street, flats and town-houses on the new Main Avenue and Dumbarton Road. These urban areas have formal planting and hard landscape materials.

Detailing should be crisp and rigorously geometric. Materials reflect the palette for Main Avenue within the existing masterplan, and buildings and external features should use the same materials eg. brick boundary walls and metalwork.



Typical street plan & cross section

3.1m 2m 2.5m 4.8m 2.5m 2m 3.1m
to tie in with Main Avenue

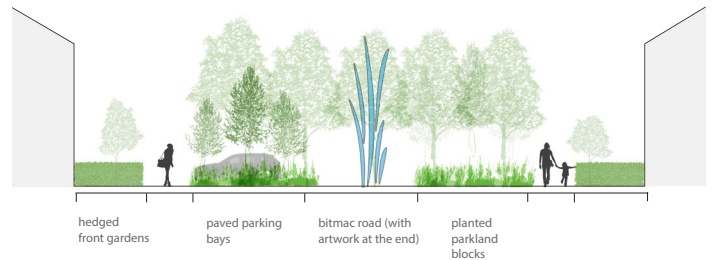
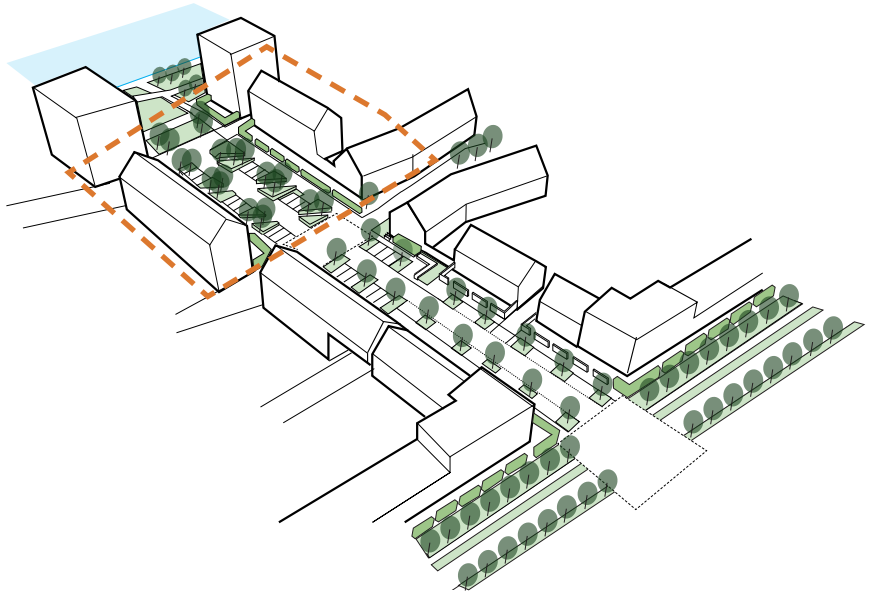


- ① Hard surfaced front courts with clearly defined thresholds
- ② Front entrances to all dwellings direct from the street
- ③ Brick boundary wall with brick cope and steel gates (1,200mm high)
- ④ Formal street trees (semi mature size) in large tree pits with grass and emergent bulbs beneath (max 12m centres)
- ⑤ Bitmac carriageway, parking bays and footways
- ⑥ Parallel visitor parking on both sides of road (max 2 spaces in a row before break)

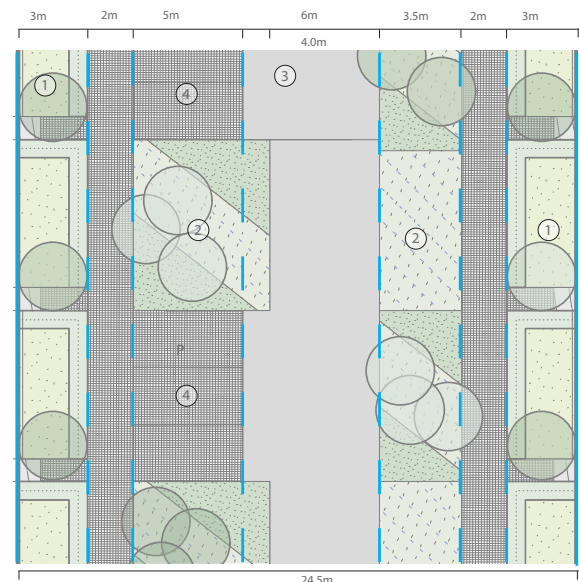
Streets with a parkland landscape and streetscape character

Refer to diagram on page 22 for the location of streets with parkland landscape character.

In this area, houses and blocks of flats feel as if they are set in the riverside park. The character is green and leafy with a palette of native plants and an informal feel. Visual and physical links to the park and the river are retained. Planting should have a native coastal palette, paving is less formal and nose-in visitor parking clusters are broken up by swathes of shrubs and grasses that reflect the park character. Careful attention should be given to creating an appropriate micro climate given the exposed nature of the site.



Typical street plan & cross section

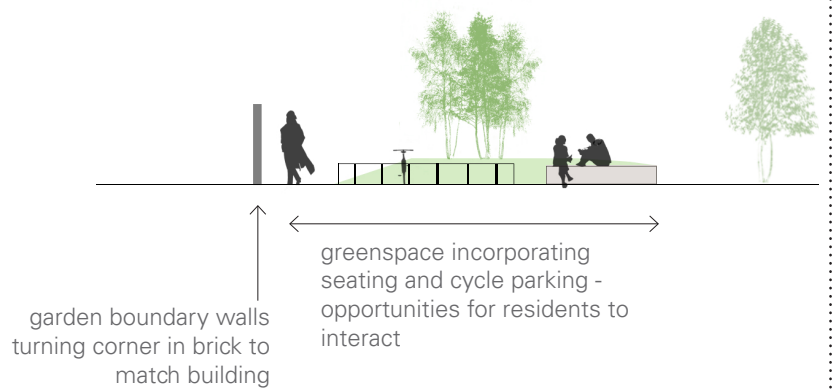
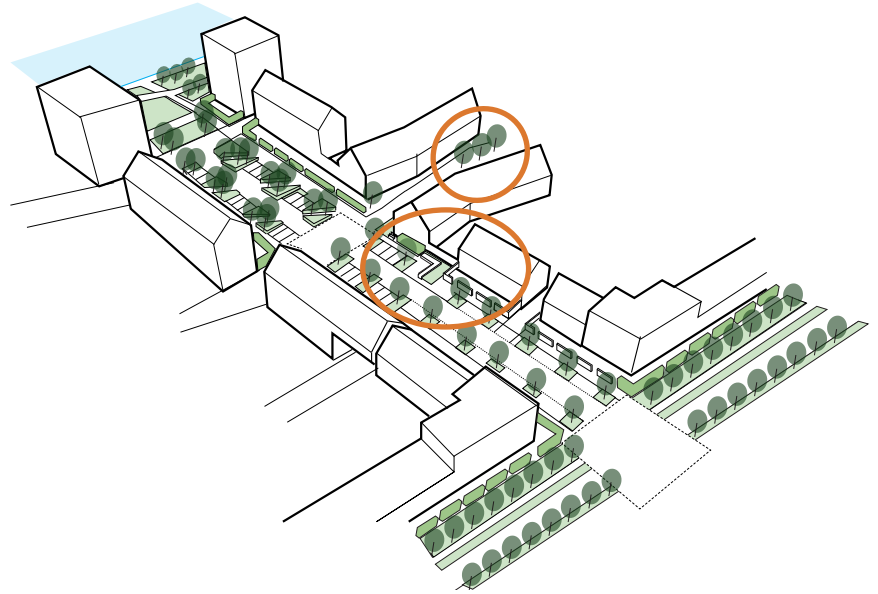


- ① Front gardens defined by a hedge and entrances marked with a tree
- ② Large scale parkland planted blocks with native shrubs and trees to extend park into housing areas
- ③ Bitmac carriageway, small unit PC paving for parking and paths between planting
- ④ Visitor parking provided as nose in spaces in blocks of 3 between planting

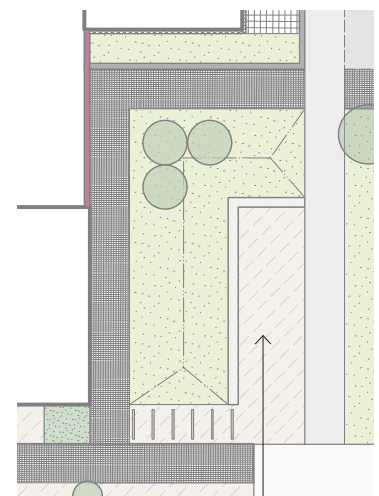
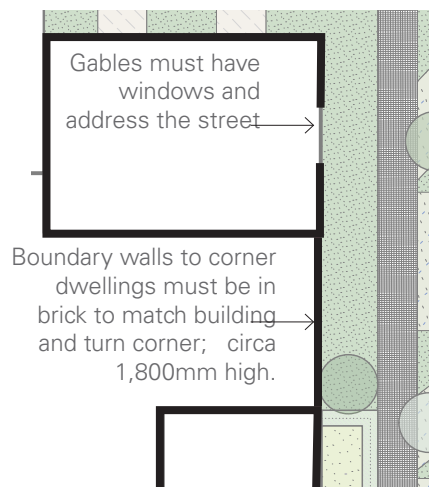
Nodes - landscape and streetscape character

Nodes are intended as important hubs for orientation and for social activities. In these locations, appropriate flexible space should be created for small gatherings or street-scale community activities.

Some nodes are located within the pedestrian walkway running through mews housing. Other nodes are at residential streets and will give pedestrians priority through a large raised table. This space will allow vehicle access on a 4.8m wide carriageway, bounded by seating areas, trees, bike and visitor parking.



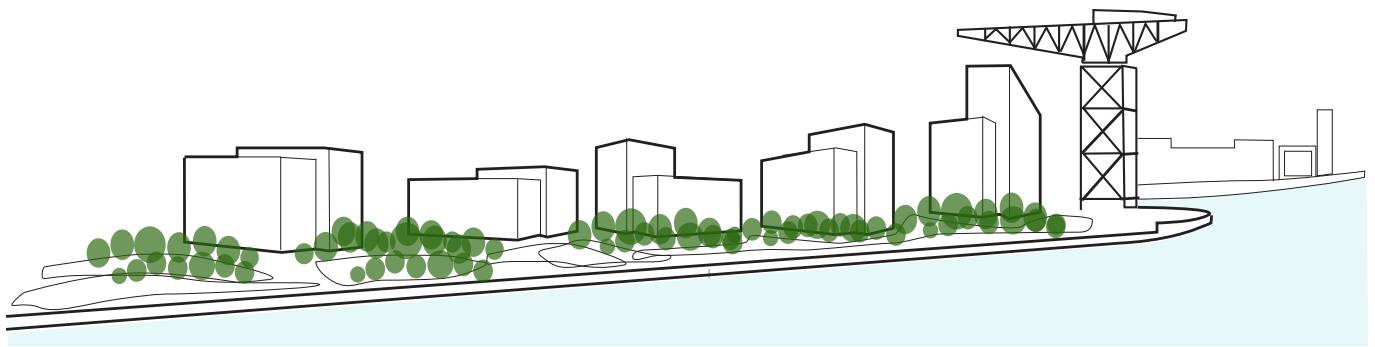
Typical street plan & cross section



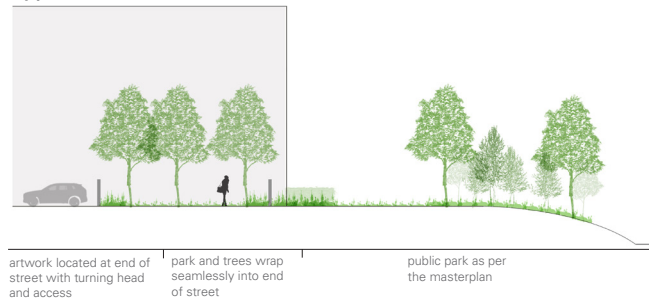
A shared river frontage - landscape and streetscape character

Refer to diagram on page 22 for the location of flats with a shared river frontage. Flats at the riverfront will make the most of views to the river for as many residents as possible. This is achieved by breaking up the massing of flats beside the river to allow properties behind to share the view. These 'pavilion' apartment blocks should vary in height and proportion.

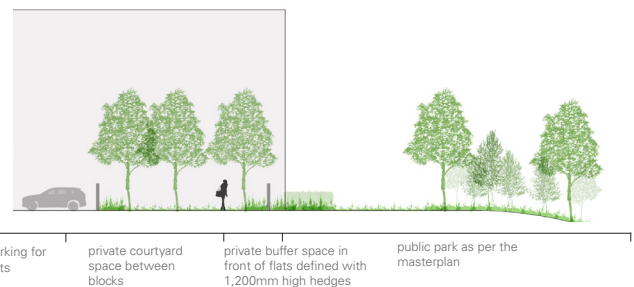
Each development plot will have an edge that seamlessly interfaces with the proposed riverside park. The apartment blocks here will sit 'in the park', with a greater extent of greenspace extending up into each street and wrapping around the buildings. A central private space is created between the higher buildings which retains views through the block. Planting in this area will be the same as the riverside park. Feature artwork could be located at the end of each residential street - connecting the two spaces.



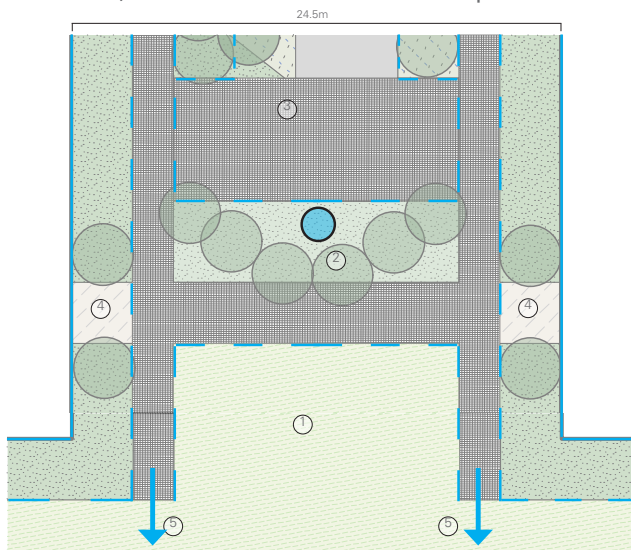
Typical cross section - end of residential street



Typical cross section - through development block



Plan; residential street meets linear park



- ① Parkland greenspace extends into street
- ② Feature artwork at the end of each street. Parkland sweeps up to the road end.
- ③ Turning head / drop off area designed in to street and extent of hard surfaces screened with planting (no parking)
- ④ Entrances to flats direct to street and across parkland
- ⑤ Direct connections into the park



greenspace at linear park beside river



busy public realm similar to Fitting Out Basin, spilling out from commercial uses at ground floor of adjacent mixed use development

Mews - landscape and streetscape character

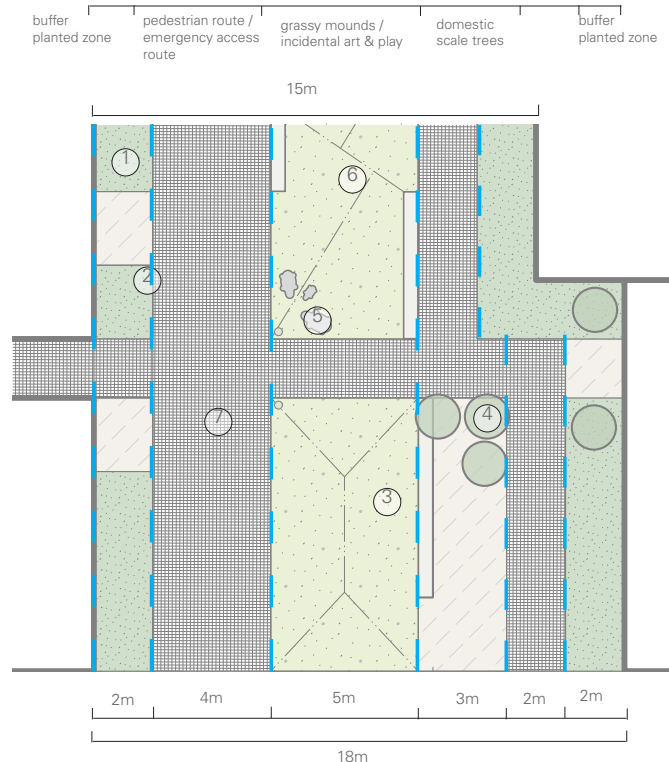
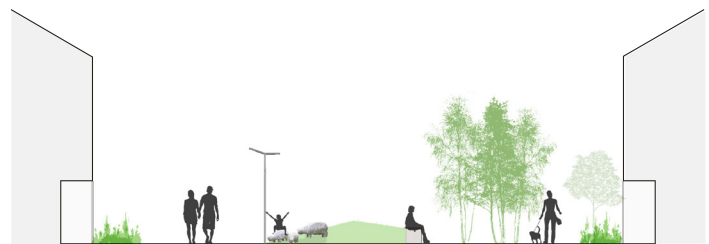
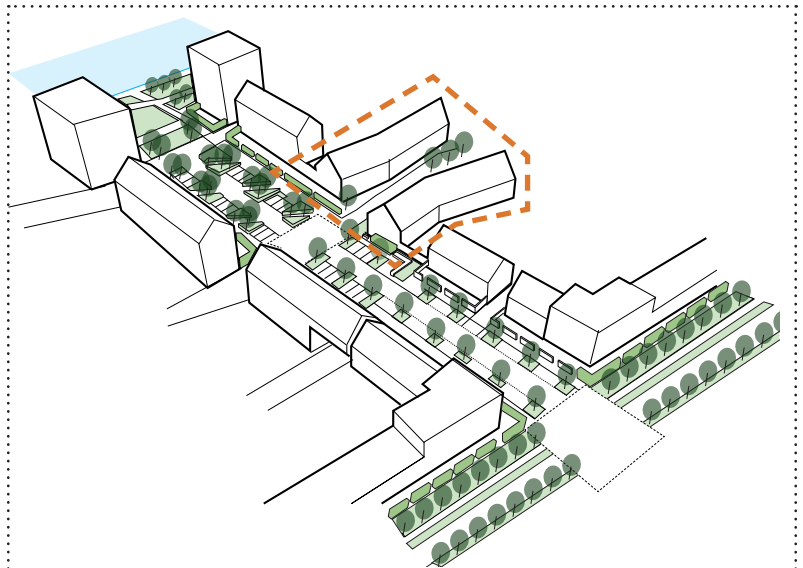
Refer to diagram on page 22 for the location of mews.

A new pedestrian and cycle route will cross residential plots in the western area of the site, running from West to East. (although service and emergency vehicles must be able to access). New two storey 'mews' homes should reflect its more relaxed character.

This should function as a social 'connector' - a fluid and seamless 'lane' that runs east to west through development plots.

The Mews is smaller in scale (15 metre to 18 metre in width) and has a less formal character, with groups of trees defining convivial spaces for seats and bike racks. Small scale pedestrian paths cut off the lane to create convenient links to car parks.

Playful artworks and greenspace replace the traditional road and create incidental spaces for children and a unique sense of place.



- ① Buffer planting - creates privacy and defines building thresholds
- ② No boundary walls/ treatments to plot edges
- ③ Greenspaces running through the street - widening with steps in the building to create space for activities
- ④ Small domestic scale trees holding in spaces and corners
- ⑤ Incidental features/ sculptures for play
- ⑥ Landform within the greenspaces to create variety and interest and integral seating
- ⑦ Occasional / emergency vehicular access route - kept to the south (shadier) side of the street

Backcourts - landscape and streetscape character

Character Area Overview

A number of the larger development plots (1, 2, 4 & 5 and 7) will have central back court spaces that are not suited for built development. These spaces provide an excellent opportunity to enhance the development in a number of ways. These spaces could create a more dispersed parking solution, provide community space for activities such as food growing, create habitat and ecological benefit such as woodland or community greenspace, be used for rain gardens and rainwater harvesting, urban



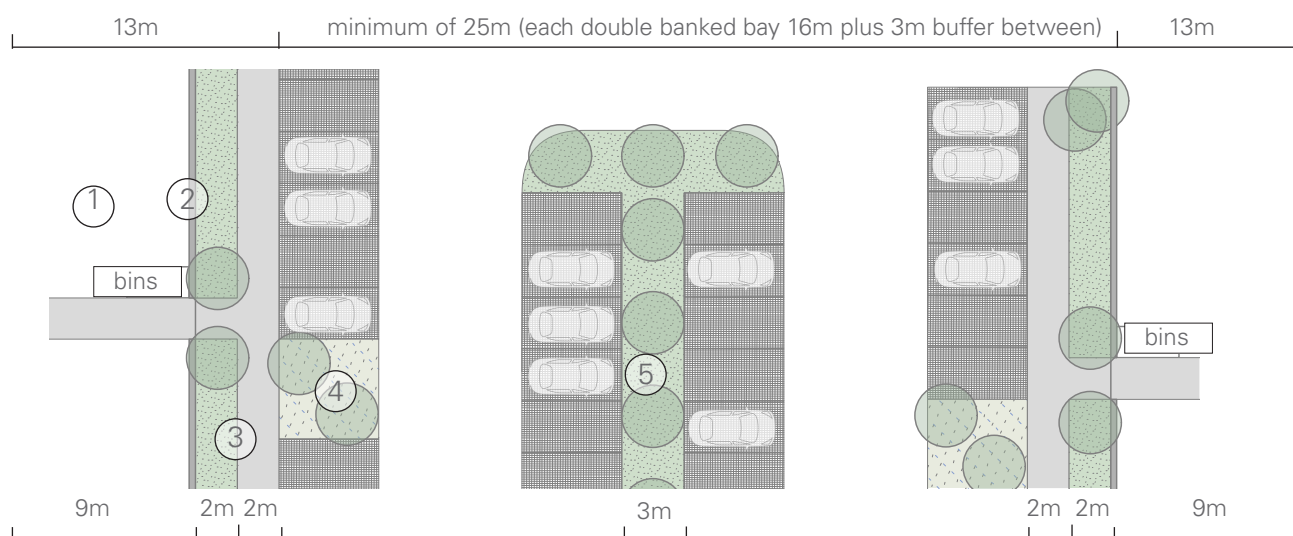
- community growing space set in community greenspace
- dispersed low density parking integrated within greenspace

Parking Courts - landscape and streetscape character

Parking courts are functional spaces which will provide residents parking, bin storage and access .

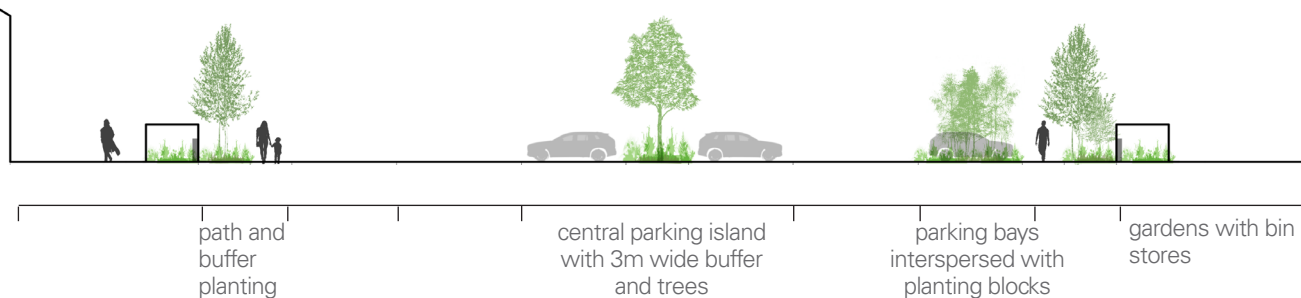
Each should have a distinctive character and a strong sense of enclosure – either through built form, tree planting or appropriate boundary treatments. Courts should be designed to accommodate wheelie bins and recycling storage so that they do not dominate views. They should provide easy and direct access to dwellings. Parking areas should not reduce useable rear garden areas and sufficient space must be given to green buffers around the edges of these spaces, and provision of trees both to the edge and at the centre of the space. To ensure the character of these areas is appropriate and does not deteriorate over time the dimensions set out below are the minimum for buffer and planted spaces.

Typical parking court plan



- ① back gardens / communal areas minimum 9m deep with bin storage at boundary allowing access from gardens and parking court
- ② boundaries treatments should be solid and not permeable (timber or brick), maximum 1,200mm high
- ③ a planted buffer including trees should be around edge
- ④ max 6 parking spaces around edge of space before broken up with a planted block - including multistem trees
- ⑤ central buffer with avenue trees between bays - minimum 2m width with trees planted in linked tree pits

Typical parking court section



multi-stem trees with blocks of robust native shrubs below low rear boundary fences with bin stores neatly located within garden space



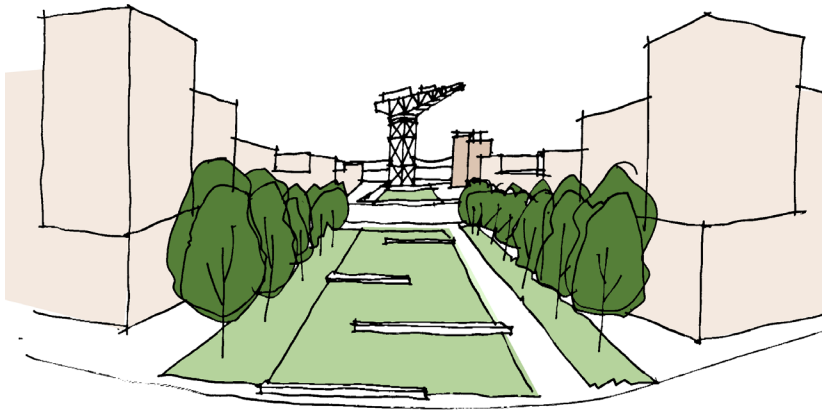
avenue street trees in centre of parking court - to bring down the scale of the space (max 6m spacing)



Masterplanned areas - landscape and streetscape character

The boulevards linking the town centre and Queens Quay, together with the area around them, will be busy, mixed use areas.

New development at each Boulevard will have a 'Gateway' function. Where buildings are situated close to Dumbarton Road, they provide an opportunity to signal the transformation of the new Queens Quay to the wider Clydebank area. High quality designs and materials are therefore essential. Some retail and commercial development is encouraged in plots alongside the boulevards, located at the ground floor of each development block.



pedestrian boulevard leading to the Titan Crane

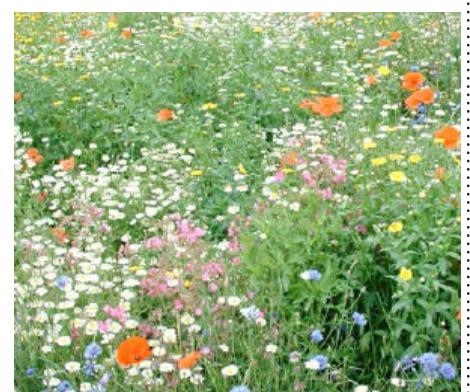
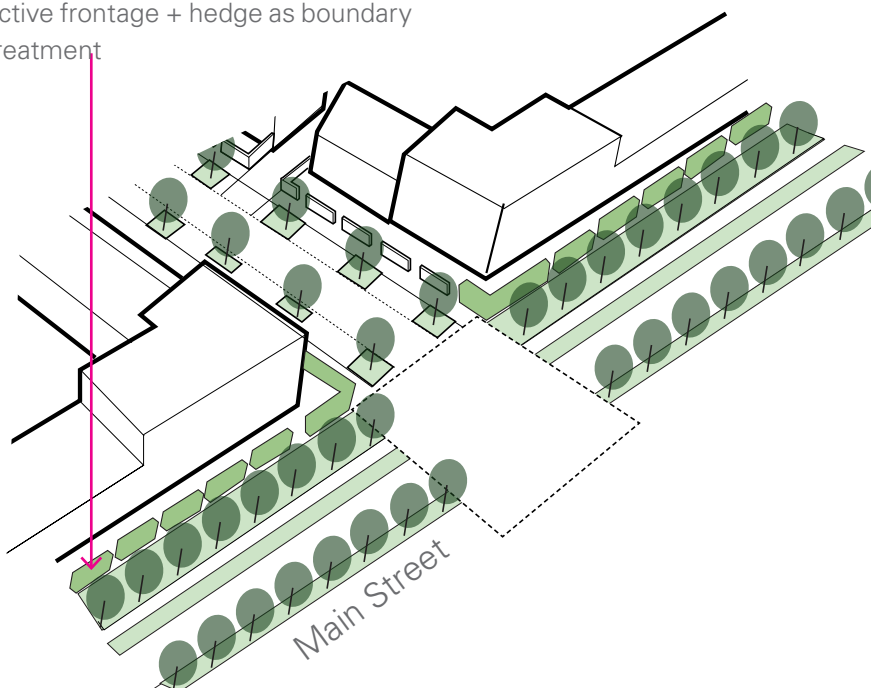


shared surface boulevard from masterplan

Development at Main Street will complement its large scale 'civic' location.

Because of Main Street's generous scale, new development beside it will need to 'contain' the space within the street, so buildings of a reasonable height, together with a continuous façade, are essential. Building typologies and massing at Main Street will change from three storey townhouses at the most western plots where the density is lower, to a maximum of seven storeys at corners closest to the Basin and the Titan Crane.

active frontage + hedge as boundary treatment



limetree avenue & wildflower planting from IWA masterplan documents

CASE STUDIES

CASE STUDIES

Each of the Case Studies on the following pages illustrates aspects of the requirements within the Design Codes. They are not intended as examples for designers to 'copy'.

Urban form: case study - Marmalade Lane designed by Mole Architects for K1 Cambridge



THEMES

- ☐ Street -urban character
- ☒ Street- parkland character
- ☒ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☐ Mews
- ☐ Mixed Use
- ☐ Parking
- ☒ Brick
- ☒ Simple eaves details
- ☒ Massing broken-up
- ☒ Vertically proportioned windows
- ☒ Simple proportions
- ☒ Good boundary treatments
- ☒ Building step up and down in height
- ☒ Active busy frontages



THEMES

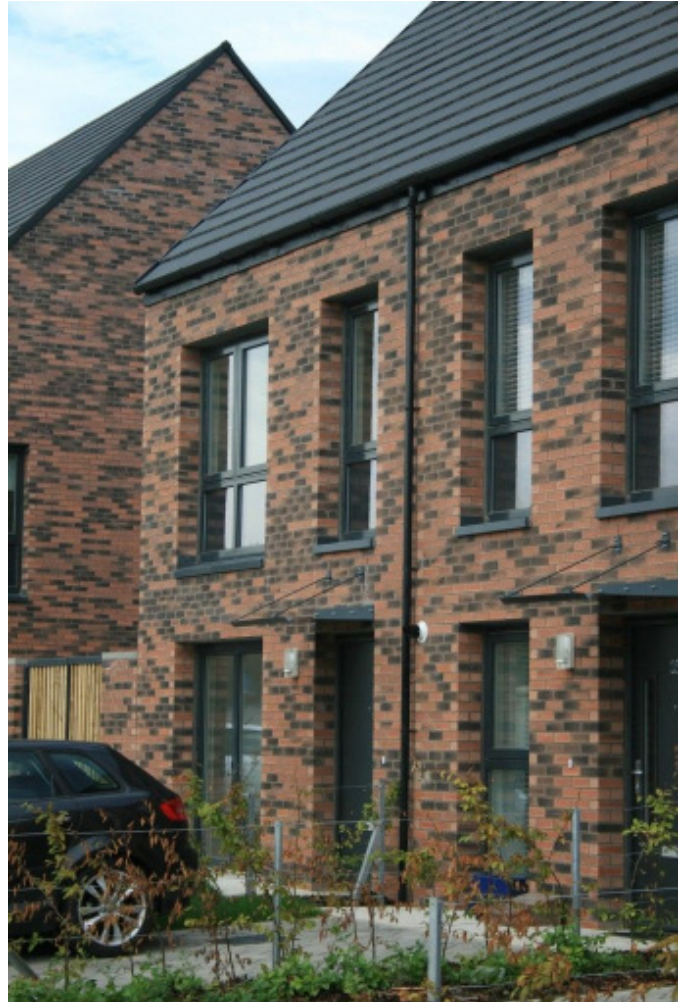
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Urban form: case study - Ryles Yard, Eddington

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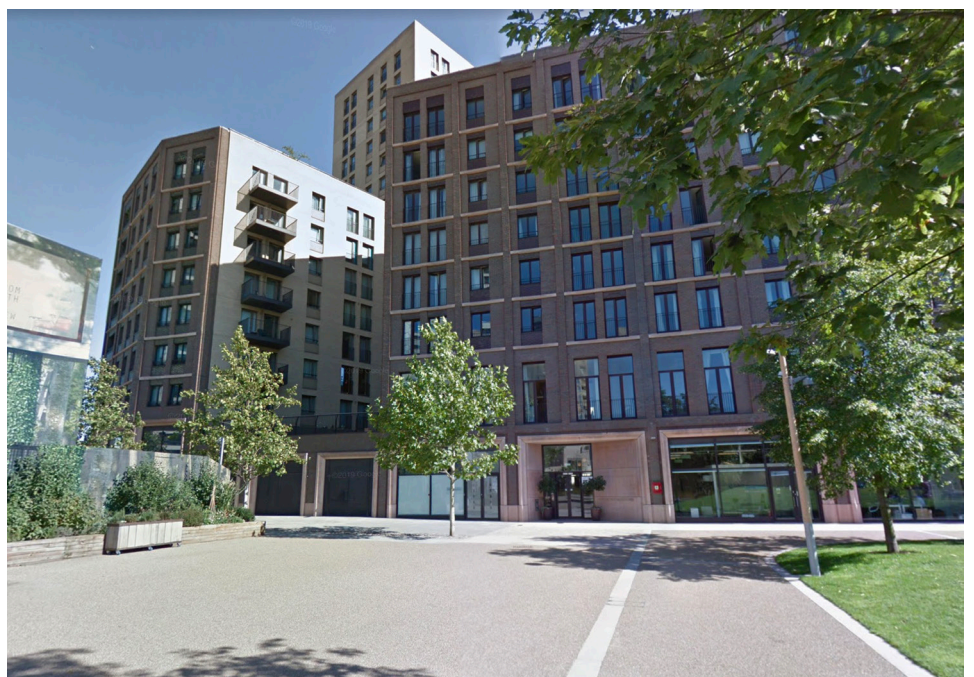
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END

West Dunbartonshire Council
Report by the Strategic Lead - Regulatory
Planning Committee: 12 February 2020

Subject: Planning Performance and Fees Consultation 2019

1. Purpose

- 1.1** To advise the Committee of the Scottish Government's "Planning Performance and Fees" consultation, and to agree the Council's response.

2. Recommendations

- 2.1** It is recommended that the Committee agrees that Appendix A forms the Council's response to the consultation paper.

3. Background

- 3.1** Over the last few years the Scottish Government has embarked upon a series of significant amendments to the Scottish planning system, and the resourcing of the planning system has been a recurring issue since the financial downturn. The Independent Panel Report in 2016 appointed to review the planning system considered both performance and fees and since then the Government have been gathering information to inform the approach going forward. Following the introduction of the Planning (Scotland) Act 2019, the Scottish Government has launched a consultation seeking comments on two aspects of planning reform: performance and fees. It is important to note that the consultation seeks views on how planning fees cover the cost of determining an application and not any additional duties required by the new Planning Act. The Government have noted that currently planning fees only account for on average 63 % of the cost of determining an application and it is expected by closing the gap would free up resources for the remainder of the planning service.

4. Main Issues

- 4.1** The consultation paper sets out the Scottish Government's proposals and contains 153 specific questions on which the Council's views are sought. The questions and recommended responses are attached in Appendix A. The Consultation Paper is split into 2 main sections.
- Performance - which formalises a mechanism of annual performance reporting by each Scottish planning authority and the introduction of a National Planning Improvement Co-ordinator.
 - Fees - substantial changes to the planning fee structure and also looks more widely at non-statutory charging for the first time.

Performance

- 4.2** The Planning (Scotland) Act 2019 places annual performance reporting by planning authorities on a statutory basis. The Planning Performance Framework (PPF) established by Heads of Planning in 2011-12 has been a valuable tool in demonstrating planning authorities' commitment to continuous improvement and all the work they do in delivering the planning service from determining applications, producing development plans, to working corporate services, sharing and learning from each other. The Scottish Government has recognised significant improvement in the markings awarded to authorities for the 15 key markers from the PPF demonstrating commitment to continuous improvement. This Council has now produced 8 PPF annual reports and reports the feedback received from the Scottish Government to Planning Committee on the key markers on an annual basis. The Consultation Paper asks views on whether a vision for the Planning Service in Scotland should be set out and whether the proposed approach set out in the PPF is correct and seeks suggestions on the Key Indicators and National Outcomes. The Council is supportive of a vision for the Planning System and has suggested in the response that the vision should be more outcome based indicating the role that planning can play in creating a better Scotland. In terms of the Key Indicators it indicates that performance should be measured on outcomes of what the planning system delivers i.e. quality housing, quality places, environment etc. rather than be focused purely on how long it takes to process an application; or the age of the development plan.
- 4.3** The Scottish Government is also proposing the appointment of a National Planning Improvement Co-ordinator to monitor and provide advice to planning authorities and others on the performance of general or specific functions. The Co-ordinator will be appointed by the Scottish Ministers and their role will be to focus on the performance of the planning system as a whole. It is recommended in the response that their role should be one of support when authorities are experiencing performance issues and they should be responsible in the PPF process and sit independently from the Scottish Government.

Planning Fees

- 4.4** A root and branch review of the planning application fee structure has been under consideration for over a decade and this Council has responded to the Consultation Papers in 2010 and 2012 to the levels of fees charged with no changes made to the method of calculation fees or to the categories. Fee levels were increased in 2013, 2014 and 2017 with the maximum planning fee increased to £125,000 in 2017. This present consultation paper does not commit to full cost-recovery through the proposed changes but proposes it moves closer towards that outcome. The consultation looks at how the fee regime could be revised as well as looking at the potential for discretionary charging, increased fees for retrospective applications, the removal of fees for advertising planning applications and reducing and waiving fees.
- 4.5** In terms of the proposed fee changes, many of the 153 questions follow the same standard format but repeated for different classes of development, all of

which are detailed in Appendix A. The key changes proposed of greatest relevance to West Dunbartonshire Council as Planning Authority include the following:

- Housing Applications - a sliding, reducing scale is proposed in recognition of the fact that the amount of work and cost involved for a single house can often be as much as for multiple units. The current fee for up to 10 units is £401 per unit and would increase to £600, 11 to 49 units would be £450 per unit and housing developments containing 50 or more units would be £23,550 with each additional unit charged at £250 until a new fee maximum of £150,000 is reached (current maximum fee is £124,850);
- Planning Permission in Principle (PPP) - 1 residential unit is currently £401 and would be reduced to £300. Where an application for PPP is based on site size the fee would rise by £300 per 0.1 ha incrementally until the maximum fee of £75,000 is reached;
- Extensions to Dwellinghouses - would increase from £202 to £300. However, for lesser domestic alterations (replacement of windows, sheds, gates, fences and other enclosures, garages and microgeneration equipment) the fee would reduce to £150;
- Retail & Leisure - Applications for development creating no new floor space, or not more than 50m² of new floor space will be charged a fee of £300. For developments above 50m² the fee is £1,500 for the first 50-100m² of the development followed by £800 per 100m² thereafter up to 2,500m², then the fee reduces to £500 per 100m² or part thereof subject to a maximum of £150,000. Applications for PPP shall be charged at £500 for each 0.1 hectare of the site subject to a maximum of £75,000.
- Business & Commercial – these buildings are charged according to the gross floor space to be created. Applications for development creating no new floor space, or not more than 50m² of new floor space, are charged a fee of £300. For buildings above that size the fee is £800 for the first 100m² of floorspace with this falling to £400 per additional 100m² or part thereof subject to a maximum of £150,000 (20% increase). Applications for Planning Permission in Principle shall be charged at £400 for each 0.1 hectare of the site subject to a maximum of £75,000;
- Electricity Generation – to be divided into 3 categories, instead of the current single category, wind farms, hydro schemes, and other energy generation projects (solar, energy storage, heat networks). Hydro schemes would increase from £401 per 0.1ha to £500 up to a maximum of £25,000. Other energy generation projects would be calculated on site size or floor space. The first 100m² of site size / floor space to be created will be £1,000 with £500 for every 100m² thereafter to a maximum of £150,000;
- Plant & Machinery - applications will be charged according to the area of the site and increase from £401 per 0.1ha to £500 or part thereof, subject to a maximum of £150,000;
- Access, Car Parks (Existing Uses) - subject to a flat rate fee of £600;
- Winning & Working of Minerals - charged according to the area of the site at a rate of £500 for the first 0.1 ha of the site and after that at a rate of £250 per ha or part thereof, subject to a maximum of £150,000;

- Waste Disposal and Minerals Stocking - charged according to the area of the site with the first 0.1 ha requiring a fee of £500 followed by a rate of £300 per 0.1 ha or part thereof, subject to a maximum of £150,000;
- Conversion of Flats and Houses - charged at the same rate as residential units;
- Change of use of a building - charged at £600 per application;
- Change of use of land - based on the site area with an initial fee of £500 for the first 0.1 ha and £300 for each 0.1 ha or part thereof up to a maximum of £150,000;
- Conservation Area – alterations to a property which would have otherwise been permitted development would be half the normal fee, rather than the full fee as at present;
- Listed Building Consent –proposed introduction of a fee as currently no fee payable;
- Advertisement Consent - increase from £202 to £300;
- Section 42 applications (applications to vary or remove conditions of an existing planning permission) – increase from £202 to £300.

In general, the Council is supportive of the suggested proposed uplift in planning fees across all categories of development and welcomes the move towards full cost-recovery. It is suggested that the upper cap for planning fees should be increased above £150,000 for certain types of development and that fees are subject to the rate of inflation on an annual basis.

4.6 The consultation also considers the issue of discretionary charging, which has for the first time been included in the new Act. It seeks views regarding the potential for charging for the following:

- charging for pre-application discussions;
- processing agreements (a bipartite agreement for the timescales and milestones in processing more complex applications);
- non-material variations (considerations of minor changes to planning permissions),
- the monitoring and discharging of conditions;
- Masterplan Consent Area (previously known as Simplified Planning Zones);
- Enhanced Project Managed Applications (a new concept for a more corporate project management role for major developments including different permissions and licences),
- self /custom build registers;
- advertising applications;
- charging for appeals to the Scottish Ministers and Notices of Review to the Council Local Review Body (currently there is no fee for either);
- reducing and waiving fees.

In principle the Council is supportive of discretionary charging but it indicates that it should be up to each Local Authority to decide whether they wish to charge for pre application discussions or other duties as it may be a disincentive to invest in areas requiring regeneration and economic growth. The Council is however supportive of charging for non-material variations,

planning agreements, monitoring and discharging of conditions and asks that a national applicable rate is set.

- 4.7** The consultation also seeks comments regarding a surcharge for retrospective applications (a surcharge of 100% is suggested although planning authorities should have the discretion to waive this where it considers a genuine mistake has been made) and whether or not a refund should be given to applicants if there had been an unreasonable delay in the processing of their application. The Council would support the extra charge for retrospective applications which could be a disincentive for unauthorised development but would not support any refund of fees as the cost to the Planning Authority has already been incurred.

5. People Implications

- 5.1** There are no personnel issues associated with this report.

6. Financial Implications

- 6.1** It is difficult to accurately predict the actual effect that the proposed fee changes would have for the Council. It is anticipated that the new fee arrangements will be in place by mid-2020. Whilst it is clear that the proposals would result in a reduction in income in some categories (e.g. planning permission in principle applications, some householder applications), overall, the bulk of the application types received by the Council would be the subject of an increase in fees. It is expected that the outcome of this consultation and that of other local planning authorities will result in a likely increase in planning fees for the Council. The additional resources would be required to be invested to help support planning improvements.

7. Risk Analysis

- 7.1** There are no known risks associated with this report.

8. Equalities Impact Assessment (EIA)

- 8.1** There is no requirement for an equalities impact report

9. Consultation

- 9.1** A workshop took place with all Planning Officers. The Strategic Lead – Resources has been consulted.

10. Strategic Assessment

- 10.1** The above Consultation covers a wide range of topics and is considered to contribute to all of the Council's strategic priorities and in particular towards:

- Economic growth and employability.
- Local housing and sustainable infrastructure.

Peter Hesse
Strategic Lead - Regulatory
Date: 12th February 2020

Person to Contact:	<p>Pamela Clifford, Planning, Building Standards and Environmental Health Manager, pamela.clifford@west-dunbarton.gov.uk</p> <p>Antony McGuinness Team Leader-Forward Planning Antony.mcguinness@west-dunbarton.gov.uk</p> <p>Erin Goldie Team Leader- Development Management Erin.goldie@west-dunbarton.gov.uk</p>
Appendix:	Appendix A – West Dunbartonshire Council’s proposed response to “Planning Performance and Fees” consultation
Background Papers	<p>Scottish Government “Planning Performance and Fees” consultation’ https://consult.gov.scot/planning-architecture/planning-performance-and-fees/</p> <p>West Dunbartonshire Council Planning Performance Framework 2018-19 https://www.west-dunbarton.gov.uk/planning-building-standards/planning-applications/planning-application-service-and-performance/</p>
Wards Affected:	None.

PLANNING PERFORMANCE AND FEES CONSULTATION– WEST DUNBARTONSHIRE COUNCIL RESPONSE

SECTION	CONSULTATION QUESTIONS	COUNCIL RESPONSE
PLANNING PERFORMANCE		
Purpose of Planning	Should we set out a vision for the Planning Service in Scotland?	Yes
	Do you agree with the vision proposed in this consultation paper?	No – see comments below
	Do you have any comments about the proposed vision?	<ul style="list-style-type: none"> The proposed vision reads like a service delivery statement; where is the aspirational element of planning? It should have the “wow “factor and should be more outcome based focusing on how planning can make a better future Scotland.
Preparation and Content of reports	Is the proposed approach to the content correct?	Yes
	Do you have any comments on the proposed content of Planning Performance Reports?	No
	Do you have any comments or suggestions as to how reports should be prepared?	No
	What statistical information would be useful/valuable to include and monitor?	<ul style="list-style-type: none"> The Council are content with the level of statistical information already required as part of the PPF as this is uniform across the 34 Planning Authorities and easily obtainable, measurable and can be readily monitored. However, it should also include resourcing and how this should be overcome.
	What are the key indicators which you think the performance of the system and authorities should be measured against?	<ul style="list-style-type: none"> The value of quantity and qualitative indicators should be assessed and evaluated. Performance should be measured on outcomes of what the planning system delivers i.e. quality housing, quality places, environment etc. rather than be focused purely on how long it takes to process an application; or how old your development plan is.
	Do you have any other comments to make with regards to how the Performance of the Planning System and Authorities is measured and reported?	<ul style="list-style-type: none"> Consideration should be given to when PPFs are submitted to ensure that when the feedback is received it is timely and meaningful. Presently, PPF is submitted end of July and feedback is not received until December and by the time it is reported to Committee it is out of date in respect of that reporting year; Also, consideration should be given to whether the PPF

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		<p>should be submitted annually or consideration to a full PPF one year and then a shorter version the second year. The preparation of PPF is very resource intensive.</p>
	<p>Do you have any suggestions about how we could measure the outcomes from planning such as: Placemaking; Sustainable Development and Quality of decisions?</p>	<ul style="list-style-type: none"> • These outcomes should be measured qualitatively through case studies, customer reviews, and testimonies from those involved; • Should a quantities tool be required then there should be general themes that are considered for each area which are considered to contribute, for example, to good placemaking and then the Authorities can fill those in.
	<p>Do you have any suggestions about how planning's contribution to the National Outcomes contained in the National Performance Framework should be measured and presented?</p>	<ul style="list-style-type: none"> • To make it more relevant sub-criteria should be developed nationally, which relate to planning within each of the national themes, in order to make it more meaningful and measurable.
<p>National Planning Improvement Co-ordinator</p>	<p>Do you have any comments/suggestions about the role and responsibilities of the National Planning Improvement Co-ordinator?</p>	<ul style="list-style-type: none"> • The Council considers that the scope of the role outlined in the consultation paper is the correct vision for the co-ordinator role and we look forward to working with the co-ordinator as they develop their role; • The Co-ordinator should take the PPF forward and enhance good practice; • The role of the co-ordinator should be one of support and, where it relates to monitoring the performance of local authorities, should not switch to one of punishing authorities that may have slipped behind performance standards, but to help to address and improve where that happens; • The post should be independent with clear parameters for the role. It should sit separately from Government and should be properly resourced.

PLANNING PERFORMANCE AND FEES CONSULTATION– WEST DUNBARTONSHIRE COUNCIL RESPONSE

SECTION	CONSULTATION QUESTIONS	COUNCIL RESPONSE
PLANNING FEES		
Category 1 – Residential Development	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based. The Council supports the move to full cost recovery through an increase in the planning fees.
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> Appears that smaller developments are being penalised in comparison to larger developments; The proposed cap for larger developments is too low given the complexity of sites that could command fees above £150,000; If the cap for larger developments is increased then this would justify the increase for smaller developments which would be more proportionate ; Fees should be subject to the rate of inflation on an annual basis.
Categories 2, 3, 4 and 5 – Extensions and Alterations to Existing Dwellings	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based. The Council supports the move to full cost recovery through an increase in the planning fees.
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> We do not agree that these types of development should be split into 2 categories (Extensions and Alterations) and the higher fee for the higher of these should apply for all of these developments; There should be no cap on the amount of dwellings; The statement infers that applications involving alterations are easier applications and whilst in some cases this may be true, these applications still demand support staff input (validation, neighbour notification, consultation etc) and a site visit from a planning officer and can be time consuming for planning officers given the issues involved;

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SECTION	CONSULTATION QUESTIONS	COUNCIL RESPONSE
		<ul style="list-style-type: none"> • Whilst the planning officer assessment may be simple, the statutory process is no different and the fee should cover this; • The proposed categorisation will add further complexity to planning fees; • If these categories are to go ahead then their definition must be completely clear, specific and consistent especially if they are to be subject to a sliding scale of fees; • Fees should be subject to the rate of inflation on an annual basis
Category 6 – Retail and Leisure including extensions	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> • Yes, we agree that an increase in fees for this type of development is required and should be inflation based. The Council supports the move to full cost recovery through an increase in the planning fees.
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The proposed cap for larger developments is too low given the complexity of sites that could command fees above £150,000
Category 7 – Business and Commercial including extensions	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> • Yes, we agree that an increase in fees for this type of development is required and should be inflation based. The Council supports the move to full cost recovery through an increase in the planning fees.
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The Council agrees to support the expansion of small and medium businesses. However, we do not agree that a reduction in planning fee costs for these type of businesses is justified and these type of businesses could be supported by other mechanisms such as small business grants, etc; • The cost to the Planning Authority remains with validation, neighbour notification, consultation and assessment therefore the level of work being undertaken

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SECTION	CONSULTATION QUESTIONS	COUNCIL RESPONSE
		<p>by the Planning Authority remains the same;</p> <ul style="list-style-type: none"> • There are other areas of support within the Planning Authority that can support business growth and small enterprises through pre-app advice, prioritising applications and signposting to other areas of existing support within the Local Authority, and externally; • The proposed cap for larger developments is too low given the complexity of sites that could command fees above £150,000.
Category 8 - Agricultural Buildings	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The Council does not agree with any reduction in the fee, as there is no reduction in the work required as detailed above; • The basis for calculating the fee above the permitted development level is generally accepted. However, there is an anomaly in the current fee regulations which should be addressed especially around a Determination of Prior Approval (DPA) application and the fee for a retrospective planning application.
Category 9 – Glasshouses	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The fee should be proportionate to the type and scale of development proposed; • We do not agree with any reduction in the fee, as there is no reduction in the work required.
	Should a separate category be established for erection of glasshouses on land that is not agricultural land?	<ul style="list-style-type: none"> • Yes, as it is not linked to agricultural land and is likely to raise issues.
Category 10 – Polytunnels	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The Council does not agree with any reduction in the fee, as there is no reduction in the work required; • The fee should be proportionate to the type and scale of

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		development proposed.
	Should a separate category be established for erection of polytunnels on land that is not agricultural land?	Yes
Category 11 – Windfarms – access tracks and calculation	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based as this type of development deals often with complex issues.
	Is using site area the best method of calculating fees for windfarms of more than 3 turbines?	Yes.
	If not, could you suggest an alternative? In your response please provide any evidence that supports your view.	<ul style="list-style-type: none"> It could be based on site area or per turbine which is ever the highest.
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> The maximum cap of £150,000 is too low given the complexities of this type of development; As the number of sites for energy developments with complex constraints are increasing, this has a direct impact on the workload of the Planning Authority in terms of time, resources and expertise required to deal with these types of applications.
Category 12 – Hydro Schemes	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required, and should be inflation based although the cap seems low as this type of development also has complex issues requiring resolution.
	Is the definition and the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed method for calculating the planning fee?	No
	Could the planning fee be set using site area for the generating station and equipment with a separate calculation used for pipework? This could be similar to the fee for Fish Farms where the surface area is subject to a different fee to the seabed.	<ul style="list-style-type: none"> We do not agree with this as the level of work carried out by the planning authority in assessing and determining these developments does not change so therefore a split fee approach is not justified or supported; As the number of sites for energy developments with

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		complex constraints are increasing, this has a direct impact on the workload of the Planning Authority in terms of time, resources and expertise required to deal with these types of applications.
Category 13 – Other energy generation projects	Is the definition and the proposed method for calculating the planning fee correct?	<ul style="list-style-type: none"> Yes, needs to be flexible for the type of development proposed.
	Do you have any comments on the proposed method for calculating the planning fee?	No
	Should a category be created for Solar Farms?	<ul style="list-style-type: none"> Yes, it needs to be identified similar to other energy developments like hydro developments.
	Do you have any suggestions for how the fee should be calculated?	<ul style="list-style-type: none"> Yes, could be similar to hydro developments fees.
	Should a category be created for energy storage developments?	Yes
	Do you have any suggestions for how the fee should be calculated?	<ul style="list-style-type: none"> Yes, similar to hydro development fees.
	Should a category be created for Heat Networks?	Yes
	Do you have any suggestions for how the fee should be calculated?	<ul style="list-style-type: none"> Yes, as suggested above.
Category 14 – Exploratory Drilling for Oil and Natural Gas	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 15 – Fish Farming	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 16 – Shellfish Farming	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 17 – Plant and Machinery	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based
	Is the proposed method for calculating the planning fee correct?	Yes

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	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 18 – Access, Car Parks etc. for Existing Uses	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • The addition of new car parks for existing uses should not be incentivised through a low, flat planning fee at a time when we are trying to reduce car dependency; • Adding a new car park can be complex. We currently have an application for a new carpark at an existing site that has several environmental constraints thus the work involved in assessing the application is significant; the fee for this type of application should reflect this; • Fee should be on a sliding scale based on the site area and inflation based.
Category 19 – Winning and Working of Minerals	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> • No
	Is the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • We agree that an increase in fees for this type of development is required and should be inflation based but the cap is too low
Category 20 – Peat	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> • Yes, we agree that an increase in fees for this type of development is required and should be inflation based
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> • Consider the planning fee based on the volume of peat extracted as opposed to site area, whichever is higher
	In light of the climate emergency do you agree that fees for applications relating to the winning and working of peat should continue to be considered separately from other mineral operations?	Yes
Category 21 – Other Operations	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed fees and for calculating	<ul style="list-style-type: none"> • We agree that an increase in fees for this type of

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	the planning fee?	development is required and should be inflation based but the cap is too low and could be realigned with other application fees.
Categories 22 and 23 – Waste Disposal and Minerals Stocking – does not cover waste management (recycling)	Do you agree with the proposed planning fees?	No
	Is the proposed method for calculating the planning fee correct?	No
	Do you have any comments on the proposed fees and for calculating the planning fee?	<ul style="list-style-type: none"> We agree that an increase in fees for this type of development is required and should be inflation based although the cap is too low.
Category 24 – Conversion of Flats and Houses (COU)	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based
	Is the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 25 – Building (COU)	Do you agree with the proposed planning fees?	<ul style="list-style-type: none"> Yes, we agree that an increase in fees for this type of development is required and should be inflation based
	Is the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
Category 26 – Land (COU)	Do you agree with the proposed planning fees?	Yes
	Is the proposed method for calculating the planning fee correct?	Yes
	Do you have any comments on the proposed fees and for calculating the planning fee?	No
	Please list any types of developments not included within the proposed categories that you consider should be.	No
OTHER FEES		
AMSC Applications	How should applications for planning permission in principle and Approval of Matters Specified in Conditions be charged in future?	<ul style="list-style-type: none"> Seems overly complicated and a wasted opportunity not to fundamentally review this element of the fees given the principle to move to full cost recovery; A standard fee could be short-changing Planning Authorities as AMSC can be very complex.

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	How should the fee for AMSC applications be calculated?	<ul style="list-style-type: none"> • Site area; • Sliding scale based on the use being proposed.
	Should the maximum fee apply to the individual developers/applicants or applied to the whole development with applicants (if number is known) paying an equal share of the max fee?	<ul style="list-style-type: none"> • Individual developer.
	Should the granting of a Section 42 application lead to the fee calculator being reset?	<ul style="list-style-type: none"> • Yes, application fee should be proportionate to the application as they can often be complex applications and a standard fee for altering a condition does not address it; • S42 is often used by developers to avoid paying a large application fee. The consultation does not provide a fee proposal for this and consideration needs to be given to it and linked to full application fees.
Cross boundary Applications – Allocation of the fee	<p>Should the fee for cross boundary applications be split between the respective authorities?</p> <ul style="list-style-type: none"> • No change • 100% to authority where majority of development occurs – remaining 50% to other authority. • Fee divided as per how the development is split across the authority boundaries • Other – please explain <p>Please provide reasons for your answer</p>	<ul style="list-style-type: none"> • Cross Boundary applications are rare but can be complex; • The fee divided as per how the development is split across the authority boundaries seems fair; • A very clear method of calculation would therefore be required to establish the amount of work done by each Planning Authority; • If this is not possible then the easiest way could be to set percentage so 100% and 50% seems reasonable and would avoid the opportunity for protracted discussion/negotiation.
Conservation Areas	<p>Do you agree or disagree with the proposal that where applications are required because permitted development rights for dwellings in conservation are restricted, then a reduced fee should be payable?</p> <p>Please provide reasons for your answer</p>	<ul style="list-style-type: none"> • The charging of an application fee in a Conservation Area is welcomed and it seems reasonable that it is a reduced fee given that it otherwise would be permitted development.
Listed Building Consent	<p>Is the introduction of a fee for applying for Listed Building Consent appropriate?</p> <p>How should that fee be set?</p>	<ul style="list-style-type: none"> • Yes, the introduction of a fee for LBC is entirely appropriate; • In some Authorities, there is an expertise that needs to be funded and this could be an opportunity to continue

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		<p>with that to ensure heritage is well preserved and enhanced;</p> <ul style="list-style-type: none"> • Sliding scale based on type of property • 50% reduction in fee; • Fee exemption for those on Buildings at Risk Register.
Hazardous Substances Consent	<p>Should the fees for Hazardous Substances Consent be increased?</p> <p>What levels do you think are appropriate?</p>	<ul style="list-style-type: none"> • Yes, we agree that an increase in fees for this type of development is required and should be inflation based.
Other Types of Application	<p>Are the proposed increases in fees for the categories above appropriate?</p> <ul style="list-style-type: none"> • CLUD • Advertisement • Prior Notification/Approval • S42 <p>Please explain the reason for your answer</p>	<ul style="list-style-type: none"> • CLUD – agree • Advertisement – agree • Prior Notification/Approval – agree • S42 - an increase from £202 to £300 is inappropriate and does not reflect the potential complexity of Section 42 applications. We suggest 50% of fee as per current fee scales. See comments above under AMSC applications.
	<p>Should the fee for Alternative Schemes remain as it is?</p> <p>Please explain the reason for your answer</p>	Yes
	Are there other fees which have not been considered?	All fully covered.
DISCRETIONARY CHARGING		
Range of Service	<p>Do you think we should set out the range of services which an authority is allowed to charge for?</p> <p>Please provide reasons for your answer</p>	<ul style="list-style-type: none"> • Yes, the range of services for pre application charging should be set out to provide certainty, clarity and consistency across all Planning Authorities and to prospective users of the service; • It should be up to the Local Authority to decide whether they wish to charge for pre application discussions as some Councils may not wish to charge as these areas may not be so attractive to developers and wish to attract investment into the area; • Fees should be ring-fenced to ensure they go back into the planning service and deliver the expected uplift in service provision that would be the expectation of any

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		charging.
Pre-Application Discussions	How should the fee for pre-application discussions be set?	<ul style="list-style-type: none"> It would be helpful for the Government to set out what they see as being reasonable for Planning Authorities to be able to charge for but it should remain a local level decision as to how these fees are set (if it is decided to charge) and what level of service is provided; It should be based around the 3 main categories of Major & large Local / Local (non-householder) / Householder in order to reflect the different structures and geography of each area. However, advice regarding a maximum reasonable limit for each category would be beneficial.
	Should the fees for pre-application discussions be subtracted from the full fee payable on submission of an application? Please provide reasons for your answer	<ul style="list-style-type: none"> Yes, this would be reasonable as it would provide an incentive to engage in pre application discussions.
Processing Agreements	Do you think that there should be an additional charge for entering into a processing agreement to reflect the additional resource required to draft and agree the timescales to be included?	<ul style="list-style-type: none"> No, in our experience if good quality pre-app consultation has taken place then a processing agreement is not required.
	Should we set the fee for that or an upper limit allowing authorities the flexibility to set their fee within clear parameters?	<ul style="list-style-type: none"> If fee is introduced this should be set at the national level and a Processing Agreement template should be used
Non- Material Variations	Where a non-material variation is required should an authority be able to charge for each change which is made? Or per request? <ul style="list-style-type: none"> No charge Per Change Per Request 	<ul style="list-style-type: none"> Planning Authority should be able to charge for NMV processing per request as it is the process that the Planning Authority is responding to.
	Should regulations set the fee for that or an upper limit allowing authorities the flexibility to set their fee within clear parameters?	<ul style="list-style-type: none"> Scottish Government should set the fee in line with application fees.
Monitoring of Conditions	Should authorities be able to charge for carrying out the monitoring of conditions?	<ul style="list-style-type: none"> Yes, in order to go towards to cost of this service.
	Should a fee for monitoring be limited to certain types of monitoring requirements?	Yes
	What should this be limited to?	<ul style="list-style-type: none"> Major, EIA, minerals and large scale developments.
	How should the fee be set?	<ul style="list-style-type: none"> % of application fee on sliding scale Set at national level to ensure consistency as there may

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		be concern that some Planning Authorities may put on more conditions in order to get more income.
Discharge of Conditions	Do you think there should be a fee payable for the discharge of conditions? Please provide reasons for your answer	<ul style="list-style-type: none"> • Yes, this is a fair and reasonable approach as per the English planning system. • Fee should be based on a sliding scale and type of development.
Planning Agreements	Do you think that Planning Authorities should be able charge for the drafting of planning agreements? Please provide reasons for your answer	<ul style="list-style-type: none"> • Legal and planning fees should be chargeable. Legal agreements can often take a considerable amount of time to draft and negotiate so a fee should be charged to cover that cost.
	If so how this should be calculated?	<ul style="list-style-type: none"> • This should be set by the Scottish Government
Masterplan Consent Areas	Should an authority be able to charge for development within a MCA (building, or changes or use) in order to recoup the costs involved in setting one up?	Yes
	Should we set the fee or an upper limit in the regulations?	Yes
	Please provide reasons for your answer	<ul style="list-style-type: none"> • A specific fee category for MCA would be useful as although the benefits of this process in facilitating development are appreciated, it is nonetheless a very time-consuming exercise with no resultant fee income to the planning authority.
Enhanced Project Managed Applications	Should the ability to offer and charge for an enhanced project managed service be introduced? How should this process work? Please provide reasons for your answer	<ul style="list-style-type: none"> • Considerable co-ordination between the various services will be required, in effect creating a multi-consent processing agreement. This would require all Services to buy-into this process. What would make the appropriate fee difficult to easily calculate would be the differing options depending on what permission / consent / licences would be required (e.g. planning permission / building warrant / roads construction consent / licencing, etc). A lead officer would be required to project manage / co-ordinate all the input from the different services and ensure that the overall agreed timetable for the project was being met; • If introduced this should be discretionary for all Local

PLANNING PERFORMANCE AND FEES CONSULTATION– WEST DUNBARTONSHIRE COUNCIL RESPONSE

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		Authorities.
	What, if anything, should happen in the event of failure to meet timescales?	<ul style="list-style-type: none"> If there has been a demonstrated failure on the part of the local authority to deliver what was agreed at the outset, the difference between the enhanced fee and the standard fees should be refunded.
Self/Custom Build Registers	Do you think charging for being added or retained on the register of interested people should be included in the list of services which Planning Authorities should be allowed to charge for?	<ul style="list-style-type: none"> Yes, charged on an annual basis.
	Should there be a restriction on the amount that can be charged?	<ul style="list-style-type: none"> Nationally applicable rate.
	Please provide reasons for your answer	<ul style="list-style-type: none"> This would give consistency for this service.
Charging for Appeals	Do you think that, in principle, fees should be charged for appeals to DPEA?	Yes
	Should we limit the circumstances in which a fee can be charged for lodging an appeal?	No
	In what circumstances do you think a fee should be paid for lodging an appeal?	All
	Do you think that the fee should be refunded in the event of a successful appeal?	No
	If so, should this follow the same process as is currently set out for awarding costs?	See above response
	What categories of appeals should be considered for charging?	All
	Do you think that a fee scale should be provided in relation to appeals to Local Review Bodies and, if so, should the arrangements differ from appeals to DPEA?	<ul style="list-style-type: none"> There should be a set fee for appeals to Local Review Bodies. This should be of a similar level to the appeals submitted to DPEA.
Reducing And Waiving Fees	Do you have any suggestions as to the circumstances in which they could use this power?	<ul style="list-style-type: none"> No, this will be difficult to administer. The advantage of the current fee system is that all applicants have to pay a fee with very few exceptions which means there is no discretion and no pressure to provide free or reduced fees.
	Should the maximum reduction allowed be set out in regulations?	<ul style="list-style-type: none"> Only if defined at the national level
	Please provide reasons for your answer	<ul style="list-style-type: none"> See above answer.
OTHER ISSUES		
Retrospective Applications	Should the surcharge be set at 100%?	Yes

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	If not what level should it be set at?	<ul style="list-style-type: none"> • See above answer
	Authorities will need to apply discretion when applying this surcharge. Should authorities need to clearly set out the reasons why the surcharge has been applied or not in each individual case?	<ul style="list-style-type: none"> • No, if it is a retrospective application the surcharge should apply, to ensure that the planning process is not circumvented.
	Please provide reasons for your answer	<ul style="list-style-type: none"> • Change to fees will overly complicate what is an existing simple process.
Incentives	Do you consider the use of rebates, discounts or other incentives, a useful tool in delivering a more efficient service? If so what would you consider to be an effective discount, rebate or other incentive?	<ul style="list-style-type: none"> • No. We do not consider the use of these methods as an incentive, the cost to the Planning Authority has already been incurred whether the application has been decided in 26 weeks or not. These methods do nothing to provide a well-resourced and efficient planning service
	Given the success of ePlanning, the continuing increase in its use and the savings which are made to both an applicant and authority in submitting an application electronically, do you think it is appropriate to apply an increased fee for submitting a paper application due to the additional work involved? Please provide reasons for your answer	<ul style="list-style-type: none"> • We agree that an additional fee for paper applications should be introduced as there is a great administrative burden associated with these applications.
Advertisements	Do you consider there should be a single advertising fee?	<ul style="list-style-type: none"> • No, there are different costs associated with different newspapers and different parts of the country. The advertising fee should cover the cost of advertising and should be set by the Local Authority if moving towards full cost recovery; • Also, consideration should be given to whether advertising in a newspaper should be continued given other media and communication methods.
	How do you think the cost of advertising should be recovered?	<ul style="list-style-type: none"> • Before validation.
EIA	Do you consider that submission of an EIA should warrant a supplementary fee in all cases? Please provide reasons for your answer	<ul style="list-style-type: none"> • Yes, as these require specialist skills and expertise.
	If so what might an appropriate charge be?	<ul style="list-style-type: none"> • % of planning fee set at a national level.
Hybrid Applications	Do you think that applications for planning permission in principle should continue to be charged at half the standard fee?	<ul style="list-style-type: none"> • Hybrid applications for large developments can be used to effectively grant full planning permission for elements within the overall development by way of not requiring

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		<p>an AMSC application for the aspects for which full details have already been provided;</p> <ul style="list-style-type: none"> • An increase to 75% would be appropriate
	<p>Should there be a different fee for 'hybrid applications' as described here?</p> <p>Please give reasons for your answer</p>	<ul style="list-style-type: none"> • See above answer.
Charging for SG Services	Should the Scottish Government introduce a service charge for submitting an application through eDevelopment (ePlanning and eBuilding Standards)?	<ul style="list-style-type: none"> • Yes, but less than paper.
IMPACT ASSESSMENTS		
	Do you have any comments on the BRIA?	No
	<p>Do you agree with our conclusion that a full EQIA is not required?</p> <p>Please provide reasons for your answer</p>	No comment
	<p>Do you have any comments on the EQIA?</p> <p>Please provide reasons for your answer</p>	No comment
	<p>Do you agree with our conclusion that a full SEA is not required?</p> <p>Please provide reasons for your answer</p>	No comment
	<p>Do you agree with our conclusion that a full CRWIA is not required?</p> <p>Please provide reasons for your answer</p>	No comment
	<p>Do you agree with our conclusion that a full Fairer Scotland Duty assessment is not required?</p> <p>Please provide reasons for your answer</p>	No comment
Island Proofing	Do you have any comments which relate to the impact of our proposals on the Islands?	No comment

WEST DUNBARTONSHIRE COUNCIL

Report by the Strategic Lead - Regulatory

Planning Committee: 12th February 2020

Subject: Decision on appeal against refusal of application for petrol filling station, a jet wash facility and alterations to the car park at Livingstone Street, Clydebank (DC18/209).

1. Purpose

- 1.1** To inform the Committee of the outcome of the planning appeal.

2. Recommendations

- 2.1** That the Committee notes the decision taken by the Department of Planning and Environmental Appeals (DPEA) to refuse the appeal and uphold the Council's decision.

3. Background

- 3.1** The above application was refused on 24th April 2019 due to the development having detrimental impacts upon traffic, congestion and general amenity. The development was to be associated with the nearby ASDA supermarket. The applicant thereafter lodged an appeal against the refusal of the planning permission to the Scottish Government Department of Planning and Environmental Appeal's (DPEA). An update was provided to the August 2019 Planning Committee.

4. Main Issues

- 4.1** Following an initial assessment and a site visit to the site, the Reporter held a Hearing as part of the appeal process which took place on 15th November 2019 in Clydebank Town Hall. The Hearing was requested in order to clarify and discuss matters related to the effects of the development on the loss of parking and the predicted traffic movements and flows associated with the use. As part of the Hearing the Reporter also wanted to explore the methodology and scope of the transport and parking assessments undertaken as part of the initial application. Whilst the Hearing focused on the technical traffic matters associated with the proposal it provided an opportunity for the Reporter to consider and listen to the views of the appellant, their appointed transport consultants, the Council, the Council's appointed transport consultants, Councillors, the Community Council and also other interested parties.
- 4.2** Following the Hearing the Reporter initially deferred making an initial decision and requested the appellant provide a technical capacity assessment of the

Argyll Road/Living Street roundabout. This information was cited by the appellant during the Hearing but absent from all submissions made. The Council's appointed transport consultant provided a technical report in response to this and following receipt of this the Reporter advised that they had all the information required to make a decision.

4.3 On 29th January 2020, the Council received confirmation that the appeal had been refused. The Reporters assessment of the application was extensive and covered a comprehensive review of all technical matters. The Reporter concluded that the introduction of a petrol filling station at the proposed location would have an adverse impact on queue length and hence delays at the Argyll Road/ Livingstone Street/ Coldstream roundabout. Given that the only access to Linnvale is via this roundabout, the Reporter considered that this impact would have an adverse and unacceptable effect on the amenity of those living within Linnvale and would therefore be contrary to the requirements of Policy GD 1 of the local development plan. The Reporter concluded that the proposed development does not accord overall with the relevant provisions of the development plan.

4.4 The Reporter outlined in their report that the input from Councillors and the Community Council at Hearing stage was relevant. It was considered that they effectively articulated a local knowledge and demonstrated a real life understanding of the current traffic, queuing and congestion issues in the locality and the impact on residential amenity as a result. Understanding this helped to form the view that the development would alter driver behaviour and generate new and altered trips to the area. On this basis, it was considered by the Reporter that regardless of the level of additional vehicles movements generated through the development, this would influence and impact upon an already difficult and sensitive situation and would be unacceptable.

5. People Implications

5.1 None.

6. Financial Implications

6.1 There are no financial implications for the report.

7. Risk Analysis

7.1 No risks have been identified.

8. Equalities Impact Assessment (EIA)

8.1 Not required.

9. Consultation

9.1 None.

10. Strategic Assessment

10.1 There are no strategic issues.

Peter Hessett

Strategic Lead - Regulatory

Date: 12th February 2020

Person to Contact: Pamela Clifford, Planning & Building Standards Manager,
email: Pamela.Clifford@west-dunbarton.gov.uk

Appendices: None

Background Papers:

1. Committee Papers
2. Appellant's Grounds of Appeal
3. Reporter's

Wards Affected: Ward 5 (Clydebank)