



# Agenda

## Planning Committee

**Date:** Wednesday, 30 September 2020

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**Time:** 10.00

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**Venue:** Zoom Video Conference

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**Contact:** Craig Stewart, Committee Officer  
craig.stewart@west-dunbarton.gov.uk

Dear Member

The Convener has directed that the powers contained in Section 43 of the Local Government in Scotland Act 2003 will be used and so Members will attend this meeting of the **Planning Committee** remotely. The business is shown on the attached agenda.

Yours faithfully

**JOYCE WHITE**

Chief Executive

Distribution:-

Councillor Jim Finn (Chair)  
Bailie Denis Agnew  
Councillor Gail Casey  
Councillor Karen Conaghan  
Councillor Ian Dickson  
Councillor Diane Docherty (Vice Chair)  
Councillor Daniel Lennie  
Councillor Douglas McAllister  
Councillor Marie McNair  
Councillor Lawrence O'Neill

All other Councillors for information

Date of Issue: 16 September 2020

**PLANNING COMMITTEE**  
**WEDNESDAY, 30 SEPTEMBER 2020**

**AGENDA**

**1 APOLOGIES**

**2 DECLARATIONS OF INTEREST**

Members are invited to declare if they have an interest in any of the items of business on this agenda and the reasons for such declarations.

**3 MINUTES OF PREVIOUS MEETING 5 – 13**

Submit for approval as a correct record, the Minutes of Meeting of the Planning Committee held on 19 August 2020.

**4 OPEN FORUM**

The Committee is asked to note that no open forum questions have been submitted by members of the public.

**5 PLANNING APPLICATION 15 – 23**

Submit report by the Strategic Lead – Regulatory in respect of the following planning application:-

DC20/116 – Erection of outbuilding for use as a commercial gym to operate a personal training business at 5 Gilmour Avenue, Hardgate, Clydebank, by Ms Caroline Jones.

**6 DEVELOPMENT PLAN SCHEME AND PARTICIPATION STATEMENT 2020 25 - 42**

Submit report by the Strategic Lead – Regulatory providing an update on the progress of the Development Plan and seeking approval of the annual review of the Development Plan Scheme and Participation Statement.

**7 QUEENS QUAY DESIGN CODES 43 - 140**

Submit report by the Strategic Lead – Regulatory advising of the outcome of the consultation on the draft Queens Quay Design Codes and seeking approval to adopt the Guidance.

**8 QUEENS QUAY DESIGN CODES: SUSTAINABILITY ANNEX 141 - 146**

Submit report by the Strategic Lead – Regulatory seeking approval of the sustainability annex for the Queens Quay site.

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## **PLANNING COMMITTEE**

At a Meeting of the Planning Committee held by Video Conferencing on Wednesday, 19 August 2020 at 10.06 a.m.

**Present:** Bailie Denis Agnew and Councillors Gail Casey, Karen Conaghan, Ian Dickson, Diane Docherty, Jim Finn, Daniel Lennie and Lawrence O'Neill.

**Attending:** Peter Hissett, Strategic Lead – Regulatory; Pamela Clifford, Planning, Building Standards and Environmental Health Manager; Erin Goldie, Team Leader – Development Management; Antony McGuinness, Team Leader – Forward Planning; James Hall and Matthew Spurway, Policy Planning Officers; John Walker, Assistant Engineering Officer, Roads & Transportation; Sarah Christie, Team Leader – Heritage and Arts; Nigel Ettles, Section Head – Litigation (Legal Officer) and Craig Stewart, Committee Officer.

**Apologies:** Apologies for absence were intimated on behalf of Councillors Douglas McAllister and Marie McNair.

**Councillor Jim Finn in the Chair**

### **CHAIR'S REMARKS**

Councillor Finn, Chair, welcomed everyone to the August meeting of the Planning Committee which was being held remotely, in terms of Section 43 of the Local Government in Scotland Act 2003.

Accordingly, the Chair advised that a process/procedure had been developed for the meeting (a copy of which had previously been circulated to Members). Thereafter, the Committee agreed to note the procedure in place for the conduct of the meeting and the meeting then continued by video conferencing.

### **DECLARATIONS OF INTEREST**

It was noted that there were no declarations of interest in any of the items of business on the agenda.

## MINUTES OF PREVIOUS MEETING

The Minutes of Meeting of the Planning Committee held on 10 June 2020 were submitted and approved as a correct record.

## OPEN FORUM

The Committee noted that no open forum questions had been submitted by members of the public.

## PLANNING APPLICATIONS

Reports were submitted by the Strategic Lead – Regulatory in respect of the following planning applications:-

### **Continued Applications**

- (a) **DC19/260 – Appeal to Scottish Ministers against the non-determination of planning application DC19/260 for the erection of 2 six storey flatted blocks, comprising 48 flats with associated road access, parking and infrastructure at former Dalnottar House steel site, Dumbarton Road, Old Kilpatrick by Link Group Ltd.**

As Councillor Finn was not present at the June meeting of the Planning Committee when this application was first considered, it was noted that Councillor Docherty, Vice Chair, would chair the meeting at this point. Accordingly, it was also noted that Councillor Finn would not be able to participate in any decision in respect of this item.

After discussion and having heard the Team Leader – Development Management, the Planning, Building Standards and Environmental Health Manager and the Legal Officer in further explanation and in answer to Members' questions, Bailie Agnew, seconded by Councillor Dickson, moved:-

That the Committee agree that the Council's response to the appeal was set out in the June Planning Committee report contained in Appendix 1 to the report.

As an amendment, Councillor Casey, seconded by Councillor Docherty, moved:-

That the Committee agree that the Council's response to the appeal be that it be refused on the grounds that the height of the proposed development was unacceptable and would have an adverse impact on the amenity of adjacent residents.

On a vote being taken, 4 Members voted for the amendment and 3 Members voted for the motion. The amendment was accordingly declared carried.

Note: Councillor Finn assumed the Chair at this point in the proceedings.

**(b) DC17/204 – Construction of four dwellings at land adjacent to Three Acres, Cochno Road, Hardgate by Mr David Lindsay.**

After hearing the Planning, Building Standards and Environmental Health Manager it was noted that this item had been withdrawn.

**New Application**

**(c) DC19/231 – Proposed residential development comprising of 44 dwellings and 22 flats with associated car parking and landscaping at Muir Road, Dumbarton by Caledonia Housing Association.**

After discussion and having heard the Team Leader – Development Management in further explanation of the report, the Committee agreed to grant full planning permission subject to the conditions set out in Section 9 of the report, as detailed within Appendix 1 hereto.

**LOCAL DEVELOPMENT PLAN 2**

A report was submitted by the Strategic Lead – Regulatory advising that the Council has received the Examination Report and seeking authorisation to adopt Local Development Plan 2.

**ADJOURNMENT**

After hearing Councillor Finn, Chair, the Committee agreed to adjourn for a short period. The meeting resumed at 11.30 a.m. with the same Members and officers present as listed in the sederunt.

After discussion and having heard the Planning, Building Standards and Environmental Health Manager, the Team Leader – Forward Planning and the Legal Officer in further explanation and in answer to Members' questions, the Committee agreed to accept all modifications recommended in the Examination Report, as detailed in Appendix 1 to the report, and to adopt Local Development Plan 2 as contained within Appendix 2 to the report.

**QUEENS QUAY DESIGN CODES**

A report was submitted by the Strategic Lead – Regulatory advising of the outcome of the consultation on the draft Queens Quay Design Codes and seeking approval to adopt the Guidance.

After hearing the Planning, Building Standards and Environmental Health Manager it was noted that this report would be continued to the September meeting of the Committee.

## **STREET NAMES FOR QUEENS QUAY SITE, CLYDEBANK**

A report was submitted by the Strategic Lead – Regulatory seeking approval of street names for the Queens Quay site.

After discussion and having heard the Planning, Building Standards and Environmental Health Manager and the Team Leader – Heritage & Arts in further explanation and in answer to Members' questions, the Committee agreed to approve the following street names for consultation:-

- Main spine road – Queens Quay Main Avenue
- Main thoroughfare – Titan Boulevard
- Main thoroughfare – Titan View
- Civic Square – 1<sup>st</sup> preference (John Brown's Square), 2<sup>nd</sup> preference (Queens Quay Square)
- Linear Park and path – 1<sup>st</sup> preference (John Brown's Park and John Brown's path), 2<sup>nd</sup> preference (Queens Quay Park and Queens Quay path)

For the other ancillary streets:-

- Clydebank Terrace
- Titan
- Foundry
- Firesitter
- Beacon
- Tarry Kirk
- Bothy School
- Grace
- Craigallion
- Carbeth
- John Brown
- Brick Clays

## **STREET NAME FOR NEW HOUSING DEVELOPMENT SITE AT CARROCHAN ROAD, BALLOCH**

A report was submitted by the Strategic Lead – Regulatory requesting the allocation of a street name to a road within the new housing development at Carrochan Road, Balloch.

After hearing the Planning, Building Standards and Environmental Health Manager, the Committee agreed that Carrochan Gardens be approved as the street name for the development.

**STREET NAME FOR NEW HOUSING DEVELOPMENT SITE AT THE FORMER  
HIGHDYKES PRIMARY SCHOOL, BRAEHEAD, BONHILL**

A report was submitted by the Strategic Lead – Regulatory requesting the allocation of a street name to a road within the new housing development at the former Highdykes Primary School, Braehead, Bonhill.

After discussion and having heard the Planning, Building Standards and Environmental Health Manager, the Committee agreed that Highdykes Hill be approved as the street name for the development.

The meeting closed at 11.58 a.m.

**DC19/231 – Proposed residential development comprising of 44 dwellings and 22 flats with associated car parking and landscaping at Muir Road, Dumbarton by Caledonia Housing Association.**

**GRANT full planning permission subject to the following conditions:-**

1. Prior to the commencement of development on site, exact details, specifications and samples of all proposed external materials to be used for the dwellings and associated hard landscaping, to include boundary treatments, facing stone to the retaining walls and storage facilities within the development site shall be submitted to and approved in writing by the Planning Authority. Thereafter, the development shall be completed in accordance with the approved material details and palette.
2. The development hereby approved shall be constructed in accordance with the finished site levels and finished floor levels as shown on approved plan - drawing no. P-18-AG0026\_700 Rev N Site Plan as proposed, proposed levels,). Any alterations to these finished site and floor levels shall first be agreed in writing with the Planning Authority.
3. Prior to the first occupation of the flats at Plot 1 and Plot 2 the cycle storage and refuse/recycling stores shall be installed in accordance with the detail shown on drawing nos.P-18-AG0026\_715 Rev A – Plot 2 Bin Store Elevations, P-18-AG0026\_714 Plot 1 Bin Store Elevations and P- 18-AG0026\_713- Rev B – Cycle Stand details. The constructed cycle stores and refuse/recycling stores shall be maintained for the lifetime of the development unless otherwise agreed by the Planning Authority.
4. The soft landscape arrangements approved under drawings L01 and L02 'Landscape Proposals (General Arrangement South & General Arrangement North)' including the associated planting schedule shall be implemented no later than the next available planting season or after occupation of the 30<sup>th</sup> property. Any trees, shrubs or plants forming part of the approved landscape scheme which die, are removed or become seriously damaged or diseased, within a period of 5 years from the date of their planting, shall be replaced in the next planting season with others of similar sizes and species unless the Planning Authority gives written approval to any variation. The landscaping arrangements as approved shall thereafter be maintained in accordance with these details for the lifetime of the development unless otherwise agreed by the Planning Authority.
5. The play area and associated equipment hereby approved (drawing no P- 18-AG0026\_720 –Rev A Play Equipment as Proposed) shall be installed prior to the occupation of the 30<sup>th</sup> dwelling. Thereafter the play area shall be retained for the lifetime of the development.

6. No house shall be occupied within the site until the vehicle parking spaces associated with that house unit have been constructed provided within the site in accordance with approved plan - drawing no. P-18-AG0026\_700 Rev N Site Plan as proposed, proposed levels). The aforementioned parking shall thereafter be retained and be capable of use at all times and shall not be removed or altered without the prior written approval of the Planning Authority.
7. Prior to the commencement of development with the site, details of the location and design of an electric charging point(s)/unit(s) to serve the development shall be submitted to and approved in writing by the Planning Authority. The approved car charging point(s)/unit(s) and associated infrastructure shall thereafter be installed in accordance with the approved details at a timescale agreed by the Planning Authority and maintained as such thereafter.
8. Prior to the occupation of the first house within the site, the developer shall install the necessary infrastructure to enable the full development and all associated properties to be connected to the existing fibre optic network, where available in West Dunbartonshire, and in accordance with the relevant telecommunications provider's standards.
9. Prior to the commencement of the development hereby approved details of the proposed public art, identified in the Design & Access Statement and the signage detailed on (drawing nos.L01 Landscape Proposals – General Arrangement South) shall be submitted to and approved by the Planning Authority in writing. The approved art and signage shall be installed within a timescale to be agreed by the Planning Authority and thereafter maintained for the lifetime of the development.
10. No development shall commence on site until such time as a noise control method statement for the construction period has been submitted to and approved in writing by the Planning Authority. This statement shall identify likely sources of noise (including specific noisy operations and items of plant/machinery), the anticipated duration of any particularly noisy phases of the construction works, and details of the proposed means of limiting the impact of these noise-sensitive properties. The construction works shall thereafter be carried out in accordance with the approved method statement unless otherwise approved in writing by the Planning Authority.
11. During the period of construction, all external works including piling and ancillary operations shall be carried out between the following hours and at no other time, unless otherwise agreed in writing by the Planning Authority:
  - Mondays to Fridays : 0800 – 1800
  - Saturdays: 0800 – 1300
  - Sundays and public holidays: No Working
12. Unless otherwise approved in writing by the Planning Authority, no development shall commence on site until such time as a scheme for the control and mitigation of dust has been submitted to and approved in writing

by the Planning Authority. The scheme shall identify likely sources of dust arising from the development or its construction, and shall identify measures to prevent or limit the occurrence and impact of such dust. The approved scheme shall thereafter be implemented fully prior to any of the identified dust generating activities commencing on site and shall be maintained thereafter, unless otherwise approved by the Planning Authority.

13. No development shall commence on site until details for the storage and the collection of waste arising from the development shall be submitted to and approved in writing by the Planning Authority. The agreed details shall be in place prior the occupation of the first housing unit/property within the site and thereafter maintained for the lifetime of the development.
14. No development (other than investigative work) shall take place until such time as a comprehensive site investigation completed by a suitably qualified person has been carried out to the appropriate Phase level and submitted to and approved in writing by the Planning Authority. If the Phase 1 investigation indicates any potential pollution linkages, a Conceptual Site Model shall be formalised and these linkages shall be subjected to risk assessment. If a Phase 2 investigation is required, then a risk assessment of all relevant pollution linkages will require to be submitted. If the risk assessment identifies any unacceptable risks, a detailed remediation strategy/plan shall be submitted to and approved in writing by the Council's Planning Authority and implemented as approved.
15. If required, a monitoring and maintenance scheme to include monitoring the long-term effectiveness of the proposed remediation over a period of years determined by the scheme shall be submitted to and approved by the Planning Authority. Any actions ongoing shall be implemented within the timescale agreed by the Planning Authority in consultation with Environmental Health. Following completion of the actions/measures identified in the approved remediation scheme a further report which demonstrates the effectiveness of the monitoring and maintenance measures shall be submitted to and approved in writing by the Planning Authority.
16. The presence of any previously unexpected contamination that becomes evident during the development of the site shall be reported to the Planning Authority in writing within one week, and work on the site shall cease. At this stage, if requested by the Planning Authority, an appropriate investigation and risk assessment shall be undertaken and a remediation scheme shall be submitted to and approved by the Planning Authority prior to the recommencement of site works. The approved details shall be implemented as approved.
17. If there is a requirement to either re-use site won material or to import material then the assessment criteria and sampling frequency that would adequately demonstrate its suitability for use shall be submitted to and approved by the Planning Authority prior to any material being re-used or imported. In addition to this and in accordance with BS3882:2015 and BS8601:2013, material to be used in the top 300mm shall be free from metals, plastic, wood, glass, tarmac,



paper and odours. Prior to placement of any of the material, the developer shall submit a validation report for the approval in writing of the Planning Authority and it shall contain details of the source of the material and associated test results to demonstrate its suitability for use. Thereafter the development shall be undertaken in accordance with the approved details.

18. No piling works shall be carried out until a method statement has been submitted to and approved in writing by the Planning Authority. This statement shall include an assessment of and take into account the following:

- The impact of the piling on surrounding properties.
- Detail any procedures which are required to minimise the impact of noise and vibrations on the occupants of surrounding properties.

This statement as submitted shall be prepared by a suitably qualified person and shall take into account the guidance contained in BS6472:1984 'Evaluation of Human Response to Vibration of Buildings'. The piling works shall thereafter be carried out in accordance with the approved method statement until they are completed on site.

19. Prior to the commencement of development on site maintenance details of the Sustainable Urban Drainage System (SUDS) to be installed shall be submitted for the written approval of the Planning Authority and shall be maintained in accordance with the approved details. The SUDS shall be designed to ensure that contaminants present on the site are not mobilised and that pollution pathways are not created.



**WEST DUNBARTONSHIRE COUNCIL****Report by Strategic Lead - Regulatory****Planning Committee: 30<sup>th</sup> September 2020**

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**DC20/116:           Erection of outbuilding for use as a commercial gym to operate a personal training business at 5 Gilmour Avenue, Hardgate, Clydebank, by Miss Caroline Jones.**

**1.     REASON FOR REPORT**

- 1.1**    The application raises issues of local significance and is subject to a number of representations. Under the terms of the approved Scheme of Delegation, it therefore requires to be determined by the Planning Committee.

**2.     RECOMMENDATION**

- 2.1**    **Refuse** planning permission for the reasons set out in Section 9.

**3.     DEVELOPMENT DETAILS**

- 3.1**    The application site relates to a semi-detached two-storey dwelling located in an established residential area and is surrounded by residential properties. This is with the exception of part of the southern boundary of the application site with the land beyond comprising of a vacant area that previously accommodated a domestic shed and garage allotment. The rear garden area of the property is grassed and relatively flat. It is surrounded by a low perimeter fence on the southern and eastern boundaries. There is no boundary treatment in place between the rear garden of the application site and the adjoining property. The property has no in-curtilage parking spaces or a vehicular driveway.
- 3.2**    Planning Permission is sought for the erection of an outbuilding to be located within the rear curtilage of the above property for use as a commercial gym to operate a personal training business. The outbuilding would be of pitched roof construction and measure 3.5 metres in height (approx.), 2.4 metres in depth (approx.) and 10 metres in length (approx.) It would be positioned along the rear boundary line of the applicant's curtilage and would have a footprint of approximately 23 square metres. In terms of its appearance it is proposed to be finished in timber weather boards with a black felt roof. The design of the outbuilding includes the provision for two single windows and a door on the west elevation (which faces into the garden and towards the rear elevation of the applicants property).
- 3.3**    The applicant seeks to operate a gym for a personal training business on a 35 hours per week basis, spanning over a 5 day period. The hours of operation

proposed will be 9.30am – 6.30pm Monday, Tuesday and Thursday (with a 2 hour afternoon break each day) and 9.30am – 1.30pm Wednesday and Friday. The applicant advises that they are the only member of staff associated with the proposed use and that they themselves will facilitate and conduct the 'One on One' personal training sessions. Each session would take approximately 60 minutes with each individual client, with this being extended to 75 minutes if a consultation is included. The applicant proposes to construct two parking spaces to the front of the property and also refers to availability of on street parking to support the use.

#### **4. CONSULTATIONS**

- 4.1** West Dunbartonshire Council Roads Service have no objections subject to the provision of two parking spaces.
- 4.2** West Dunbartonshire Council Housing Services as landlord for the application site have advised that they would not support the proposed parking within the site as it is associated with a proposed commercial business.
- 4.3** West Dunbartonshire Council Environmental Health Service have no objections subject to the use operating in accordance with the submitted supporting information.

#### **5. REPRESENTATIONS**

- 5.1** Seventeen letters of representation have been received in response to this application. These comprise of four letters of objection from properties neighbouring the site and thirteen letters of support from the wider area. A summary of the points raised in the representations are as follows:

##### Points of Objection:

- Excessive noise levels and noise disturbance from the proposed use.
- Lack of parking provision (including any in-curtilage parking) to accommodate and support the use.
- Lack of parking will exacerbate an already prevalent issue on Gilmour Avenue.
- Impact on privacy and overlooking
- Objection to the fact that the use has been operating unauthorised from the applicants property and garden for a number of months prior to the submission of the application.
- Inappropriate for such a commercial business and use to operate from a residential area and environment.
- Potential for the use to intensify and operations to increase in the future.
- Scale and massing of outbuilding is obtrusive and excessive for a residential area

- Outbuilding is too close to neighbouring residential boundaries.
- Neighbouring gardens at lower gradient than applicants meaning outbuilding will create issues in terms of overshadowing and loss of light to neighbouring garden ground and properties.
- Inability for the Council to control and restrict the number of clients using the facility at any one time.
- Inability for the Council to control and monitor noise levels from the facility.
- Security issues and concerns regarding additional footfall of strangers and users of the facility into a residential area.

#### Points of Support:

- Business and use promotes health and wellbeing.
- Business and use will have benefits in terms of promoting physical exercise and mental health.
- This will provide a service that will positively impact the local community.
- The outbuilding and gym will be soundproofed to mitigate noise.
- As the business will only have one client at a time the lack of in-curtilage parking will not be an issue.
- Current parking and traffic issues are caused by the nearby school and Goldenhill Clinic and this proposal would have a limited impact on this.
- Most clients using the facility can either walk or use public transport, limiting the amount of vehicle trips.
- The proposed studio will provide a safe, welcoming and therapeutic environment for clients as opposed to a commercial gym which can be intimidating and uncomfortable.
- The proposed location of the gym in the outbuilding will make for a more sterile and clean environment.
- More businesses will need to operate from home
- the proposed use is compatible for a residential area given limited numbers of clients

## **6. ASSESSMENT AGAINST THE DEVELOPMENT PLAN**

### West Dunbartonshire Local Plan 2010

- 6.1** Policy GD1 seeks to ensure that all new development is of a high quality design, of an appropriate and compatible land use and that it respect the character and amenity of the surrounding area. Policy H5 seeks to ensure that the character and amenity of existing residential areas are safeguarded where new development is proposed. The criteria relevant to these considerations includes a need for proposals to reflect the character of the surrounding area in terms of its scale, design, density and materials, the requirement to avoid development that would adversely effect on local amenity (including parking). The proposed use by virtue of its residential location and setting would not

constitute a compatible use and it has the potential to adversely impact the character and amenity of an established residential area. The proposals are therefore contrary to Policy GD1 and H5 with the reasons for this outlined in more detail in Section 7 of this report.

- 6.2** Policy LE7 specifically addresses business development in mixed use or residential areas. This policy sets a requirement that any such commercial or business uses can only be supported in residential areas where it is robustly justified that the residential amenity of the surrounding uses will not be adversely affected or compromised by the proposals. The proposals are also contrary to this policy and this is considered in more detail in Section 7 below.

## **7. ASSESSMENT AGAINST MATERIAL CONSIDERATIONS**

### West Dunbartonshire Local Development Plan (LDP1) Proposed Plan

- 7.1** On 27 April 2016, the Planning Committee took a final decision not to accept the Local Development Plan Examination Report recommended modification in respect of including the Duntiglenan Fields site in Clydebank as a housing development opportunity, and therefore, as a result of the Scottish Ministers' Direction, the Local Development Plan has remained unadopted but continues to be a material consideration in the determination of planning applications.
- 7.2** The application site is located within an established residential area. Policy BC4 states development that would potentially significantly harm the residential amenity, character or appearance of existing neighbourhoods will not be permitted. Similar to Policy LE7 of the Adopted Plan (2010), Policy DS1 of this plan states that all development should avoid unacceptable impacts on adjoining uses including those of noise, vibration, invasion of privacy and traffic implications. The proposals are considered to be contrary to the applicable policies and this is discussed further in Section 7 below.

### West Dunbartonshire Local Development Plan (LDP2) Proposed Plan

- 7.3** The modified Plan and associated documents was approved by the Council on 19 August 2020. The Council has now advised the Scottish Ministers of its intention to adopt the Plan. Local Development Plan 2 is therefore the Council's most up to date policy position and is afforded significant weight in the assessment and determination of planning applications.
- 7.4** Policy SC1 'Sequential Approach Proposals' sets a hierarchical direction for the preferred location of retail, commercial and leisure proposals, and other key town centre uses. This policy requires any such uses (including commercial gyms and personal training business uses) to align with the sequential approach and network of centre hierarchy defined by the plan. Policies SC1 and SC3 direct such uses to be located firstly within town centres, thereafter edge of town centre commercial centres, other edge of town centre sites and out of centre sites (which are accessible to public transport). Other sites will only be considered where it is demonstrated that by virtue of that location they are serving a specific neighbourhood, community or catchment which is best served more locally. The proposal is to introduce a gym business for personal training purposes use into an established

residential area. It has not been sufficiently evidenced that any consideration has been given to those sites or locations set out in the hierarchy of the plan and which are preferred for these types of uses. Equally, insufficient evidence or justification has been provided to justify a site specific locational need for the use in the domestic location as proposed. The proposals are therefore contrary to Policy SC1 and SC3.

- 7.5** Policy H4 focuses on the safeguarding of amenity in existing residential areas to protect, preserve and enhance their residential character and amenity. As part of this, the policy specifies a presumption against the establishment of non-residential uses within, or in close proximity to, residential areas which potentially have detrimental effects on local amenity. Similarly to policy H4, Policy CP1 states that new developments must respond to their local context and must protect and enhance the amenity of existing communities and neighbouring development sites. The proposed use is not compatible with the residential area and it has the potential to adversely impact upon existing residential areas and properties surrounding the site. Therefore the proposal is contrary to Policy H4 and CP1.
- 7.6** Policy ENV8 seeks to ensure that developments do not have a significant impact on established residential areas and properties by way of noise pollution. Where required proposals that have the potential to impact, they will require to demonstrate that their impact is not significant and provide adequate mitigation where necessary. Policy CON1 relates to transport matters and requires all developments to comply with national, regional and local transport requirements. The proposals are contrary to both of these policies, with the reasons for this outlined in more detail in Section 7 of this report.

#### Principle of Development and Site Selection

- 7.7** The applicant has provided statements in support of the proposal which set out the proposed days and hours of operation as outlined in section 3.3 above and advises that sessions with clients would be one to one only. In attempt to address noise concerns, the applicant states that the proposed outbuilding would be constructed to include some measures to help attenuate noise such as insulation, noise block sheets and rubber floor mats; music would be played from a mobile phone only; there would be no heavy weight training equipment. The applicant also states that there is a requirement to have the business operate from their property because the personal training business will provide a mental health and wellbeing focus that requires a quiet environment that can be provided from the location and cannot be provided from a commercial setting. Two car parking spaces are proposed to be constructed within the site and the applicant also states that on street parking on Gilmour Avenue can be used and on particularly busy days, clients can park at the nearby Hardgate shops. It is also anticipated that local clients would walk to the application site.
- 7.8** The supporting statement provided by the applicant does not provide sufficient justification for the acceptability of the use at this location. The

business is to be operated from the rear garden within an established residential area. This does not represent a suitable or appropriate location for a gym/ personal training business use. The relevant policies of LDP2 specifically direct gym and personal training type business uses to town centres as the most preferred location with out of town and mixed use locations considered favourably in that order thereafter. The supporting information submitted does not sufficiently demonstrate and evidence that the range of other more suitable locations within the Council area have been considered or why they have been discounted as unsuitable to accommodate the proposed use. The relevant policies of the Adopted and Proposed Plans do allow alternatives to town centres and out of town mixed use areas to be considered only where it has been sufficiently demonstrated and evidenced as the most appropriate site to be able to serve a specific neighbourhood, community or catchment which is best served more locally. In this case however, it has also not been demonstrated why the proposed use requires to be specifically located within the grounds of the applicants home. The principle of the proposed use at this location is therefore not considered acceptable when considered against the relevant policies of the Adopted and Proposed Plans.

#### Compatibility of Use and Impact upon Residential Amenity

- 7.9** In cases where sufficient justification is provided to evidence a site specific locational need and that other appropriate and more preferred sites have been sufficiently discounted, there is still a requirement that businesses and uses proposed within a residential area can only be supported where it is has been satisfied that the residential amenity of that area will not adversely affected or compromised.
- 7.10** The gym for a personal training business is not considered to be compatible with the residential nature of the application site and surrounding area. The site is in close proximity with neighbouring residential properties and there is no existing boundary treatment to the adjoining neighbours garden. The proposed use would be operational including more footfall over 35 hours, five days per week. This increased activity would be more than what would be typically expected for a residential property and have an adverse impact on adjacent properties. It would also create potential for noise generation through the increased activity, use of equipment, playing music regularly and increased traffic. The case presented does not sufficiently demonstrate that these matters can all be adequately mitigated. It is therefore considered that the proposal would be out of keeping and harmful to the residential character and amenity of the area and contrary to the relevant policies of the Adopted and Proposed Plans.

#### Parking and Traffic Impacts

- 7.11** There is currently no provision for parking within the application site. The Roads Service have advised that two parking bays would be required; one for staff parking and one for client parking. The applicant has provided a plan as part of the application annotating two in-curtilage parking bays to the front of



the property. Whilst this would in principle satisfy the Roads Service requirements, the Council is the landlord for the applicants property and the Housing Service have advised that they would not support the formation of the proposed parking area as it is associated with a commercial business. On this basis, it is unlikely that the parking proposals could be implemented and therefore the two parking bays as required by the Roads Service cannot be achieved. The applicant has indicated that on street parking could be achieved but this is not sufficient to address the Roads Service comments as the applicant has no control over on street parking. The applicant has also suggested that clients could park at the nearby Hardgate shops or walk to the site however this cannot be controlled and these considerations do not outweigh the Roads Services requirement for parking to be provided within the site.

#### Other Matters Raised in Representations

- 7.12** Whilst the letters of support refer to a host of health and wellbeing benefits and incentives to the local and wider community and public are noted and acknowledged, these benefits do not outweigh the impact that the proposal would have on the residential amenity of the immediate area. The representations also raise the role of the Council to support economic growth and enable businesses such as this to set up and grow and this is acknowledged. However, the Council also has a role to safeguard residential amenity and to direct business uses to the most appropriate areas as identified in the Adopted and Proposed Plans. Any direct or indirect benefits gained from the introduction and operation of this commercial use would at the same time compromise the amenity of the established residential area and the proposals cannot therefore be supported at this location.
- 7.13** With regard to the matters raised in the objections relating to the ongoing unauthorised operation of the gym and personal training use from the applicants property, this is being pursued separately by the Council.

### **8. CONCLUSION**

- 8.1** The proposed outbuilding which is proposed to be used as a commercial gym is not an appropriate form of development at this residential location and it is contrary to the policies of both the Adopted and Proposed Local Plans. No site-specific locational need for the development has been established and there is a lack of justification to demonstrate that the use could not be accommodated in other more suitable, available town centre and other commercial areas.
- 8.2** The site is situated in an established residential area and the commercial use by virtue of its nature, characteristics and associated activities will have an adverse impact upon residential amenity and would be out of keeping and harmful to the established residential character of the area including neighbouring residential properties to the site.

## 9. REASONS FOR REFUSAL

1. The proposal is contrary to Policies LE7 and GD1 of the Adopted Plan (2010), Policy BC4 of the Local Development Plan 1: Proposed Plan and Policies CP1 and H4 of Local Development Plan 2: Proposed Plan (LDP2 2018) as the proposed gym for personal training business is considered to be incompatible and will adversely impact on the amenity of the surrounding residential area by virtue of noise, increased footfall and activity and disturbance beyond what would ordinarily and typically be considered acceptable for an established residential area and environment.
2. The proposals are contrary to Policy SC1 and SC3 of Local Development Plan 2: Proposed Plan (LDP2 2018) as the proposals fail to sufficiently demonstrate and evidence any reasonable assessment and consideration of available town centre sites and locations and other alternative preferred sites within the West Dunbartonshire area to sufficiently discount their suitability in favour of the proposed residential setting as proposed.
3. The proposal is contrary to Policy GD1 of the Adopted Local Plan (2010) and Policy CON1 of Local Development Plan 2: Proposed Plan (LDP2 2018) as the proposed gym for use as a personal training business would adversely impact upon and exacerbate existing on street parking.

**Peter Hessett**  
**Strategic Lead - Regulatory**  
**Date: 30<sup>th</sup> September 2020**

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**Person to Contact:** Pamela Clifford, Planning, Building Standards and Environmental Health Manager  
Email: [Pamela.Clifford@west-dunbarton.gov.uk](mailto:Pamela.Clifford@west-dunbarton.gov.uk)

**Appendix:** None

**Background Papers:**

1. Application documents and plans
2. West Dunbartonshire Local Plan 2010
3. West Dunbartonshire LDP - Proposed Plan
4. West Dunbartonshire LDP - Proposed Plan 2
5. Consultation responses
6. Representations

**Wards affected:** Ward 4 (Kilpatrick)

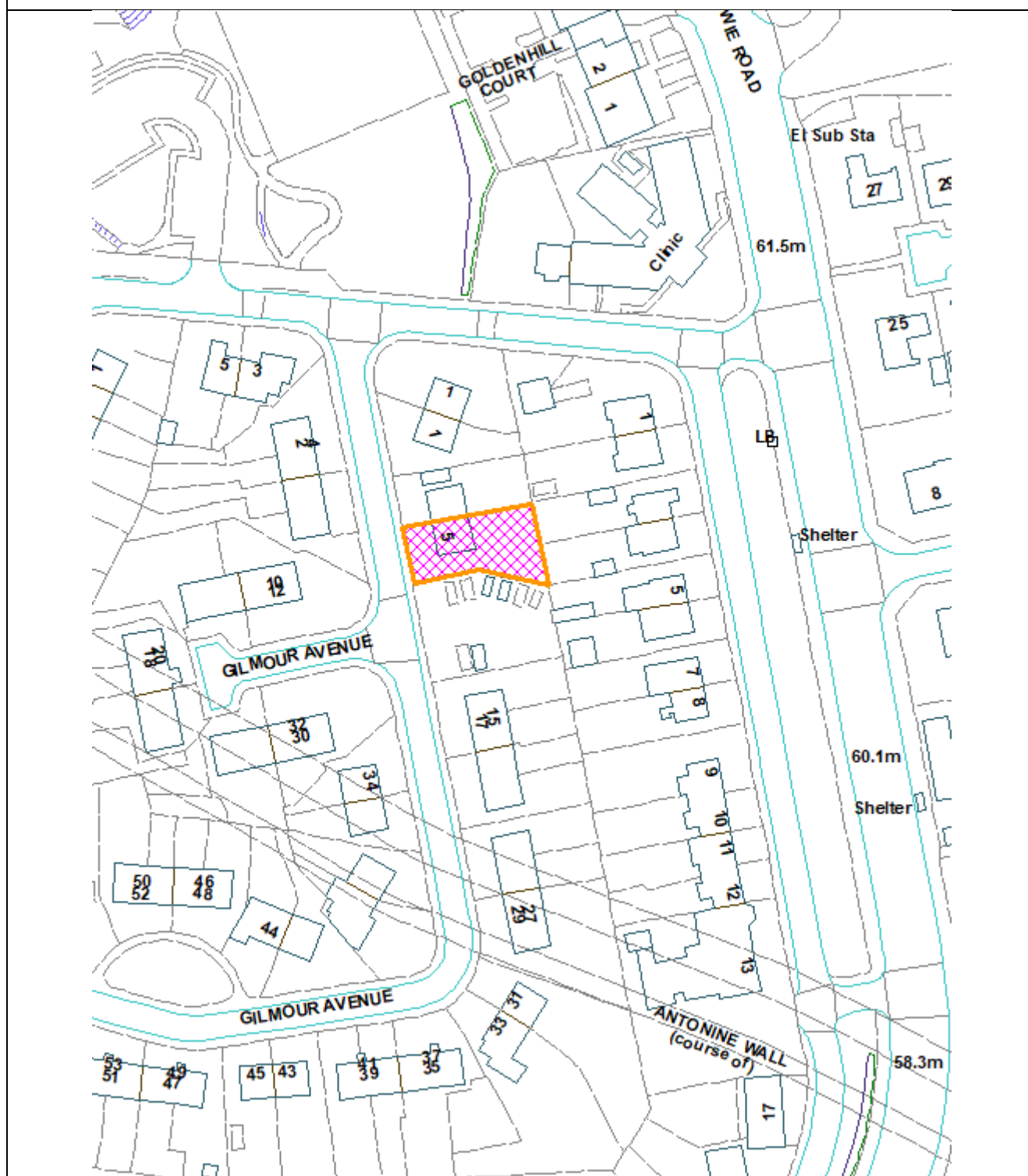
Map Register No: HQ646

Date: 16 September 2020

DC20/116

Erection of outbuilding for  
use as a commercial and  
personal training studio  
and gym

5 Gilmour Avenue  
Hardgate  
Clydebank  
G81 6AN





**WEST DUNBARTONSHIRE COUNCIL****Report by the Strategic Lead – Regulatory****Planning Committee: 30 September 2020**

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**Subject: Development Plan Scheme and Participation Statement 2020****1. Purpose**

- 1.1** To update the Committee on the progress of the Development Plan and to seek approval of the annual review of the Development Plan Scheme and Participation Statement.

**2. Recommendation**

- 2.1** It is recommended that the Committee approve the Development Plan Scheme and Participation Statement contained in Appendix 1.

**3. Background**

- 3.1** The Development Plan sets out the strategy and policies for the use of land. The statutory Development Plan for the West Dunbartonshire planning authority area currently comprises the Glasgow and the Clyde Valley Strategic Development Plan and the West Dunbartonshire Local Plan. The West Dunbartonshire Local Development Plan 1 (Proposed Plan) 2016 and the Local Development Plan 2 (2020) are material considerations. The National Park Authority prepares the Development Plan for the National Park area within the West Dunbartonshire Council area. At the time of writing this Committee Report, Local Development Plan 2 had been submitted to the Scottish Government for adoption. As detailed in paragraph 4.3 below, the Plan will become the adopted Plan on 23 September 2020, unless otherwise directed by the Scottish Government and will replace the West Dunbartonshire Local Plan 2010 and Local Development Plan 1: Proposed Plan (2016) on adoption.
- 3.2** A requirement of the Development Plan process is the preparation of a Development Plan Scheme and Participation Statement, and its review on an annual basis. The Development Plan Scheme sets out which Development Plans are in effect in the authority area and the timetable for their replacement. The Participation Statement sets out how the Council will encourage participation in the preparation of the new Local Development Plan.

#### **4. Main Issues**

##### West Dunbartonshire Local Plan

- 4.1** The West Dunbartonshire Local Plan, adopted in March 2010, remains the most up to date adopted Local Plan covering the West Dunbartonshire planning authority area. At the time of writing this report, the 2010 Local Plan remains a statutory part of the Development Plan and will be used in the assessment of planning applications until it is superseded by Local Development Plan 2.

##### West Dunbartonshire Local Development Plan (Proposed Plan) 2016

- 4.2** The above plan remains at proposed plan stage as a Scottish Ministers Direction prevented the plan from being adopted unless the Duntiglennan Fields site in Clydebank was included as per the Examination Report. At the time of writing this report, the Proposed Plan (2016), incorporating the accepted recommended modifications of the Examination Report, is a material consideration in the determination of planning applications and will remain until superseded by Local Development Plan 2.

##### West Dunbartonshire Local Development Plan 2

- 4.3** The Council agreed to adopt Local Development Plan 2 on 19<sup>th</sup> August 2020. The notice of intention to adopt the Plan was publicised on 26<sup>th</sup> August 2020. Local Development Plan 2 was also forwarded to the Scottish Government on the same date. Unless directed otherwise, or the time frame is extended, the Council will be able to adopt the Plan 28 days after submission to the Scottish Ministers, which would be 23 September 2020. On adoption, Local Development Plan 2 will replace the West Dunbartonshire Local Plan 2010 and Local Development Plan 1: Proposed Plan (2016). It will then form the development plan for West Dunbartonshire alongside Clydeplan.

##### Glasgow and the Clyde Valley Strategic Development Plan (Clydeplan)

- 4.5** The current Glasgow and the Clyde Valley Strategic Development Plan, known as Clydeplan, was approved by Scottish Ministers on 24<sup>th</sup> July 2017. Currently, the Clydeplan team are working on the Regional Spatial Strategy which is required under the new Planning (Scotland) Act 2019, which will replace Strategic Development Plans.

##### Loch Lomond and the Trossachs National Park Local Development Plan

- 4.6** The Loch Lomond and the Trossachs National Park Local Development Plan 2017 – 2021 was adopted in December 2016 and replaces the former Local Plan (2011 - 2015).

##### Planning (Scotland) Act 2019

- 4.7** The development plan scheme and participation statement requires the Council to provide a timeframe for the publication of the next Local Development Plan. The Council will prepare a new Local Development Plan 3 under the new Planning System when it has been fully implemented.

- 4.8** However, the timeframes for producing Local Development Plan 3 will not be known until the provisions of secondary legislation have been produced. Therefore, the 2020 development plan scheme and participation statement cannot provide an accurate timeframe for the production of the next Plan. Until that happens, the Council will publish the Action Programme for Local Development Plan 2; undertake preparatory work for the Evidence Report, which is the first stage in the development of a new Plan under the new system; undertake an Urban Capacity Study reviewing the land supply in West Dunbartonshire; and progress the Council's approach to Locality Place Plans.

## **5. People Implications**

- 5.1** There are no personnel issues associated with this report.

## **6. Financial and Procurement Implications**

- 6.1** There are no financial or procurement implications associated with this report.

## **7. Risk Analysis**

- 7.1** There are no known risks associated with this report.

## **8. Equalities Impact Assessment (EIA)**

- 8.1** The new Local Development Plan has been subject to an Equality Impact Assessment.

## **9. Environmental Sustainability**

- 9.1** A Strategic Environmental Assessment is not required.

## **10. Consultation**

- 10.1** The Development Plan Scheme and Participation Statement attached at Appendix 1 sets out what consultation has been undertaken and what is still to be undertaken in the forthcoming stages of Local Development Plan 2.

## **11. Strategic Assessment**

- 11.1** Local Development Plan 2 has a strong focus on delivering regeneration, placemaking and design. It is considered to contribute to all of the Council's strategic priorities.

**Peter Hessett**  
**Strategic Lead - Regulatory**  
**Date: 30 September 2020**

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**Person to Contact:** Pamela Clifford, Planning & Building Standards Manager,  
[pamela.clifford@west-dunbarton.gov.uk](mailto:pamela.clifford@west-dunbarton.gov.uk)  
0141 951 7938

Antony McGuinness, Team Leader – Forward Planning,  
[antony.mcguinness@west-dunbarton.gov.uk](mailto:antony.mcguinness@west-dunbarton.gov.uk)  
0141 951 7948

**Appendices:** Appendix 1: Development Plan Scheme and Participation Statement

**Background Papers:** None

**Wards Affected:** All



# **LOCAL DEVELOPMENT PLAN 2**

**Development Plan Scheme and  
Participation Statement**

**West**   
**Dunbartonshire**  
COUNCIL

**September 2020**

## **West Dunbartonshire Development Plan Scheme and Participation Statement**

### **Introduction**

This document provides information on existing Development Plan coverage within West Dunbartonshire and the timetable for preparing replacement plans. It also sets out the means by which participation in the preparation of the new Local Development Plan is being undertaken.

### **What is a Development Plan?**

It is a document which sets out policies and proposals for the use of land and buildings in the area it covers. It does this by identifying uses that will be acceptable on particular sites and by setting out criteria for the assessment of development proposals.

### **What types of Development Plan are there?**

There are two tiers of Development Plan currently in operation within Scotland.

Strategic Development Plans are applicable to the city-regions of Glasgow, Edinburgh, Aberdeen and Dundee, and cover more than one local authority area. These set out a long-term vision for the development of the city-regions and provide a framework for Local Development Plans.

Local Development Plans provide a more detailed planning framework, usually at a local authority level, setting out policies for assessing planning applications and identifying sites for development or protection from development.

Local Development Plans can be supported by Supplementary Guidance, which provides more detailed information on specific issues. Councils can also produce non-statutory Planning Guidance, which does not form part of the Development Plan

### **What Development Plans are currently in place in West Dunbartonshire?**

The Strategic Development Plan for the area: Clydeplan, was approved by Scottish Ministers on 24<sup>th</sup> July 2017 and covers the West Dunbartonshire Council area with the exception of that area within the Loch Lomond and the Trossachs National Park. Further information, including the Development Plan Scheme and Participation Statement for Clydeplan, can be found at [www.clydeplan-sdpa.gov.uk](http://www.clydeplan-sdpa.gov.uk).

The West Dunbartonshire Local Plan was adopted in March 2010. The West Dunbartonshire Local Development Plan remains at proposed plan stage and this plan was not adopted. The Examination Report included a recommendation that a site at Duntiglenan Fields in Clydebank be included as a housing development opportunity. At its meeting of 27 April 2016, the Planning Committee declined this recommendation and the Scottish Ministers directed that the Plan is not to be adopted unless the recommendation is accepted. The plan, incorporating the accepted modifications of the Examination Report, is referred to as the West Dunbartonshire Local Development Plan: Proposed Plan (2016). This, along with the adopted Local Plan will continue to be material considerations in the determination of planning applications. Both plans cover West Dunbartonshire with the exception of that area within the National Park.

Local Development Plan 2 was submitted to the Scottish Ministers for adoption on 26 August 2020. Unless the Scottish Government direct otherwise, or the timeframe is extended, the Council are free to adopt Local Development Plan 28 days after submission to the Scottish Government. At the time of writing this Development Plan Scheme and Participation Statement, adoption of Local Development Plan 2 was expected to be confirmed on 23rd September 2020. On adoption, Local Development Plan 2 will replace both the West Dunbartonshire Local Plan 2010 and Local Development Plan 1: Proposed Plan (2016) and alongside Clydeplan, will form the development plan for West Dunbartonshire.

In addition, the Council has produced Planning Guidance associated with the Proposed Plan, non-statutory Planning Guidance and other useful information documents which are listed in Appendix 1.

The Plan covering the National Park is the Loch Lomond & the Trossachs National Park Local Development Plan 2017 - 2021. Further information, including the Development Plan Scheme and Participation Statement for the Park Authority Local Development Plan can be found at [www.ourlivepark.com](http://www.ourlivepark.com).

## What are the main stages in preparing the new Local Development Plan?

The significant documents and stages in the preparation of the new Local Development Plan are as follows:

**Survey and Technical Work and Early Participation** – survey and technical work undertaken to inform the Main Issues Report and stakeholders consulted with regard to their requirements for the Plan.

**Main Issues Report** – the findings of the survey and technical work and early participation is reflected in the Main Issues Report, which sets out the issues to be addressed by the Local Development Plan and the options for addressing these. Issues will focus on the main changes between the existing Local Development Plan and the new Plan.

**Proposed Plan** – the Proposed Plan is a consultative version of the Plan published as the Council would like to see it adopted.

**Submission to Scottish Ministers** – following consultation and amendment if necessary the Proposed Plan is submitted to Scottish Ministers for examination.

**Examination** – if there remain unresolved representations to the Proposed Plan when submitted to the Scottish Ministers, a Reporter will be appointed to examine the Plan.

**Adoption** – following an examination of the Plan, it is modified if necessary and then published for adoption.

### Planning (Scotland) Act 2019

The Planning (Scotland) Act 2019 makes fundamental changes to the development planning system in Scotland. The first step towards implementing this new system is the preparation and publication National Planning Framework 4 (hereafter referred to as NPF 4). The draft NPF 4 is to be laid before Parliament in September 2021 and will be consulted upon with the revised NPF 4 forming the development plan alongside a new Local Development Plan in September 2022. Secondary legislation and guidance to implement the new development planning system is not anticipated by the Scottish Government to be in place before December 2021; however, due to the Covid 19 situation this date may be subject to revision by the Scottish Government.

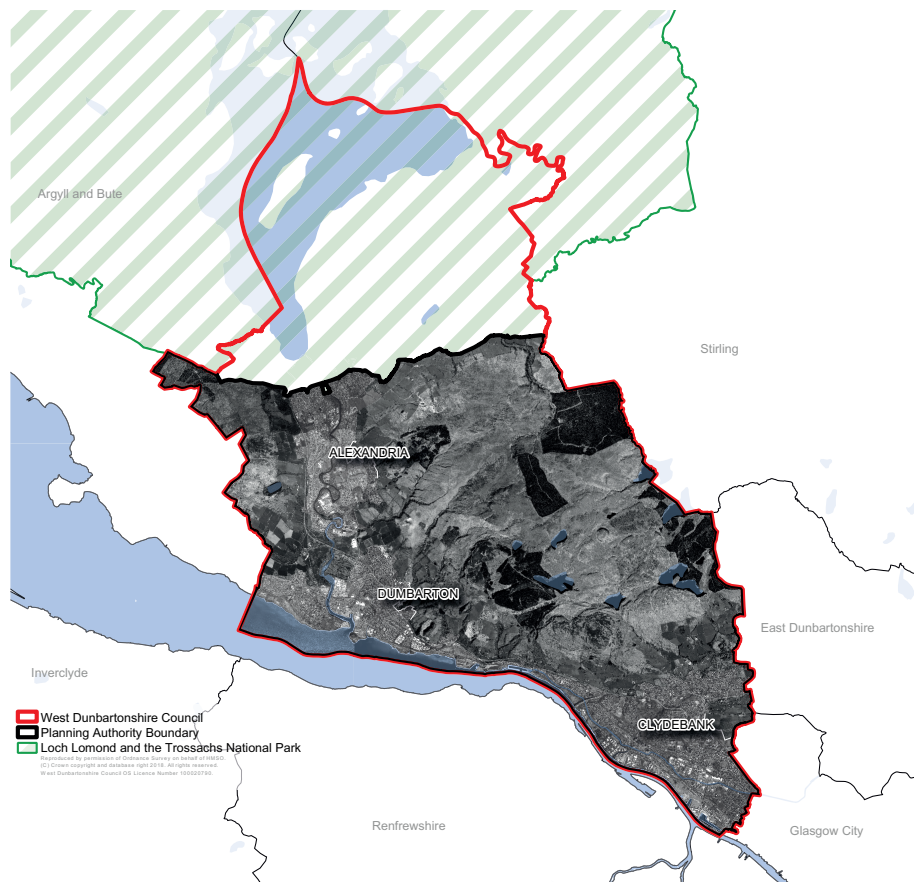
Therefore, as Local Development Plan 2 will only be recently adopted, the Council will wait and prepare its new Local Development Plan 3 under the new planning system. Until the secondary legislation and guidance has been published there are no firm dates that the Council can give for the preparation of Local Development Plan 3; however, the Council will start preparing and undertaking work required for the Evidence Report, which is the first step in the preparation of Local Development Plan 3 under the new system.

## Timetable for the preparation of the new Local Development Plan

<b>Publication of Main Issues Report – June 2017</b>
<b>Main Issues Report Consultation – June 2017 to September 2017</b>
<b>Preparation of Proposed Plan – October 2017 to September 2018</b>
<b>Publication of Proposed Plan – October 2018</b>
<b>Proposed Plan Consultation – October 2018 to November 2018</b>
<b>Submission of Proposed Plan to Scottish Government – May 2019</b>
<b>Examination – May 2019 to April 2020</b>
<b>Adoption of Plan – September 2020</b>
<b>Publication of Action Programme – December 2020</b>
<b>Undertake preparatory work for the Local Development Plan 3 Evidence Report – December 2021</b>

### Strategic Environmental Assessment

The Council has undertaken a Strategic Environmental Assessment of the new Local Development Plan. An Environmental Report was published alongside the Main Issues Report and the Proposed Plan. The Environmental Report was subject to consultation alongside the Main Issues Report and Proposed Plan.



### Participation

The Council is keen to encourage participation in the preparation of the new Local Development Plan (LDP).

#### Who can participate in the preparation of the new Plan?

Participation in the plan-making process is open to everyone: from individuals, community groups and small local businesses to investors, land owners, housebuilders and national and international companies.

## When will participation happen?

Participation will continue to be encouraged throughout the preparation of the new Local Development Plan, but the key stages are pre-Main Issues Report and the consultation phases following the publication of the Main issues Report and the Proposed Plan. All consultation detailed above has been undertaken in line with the participation statement.

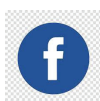
As detailed above, the Planning (Scotland) Act 2019 will not be fully implemented until secondary legislation and guidance has been published in December 2021. Therefore, the new requirements for consultation and participation were not known at the time this participation statement was written. However, the Council will encourage participation throughout the development of Local Development Plan 3 when the new planning system has been fully implemented.

## Contact us and register for participation

A Local Development Plan participant database has been established to record the details of all individuals and organisations that want to be kept up to date with the Local Development Plan process. Interested individuals or organisations can have their details added to the database by contacting us through any of the methods below -

**Email:** [ldp@west-dunbarton.gov.uk](mailto:ldp@west-dunbarton.gov.uk)  
**Telephone:** 0141 951 7930  
**E-form:** [Click here](#)  
**Write:** Forward Planning  
Planning & Building Standards  
West Dunbartonshire Council  
16 Church Street  
Dumbarton  
G82 1QL

Further information and key documents can be found online at –  
[www.west-dunbarton.gov.uk/council/strategies-plans-and-policies/local-development-planning/local-development-plan/](http://www.west-dunbarton.gov.uk/council/strategies-plans-and-policies/local-development-planning/local-development-plan/)



<https://www.facebook.com/WDCouncil>



<https://twitter.com/wdcplanning>



### How will the Council manage and encourage participation?

The following tables outline what methods will be used to encourage and undertake participation at each stage of the LDP process.

Pre-MIR Stage – Background work& evidence gathering		
Who will be involved?	Publicity and Participation Methods	Progress at September 2020
Scottish Government, Key Agencies, developers, planning consultants, land owners, members of the public, Community Councils and other community groups, other Council services, and adjoining and Clydeplan planning authorities	Invitation to participate and call for sites and issues will be sent out to all potentially interested parties.	Complete
	Development Plan Scheme & Participation Statement will be placed on Council website, along with online form to register for participation and submit sites/ issues.	Complete
	Social media and local newspapers will be used to publicise the commencement of new Local Development Plan preparation.	Publicised on social media
	A newsletter will be sent to all registered participants, and made available online and in Council offices and libraries.	Complete
	Meetings/workshops will be held with Key Agencies, other Council services, and interested individuals and organisations, including community organisations, to explore potential main issues.	Complete
	Meetings will be held with elected members on a ward and/or individual basis and workshops will be held with the Planning Committee.	Complete

<b>MIR Stage – Main Issues Report, Monitoring Statement &amp; Environmental Report</b>		
<b>Who will be involved?</b>	<b>Publicity and Participation Methods</b>	<b>Progress at September 2020</b>
Scottish Government, Key Agencies, developers, planning consultants, land owners, members of the public, Community Councils and other community groups, other Council services, adjoining and Clydeplan planning authorities	Publish Main Issues	Complete
	Place publication notice in local papers.	Complete
	Notification of Main Issues Report publication and invitation to comment will be sent out to all stakeholders and those on participant database via email and letter.	Complete
	Social media will be used to publicise the Main Issues Report and encourage participation.	Publicised on social media
	Meetings will be offered and sought to discuss Main Issues Report.	Complete
	Information sessions will be held across the Council area to discuss Main Issues Report and encourage engagement.	Complete
	A newsletter will be published to inform of Main Issues Report publication. This will be sent to all participants, made available online and in Council offices and libraries.	Complete

Proposed Plan Stage – Proposed Plan, Proposed Action Programme & Revised Environmental Report (if necessary)		
Who will be involved?	Participation Methods	Progress at September 2020
Scottish Ministers, Key Agencies, owners/occupiers/lessees of land or neighbouring land identified in the proposed plan for development, those who commented on the MIR, any other interested parties involved at the previous stage.	Publish Proposed Plan online and place copies in all libraries and main Council Offices.	Complete
	Place publication notice in local papers.	Complete
	Owners, lessees, occupiers of sites (or neighbouring land) which the Proposed Plan specifically proposes to be developed (and which would have a significant affect on the use and amenity of the site) will be notified.	Complete
	Notification of Proposed Plan publication will be sent to all those who participated at Main Issues Report stage, other stakeholders and those on the participant database.	Complete
	Social media will be used to publicise Proposed Plan publication.	Publicised on social media.
	Meetings will be offered and sought to discuss Proposed Plan and resolve any issues where possible.	Complete
	Information sessions will be held across the Council area to discuss Proposed Plan and encourage engagement.	Complete
	A newsletter will be published to inform of Main Issues Report publication. This will be sent to all participants, made available online and in Council offices and libraries.	Complete

## **Final Stages - Modifications, submission to Scottish Ministers, Examination & Adoption**

Following the end of the period allowed for representations to be made on the Proposed Plan, the Council may make Proposed Modifications to the Plan but only to take account of representations, consultation responses or minor drafting/technical matters. If Proposed Modifications are made a modified version of the Proposed Plan will be published and the same method of publicity and participation will apply at this stage as at the stage of the original Proposed Plan.

When the Proposed Plan is submitted to the Scottish Ministers a proposed Action Programme must also be submitted. This sets out a list of the actions required to deliver each of the Plan's policies and proposals, along with who is to carry out the action and a timescale.

Another requirement at this stage will be to submit a report of conformity with the participation statement.

If there are unresolved representations the Scottish Ministers will appoint a Reporter to examine the plan.

Following examination, the recommendations of the Examination Report will be incorporated into the Plan, unless there is justification not to do so. A version of the Plan incorporating the recommended modifications will then be published and sent to the Scottish Ministers and a notice of intention to adopt the Plan placed in the local press. The Council will then adopt the Plan 28 days after advertisement of intention to adopt unless the Scottish Ministers advise or direct that this should not happen.

3 months after Adoption of the Plan, the Council is required to publish the Plan's Action Programme and review this every 2 years.

### Appendix 1: Guidance and other Useful Information

#### Planning Guidance - Local Development Plan (Proposed Plan) 2016

Residential Development: Principles for Good Design, 2014

Our Green Network, 2015

Renewable Energy, 2016

#### Planning Guidance

Dumbarton Waterfront Path Planning Guidance, 2017

Clydebank Business Park Planning Guidance, 2017

Alexandria Town Centre Masterplan, 2008

Clydebank Design Guidelines (Page and Park), 2003

Commuted Payments for Parking 2015

Frontiers of the Roman Empire (Antonine Wall) World Heritage Site, 2011

Pay Day Lending & Betting Shops, 2016

#### Other Useful Information

Alexandria Streetscape Design Guide, 2007

Bowling Basin Charrette Report 2014

Clyde Riverside Masterplan, 2000

Clydebank Town Centre Charrette report, 2015

Dumbarton Town Centre Public Realm Design Guide, 2001

Dumbarton Town Centre & Waterfront – Revised Urban Strategy, 2014

Dumbarton Rock & Castle Charrette Report, 2015

Kilpatrick Hills Local Landscape Area: Statement of Importance, 2015

#### Protocols on Facilitating Appropriate Development on Key Development Sites

Protocol 1: Pre-Application Advice

Protocol 2: Liaison Meetings

Protocol 3: Elected Member Involvement in Pre-application Discussions

Protocol 4: Planning Processing Agreements

#### Development Management Advice Notes and Design Guides

Fences, walls, enclosures

Driveways

Decking

Satellite Dishes

Extensions & Conservatories

Domestic Garages & Garden Outbuildings

Porches

Dormers & Velux Windows

Shopfront Design Guide

## CONTACT DETAILS

Planning and Building Standards  
Council Offices  
16 Church Street  
Dumbarton  
G82 1QL  
Telephone: 0141 951 7948  
Email: [ldp@west-dunbarton.gov.uk](mailto:ldp@west-dunbarton.gov.uk)

## OTHER FORMATS

This document can be made available on request in alternative formats such as large print, Braille, audio tape or computer disc as well as in five community languages.

本文件也可應要求，製作成其他語文或特大字體版本，也可製作成錄音帶。

अनुरोध पर यह दस्तावेज़ अन्य भाषाओं में, बड़े अक्षरों की छपाई और सुनने वाले माध्यम पर भी उपलब्ध है

ਇਹ ਦਸਤਾਵੇਜ਼ ਹੋਰ ਭਾਸ਼ਾਵਾਂ ਵਿਚ, ਵੱਡੇ ਅੱਖਰਾਂ ਵਿਚ ਅਤੇ ਆਡੀਓ ਟੇਪ 'ਤੇ ਰਿਕਾਰਡ ਹੋਇਆ ਵੀ ਮੰਗ ਕੇ ਲਿਆ ਜਾ ਸਕਦਾ ਹੈ।

درخواست پر یہ دستاویز دیگر زبانوں میں، بڑے حروف کی چھپائی اور سننے والے ذرائع پر بھی میسر ہے۔

هذه الوثيقة متاحة أيضا بلغات أخرى والأحرف الطباعة الكبيرة وبطريقة سمعية عند الطلب.

## **WEST DUNBARTONSHIRE COUNCIL**

### **Report by the Strategic Lead - Regulatory**

**Planning Committee: 30<sup>th</sup> September 2020**

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**Subject:       Queens Quay Design Codes**

#### **1.       Purpose**

- 1.1**    To advise Committee of the outcome of the consultation on the draft Queens Quay Design Codes and to seek approval to adopt the Guidance.

#### **2.       Recommendation**

- 2.1**    It is recommended that the Committee agrees the proposed changes to the guidance and agrees that the Design Codes are adopted as non-statutory planning guidance.

#### **3.       Background**

- 3.1**    The Committee will be aware that a report was presented to the June Planning Committee on the Queens Quay Design Codes outlining the responses received to the consultation undertaken during the 9 week consultation period. The report is contained in Appendix 1. It was indicated that a further report detailing the Council's responses to the comments received from the consultation process would be presented to the Planning Committee. A total of 25 responses were received to the consultation including 4 from key agencies and 21 from the landowner, design professionals, individual developers, individuals, the local MP, Council Services, partner organisations and community groups.

#### **4.       Main Issues**

- 4.1**    The Design Codes set out guidance for designers and developers and will provide a structured approach to assess planning applications on Queens Quay. The majority of the responses received to the consultation were supportive of the role and content of the guidance. Some of the comments made suggestions beyond the role of the Design Codes such as relating to public art and street naming and these will be taken forward within the respective annexes. Appendix 2 summarises the comments received and outlines the Council's proposed response to these comments. The full content of the comments will be made available on the Council's website alongside the Design Codes themselves.
- 4.2**    Many of the respondents indicated that the intention of the guidance is clear from the outset to ensure that a high standard of design quality is maintained throughout the entire development to deliver the masterplan vision. The

Design Codes try to get the correct balance between being overly prescriptive but encouraging design innovation and achieve quality development. The Design Codes have been modified to address the majority of comments received and the landowner has been extensively consulted on the content of the Design Codes. Appendix 3 contains the modified Design Codes.

Key changes to the document are summarised below:

- Greater clarity about the form of the Waterfront development basing it more on key design principles and on development forms;
- Clarification on street hierarchy, typology and building heights;
- Eastern edge of the basin - identify for mixed use and design principles to be used to guide future building form;
- Strengthen use of traditional materials and connections to the wider area in terms of Kilpatrick Hills, River Clyde;
- More detail on landscape strategy and clarification on Sustainable Drainage Systems;
- Clarity on the masterplan and approved planning permissions;
- Revisiting some of the images, diagrams and plans to ensure consistency and accuracy; and
- Addressing detailed building features such as balconies and windows.

#### Health Assessment

- 4.3** The Design Codes also have been assessed from a health perspective as place-based approaches can have a positive impact on health outcomes over time and these Design Codes form part of a wider strategy to influence future population health. The Design Codes have been developed on the basis to improve health and wellbeing from the outset based on a place based approach. The Design Codes will have significant impact on the people who live, work and visit Queens Quay and it is intended that Queens Quay will become a place to encourage aspiration and prosperity. The elements in the Design Codes are in line with the dimensions of the Place Standard Tool and the PHE Spatial Planning for Health :An evidence resource for planning and designing healthier place. From the health impact assessment it indicates that the overall development should be inclusive and types of housing adapted to peoples needs to become a lifetime neighbourhood in terms of public realm improvements, encouraging public transport, natural space, play and recreation and public art. These health impacts have been addressed in the Design Codes.

#### Next Steps

- 4.4** If the Committee agree to the Design Codes being adopted as non-statutory planning guidance then the Design Codes will be a material consideration in the determination of planning applications on Queens Quay. There are a number of annexes associated with the Design Codes - the Queens Quay



Sustainability Annexe, Street Naming Strategy Annexe and Arts Strategy, which require to go through a full consultation process. Once this has taken place the Design Codes and associated annexes will be presented to the Planning Committee to be agreed as Statutory Supplementary Guidance which will form part of Local Development Plan 2.

## **5. People Implications**

**5.1** There are no personnel issues associated with this report.

## **6. Financial Implications**

**6.1** There are no financial issues associated with this report.

## **7. Risk Analysis**

**7.1** It was not considered necessary to carry out a risk assessment on the matters covered by this report.

## **8. Equalities Impact Assessment (EIA)**

**8.1** Screening has been carried out on the Design Codes Document.

## **9. Consultation**

**9.1** Details of the consultation are set out in Section 3.1 above and the responses to the consultation are set out in Appendix 2 of the report.

## **10. Strategic Assessment**

**10.1** The guidance is considered to support the Council's strategic priority of improving economic growth and employability.

**Peter Hessett**  
**Strategic Lead - Regulatory**  
**Date: 30<sup>th</sup> September 2020**

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**Appendices:** Appendix 1 – Planning Committee Report – 10<sup>th</sup> June 2020  
Appendix 2 – Summary of consultation responses and proposed council response  
Appendix 3 – Queens Quay Design Codes and Health Impact Assessment (Appendix 1)

**Background Papers:** Committee report: Draft Design Codes Committee Report  
12<sup>th</sup> February 2020.  
Equalities Impact Assessment Screening

**Wards Affected:** Ward 6 Clydebank Waterfront.

**WEST DUNBARTONSHIRE COUNCIL****Report by the Strategic Lead - Regulatory****Planning Committee: 10<sup>th</sup> June 2020**

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**Subject: Queens Quay Design Codes: Consultation Responses and Street Naming Strategy**

**1. Purpose**

- 1.1** To inform the Committee of the main points of the representations received from the consultation exercise, details of the Proposed Street Naming Strategy and to advise on the next steps.

**2. Recommendation**

- 2.1** That the Committee notes the main points raised within the consultation responses which will shape the finalised design codes which will be presented to the August Planning Committee for approval.
- 2.2** That the Committee approve the draft Street Naming Strategy Annexe and that details of the street names based on the Strategy will be presented to the August Planning Committee for approval.

**3. Background**

- 3.1** A Design Framework, including Masterplan has been approved for the central 23 hectares of the site and Planning Permission in Principle has been granted for up to 1,000 new houses, commercial, health and leisure uses, public realm and road infrastructure. The Design Codes built upon these approved documents by providing detailed guidance regarding the future development form in particular the housing element.
- 3.2** The Draft Queens Quay Design Codes were approved for consultation by Planning Committee on 12<sup>th</sup> February 2020. Appendix 1 includes the Design Code document. The guidance was published for consultation on 17<sup>th</sup> February 2020 for a 9 week period, with comments sought by 20<sup>th</sup> April 2020. The guidance was made available on the Council's website, in libraries and sent to relevant organisations such as community councils, housebuilders, housing associations and relevant Council services. It was also sent directly to relevant landowners/developers and advertised in the local press. In light of the disruption of COVID 19 the consultation period was extended by another 4 weeks to the 20<sup>th</sup> May 2020.

#### **4. Main Issues**

- 4.1** In response to the consultation, 25 responses were received: 4 from key agencies and 21 from design professionals, individual developers, individuals, Council Services, partner organisations and community groups. This included the landowner and the local MP. Some of the responses received were anonymous. A short summary on the responses received is provided below together with initial responses.

##### Key Agency Consultees

- 4.2** The comments from the Key Agencies centred on the features of the landscape approach applied across the site that related to the specific discipline of the agency/consultee.

This included;

- Open Space provision
- Sustainable Urban Drainage solutions
- Diversity in tree and planting species
- Measure to protect the Wintering Redshank.
- Relationship of development to the Titan Crane

- 4.3** The comments are noted and the Design Codes could provide guidance around the approaches to SUDS and the diversity of planting species to compliment the site wide approaches to this. This could be linked to the planning permissions on the site and the masterplan.

##### Responses from design professionals/developers/ individuals/council services and community groups

- 4.4** The majority of the respondents agreed that the document was easy to use and navigate due to the formatting and approach taken and that the use of photographic examples of 'preferred' and 'not preferred' development details was particularly helpful. It was felt by respondents that the document would give comfort to developers that their development would sit within a community of consistently high standards of development, while giving room for architectural interpretation and expression. Some parties suggested that the level of prescribed detail should be increased to be more aligned with more prescriptive Design Codes, while others suggested the existing codes were too prescriptive. The respondents also helpfully identified the areas of the document where they felt this applied and where further clarity would be beneficial in communicating what the Council wished to see.

The main points of responses are summarised as follows:

- Clarity over the level of detail and prescription contained in the document and what falls to developer design.
- The importance of maintaining consistency of the quality of development proposals over time.
- Clarity around the areas that the Design Codes can influence and those that fall under the remit of the Planning in Principle Permission.

- Additional detail around the character and built form of the waterfront in the diagrams, explanations and images.
- More information on roads and parking infrastructure.
- Consideration of the local climate and the impact on development.
- Suggestion to include details of public art in the document.
- Consideration of street naming to link in with addressing the culture and history of the site, including pre-industrial heritage.
- The inclusion of health and wellbeing as a thread underpinning the document.

**4.5** Many of the comments received through the consultation response will require further consideration to ensure that there is clarity around the detail, the purpose and application of the Design Codes. It has always been envisaged that the Design Codes document would strike a balance between providing guidance and being overly prescriptive to deliver high quality places. This would be integrated into the planning and design processes that would shape the Queens Quay environment. An over prescriptive document would weaken the approach and vision for the Queens Quay site, however, a level of guidance and advice that still allows freedom for developers to innovate would be encouraged with marketability and commercial viability would be embedded in the Design Codes.

#### Response from the landowner

**4.6** The landowner/developer (CRL) responded on the online survey and this was supplemented further with a detailed review of the Design Codes prepared by a consultant architect firm in order to communicate more fully their views on the Codes. The document 'Queens Quay Design Guidelines Review' acknowledges that the aspirations to realise a high quality development that creates distinctive and memorable regeneration of the site are goals that are shared between the Local Authority and the developer. The importance of balancing flexibility, creative response and the shifting economic and social picture is highlighted in the document.

The main points of responses are summarised as follows:

- The document is aligned in many respects with the approach taken in the Design Codes to offer a flexible way forward for developers with enough detail to create cohesion across the whole site.
- Demonstrates possible detailed approaches to the design of the development plots and streets leading to the waterfront while highlighting where further information would be beneficial.
- Seeks additional detail around roof pitches, massing and building heights.
- It mentions the section on 'materiality and robustness' as a particular strength of the Design Codes.
- It shows the eastern edge of the basin and the mixed use development as pavilion blocks with surface parking (which is a departure from the Masterplan).

- Suggests the need for confirmation from council services on the acceptance of the proposed parking and roads strategies in the Design Codes.
- Looks for more detail and emphasis on the importance of the design quality on the waterfront including giving more scope for variation and flexibility in massing and height.
- Suggests that the Design Codes may be inadvertently promoting suburban development over urban and more clarity is required around this.
- Seeks clarity over some of the diagrams, including the treatment of the mews area, street widths that are dictated by the masterplan, the nature of the street/road and the approach to landscaping and character areas, especially around the basin.

**4.7** The detailed response from the landowner is welcomed and the Review document which critiques the Design Codes offers clarity around where the ambitions of the Council and the developer are aligned. The suggested built form diagrams and drawings shown in the Review document give weight to the principles set in the Design Codes while demonstrating what can be achieved when using the Design Codes to take forward development on the various plots. The Review document assists in giving some direction as to where more clarity or more detail is required, for example; the response demonstrated that the reference to ‘pavilions’ at the waterfront could be misconstrued and that a very literal interpretation of the diagrams could be taken by a prospective designer. This submission will facilitate further discussion before the Design Codes are finalised.

#### Annexes to the Design Codes

**4.8** The work done around the Design Codes by officers and some of the responses has suggested that further work to develop strategies around sustainability, street naming, heritage and the arts should be developed and taken forward. Already, work has commenced on an Energy Strategy annexe which will encourage a ‘fabric first’ approach in the design of the future homes on Queens Quay. This will ensure the sustainability credentials of new development work cohesively with the new energy centre and this will be presented to a future Planning Committee for approval.

- Annexe 1: Queens Quay Energy Strategy
- Annexe 2: Queens Quay Street Naming Strategy
- Annexe 3: Queens Quay Arts Strategy

It is recognised that a well designed place, good quality housing and the sense of identity that comes from heritage and culture have positive effects on levels of health and wellbeing. The Design Codes and subsequent annexes for the Queens Quay site are intended to work as a suite of documents with health and well being strategies embedded as a cross cutting theme in all documents.

#### Annexe 2: Queens Quay Street Naming Strategy

- 4.9** A Street Naming Strategy will reflect the aspiration in the draft Queens Quay Design Codes to ensure the history and heritage of Clydebank and the surrounding area is referenced in the wider development. This document will form Annexe 2 to the final Queens Quay Design Codes and is contained in Appendix 2. This strategy seeks to proactively contribute to placemaking across the Queens Quay site, making known key heritage narratives and telling Clydebank's stories. It is intended that, through this innovative approach to street naming, both residents and visitors to the area alike will feel positively engaged in Clydebank's streetscape and the town's unique identity.
- 4.10** The strategy seeks to link with the other annexes and set out a contextualised and scalable framework for street naming that centres on a storytelling approach and a strong narrative. This may open up opportunities to create digitally available heritage interpretation and associated learning materials. Narratives to the history of Clydebank as a town of shipping and industry but may also relate to the pre burgh history or individuals such as Ian McHarg, a Clydebank born landscape architect of international reputation who offers a clear link to the wider landscape of the Kilpatrick Hills and beyond.

#### Responses from Elected Members Workshop

- 4.11** Elected members were presented with the Design Codes Draft Document at a workshop on the 5<sup>th</sup> December 2019. The document was well received and offered assurances that the development coming forward would be held to a high standard and that the approach across the site would be consistent. Members sought clarity on how the details provided can help to establish the character on a new development; they encouraged the use of colour on the site, guidance on garden sizes and boundary treatments onto streets and the approach to bin stores. The comments have been considered and will be taken forward with further clarity provided in the Design Codes document.

#### Response by Place and Design Panel

- 4.12** A Place and Design Panel session was held on 14<sup>th</sup> January 2020 where the Design Codes were presented. The Panel praised the ability of the document to tell prospective developers enough to ensure clarity around expectations but still offer flexibility for design. They could see the 'deep thinking' and 'rigour' that had been applied to the Design Codes and were in favour of the approach to limit prescription while driving quality and good design. The key areas where the Panel felt the document could be strengthened: additional detail around pavilion buildings on waterfront; the landscaping illustrations could be positioned first in the document; the application of a 5 year defects liability period to the landscaping; cycling should feature more in the document. The Panel also suggested that consideration should be given of what it takes to establish communities for example schools. The comments of the Panel are noted and that it is considered that there is scope to include recommendations around the landscape defects period, the inclusion of more focus on cycling and the addition of more detail around the possibilities for the pavilion blocks on the waterfront. Creating a sustainable community on this

site will be fundamental to the success of this site as well as for the wider Clydebank area.

#### Next Steps

- 4.13** Due to the level of detail raised within many of the consultation responses, further time is required to fully consider, respond and amend the Design Codes as appropriate. The representations received will result in changes to the document and these will also require to be discussed with the landowner before finalising the Design Codes, especially in relation to the comments raised to the document by the landowner themselves. A finalised version of the Design Codes, incorporating the Queens Quay Energy Strategy Annex, Street Naming Strategy Annex along with the full responses proposed to the points raised in the consultation will be presented to August Planning Committee for approval. A list of proposed street names for future streets in the Queens Quay development will also be presented to the August Planning Committee.

### **5. People Implications**

- 5.1** There are no personnel issues associated with this report.

### **6. Financial Implications**

- 6.1** There are no financial issues associated with this report.

### **7. Risk Analysis**

- 7.1** It was not considered necessary to carry out a risk assessment on the matters covered by this report.

### **8. Equalities Impact Assessment (EIA)**

- 8.1** Screening has been carried out on the Design Codes Document and the final document will address the issues raised in full.

### **9. Consultation**

- 9.1** Details of the consultation are set out in Section 4.1 above and the responses to the consultation are set out in the report.

### **10. Strategic Assessment**

- 10.1** The guidance is considered to support the Council's strategic priority of improving economic growth and employability.



**Peter Hessett**  
**Strategic Lead - Regulatory**  
**Date: 10<sup>th</sup> June 2020**

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**Appendices:** Appendix 1 –Queens Quay Design Codes Draft 2<sup>nd</sup>  
February 2019  
Appendix 2 – Proposed draft street naming strategy –  
Annexe 2.

**Background Papers:** Committee report: Draft Design Codes Committee Report  
12<sup>th</sup> February 2020.

**Wards Affected:** Ward 6 Clydebank Waterfront.

## APPENDIX 2: RESPONSES TO THE CONSULTATION ON THE DESIGN CODES AND THE COUNCIL'S OBSERVATIONS AND RECOMMENDED COURSE OF ACTION

Question 1: How would the Design Codes be relevant to you?	
Summary of Responses	Council Responses
<ul style="list-style-type: none"> <li>• Member of Parliament;</li> <li>• Developer;</li> <li>• Design professionals;</li> <li>• SEPA;</li> <li>• Historic Environment Scotland;</li> <li>• Community Member;</li> <li>• Glasgow and Clyde Valley Green Network Partnership;</li> <li>• Local community groups;</li> <li>• Academia;</li> <li>• Housebuilder – Homes for Scotland; and</li> <li>• Landowner – CRL/Dawn</li> </ul> <p>Unless otherwise indicated, these responses were anonymous and some of the responses were not directly to the survey.</p>	Noted.
Question 2: What is your overall view of the document in relation to how you would use it in your relevant professional capacity?	
Summary of Responses	Council Responses
The majority of responses to this question stated that the document was excellent or very useful. However, a few	Noted.

<p>responses noted that the document was average, poor or very poor. No explanation or reasons were given on why the respondents thought the document was poor or very poor.</p> <p>Detailed comments from the respondents were received to this question and these are summarised into the following issues below:</p> <ul style="list-style-type: none"> <li>• Context, content and layout of the Design Codes;</li> <li>• Intended use of the Design Codes;</li> <li>• Clarity of the Design Codes and what they are trying to achieve;</li> <li>• Level of prescription and scope for interpretation and design for development;</li> <li>• Needing to reflect the approved masterplan; and</li> <li>• Status of the guidance.</li> </ul>	<p>One of the respondents suggested having less background information and to introduce the design codes earlier in the document. While other respondents suggested more background information in order to set the context of the site and the document. It is considered that the information in the introduction provides the correct balance of setting out the historic and townscape context of the site and the Council's aspirations for high quality design. This will give a better understanding for future developers which they can respond appropriately. The examples shown of "preferred" and "not preferred" development will aid discussions during the planning application process. The guidance from the outset indicates that it will offer a structure approach to the assessment of planning applications on Queens Quay.</p> <p>The indicative masterplan and design framework which formed part of the Planning Permission in Principle sets out the foundations for development with the Design Codes building upon these guiding principles. The Codes are not intended to be prescriptive but offer structured guidance and in build flexibility to achieve high quality development. They encourage developers to take a range of innovative design approaches. They are not intended to inhibit good design rather to clarify the parameters, and ensure a consistency of approach across the</p>
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	<p>site.</p> <p>It is intended that the Design Codes are adopted as non-statutory planning guidance in the interim until consultation can take place on the various annexes and then adopted as supplementary guidance.</p>
<b>Question 3: By detailing traffic management/parking/waste strategies the council would be accepting of – will this give comfort or assurances about the expectations of such council service areas when designing? Are there other areas of detail we could look to incorporate?</b>	
Summary of Responses	Council Responses
<p>The majority of the responses to this question raised a number of detailed issues, which are summarised into the following issues:</p> <ul style="list-style-type: none"> <li>• Roads construction;</li> <li>• Parking and over spilling;</li> <li>• Access and traffic flow;</li> <li>• Road hierarchy and parking requirements;</li> <li>• Surface water drainage and SUDS;</li> <li>• Sustainable Waste Management; and</li> <li>• Involvement of the Roads and Waste Services in their preparation.</li> </ul>	<p>it is important that each residential development parcel has its own clearly defined parking and there is no overspill into these areas from other neighbouring uses. This will be carefully considered as each development proposal comes forward. The road hierarchy details, traffic flow and parking levels are provided within the document. 100% parking levels are proposed for the development plots furthest from the crane and town centre and 70% for the other development plots. The Roads Service is satisfied with proposed parking levels and have requested that visitor parking be considered on a site by site basis to ensure it is not abused by residents and assists in reducing on street inconsiderate parking.</p> <p>Parking levels have been minimised given the sustainable location of the site which is within walking distance of the town and other facilities, in close proximity to the train station and bus</p>

	<p>routes. Walking, cycling and the use of public transport is encouraged within the development proposals with Travel Plans given to new residents.</p> <p>Details are also provided for the routing of cleansing vehicles, to ensure adequate width and swept paths. Further details on surface water drainage and SUDS have also been provided within the document which is determined by the Planning in Principle Permission.</p>
<b>Question 4: Do you think the document would give you comfort that there will be coherence and consistency across the whole site particularly where there may be different developers delivering development over a long period of time?</b>	
<b>Summary of Responses</b>	<b>Council Responses</b>
<p>The majority of the responses answered yes to this question. Some of the comments highlighted the impressive level of detail and presentation of the document in this section and noted the examples shown as being excellent, as is the strategic direction of this document.</p> <p>Other responses raised more detailed points which are summarised around the following themes:</p> <ul style="list-style-type: none"> <li>• Need for consistency;</li> <li>• Terminology changes;</li> <li>• Aspiration levels for the site and consideration of demographics and deprivation;</li> <li>• Demand for certain types of houses;</li> <li>• Housing design detail: including local identity, impact, internal spaces, materials and windows;</li> <li>• Parking areas, treatment of Parking Courts and</li> </ul>	<p>These comments are welcomed and noted.</p> <p>This early upfront guidance sets out the Council's expectation for the design development of the site and provides developers the certainty and consistency of what will be required as part of the planning process and is welcomed by the majority of respondents. However, one of the respondents suggests the terminology especially the use of the word requirement, is too restrictive and prescriptive. The Design Codes are intended to strike a balance between providing guidance and not being overly prescriptive to deliver a high quality place. The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the</p>

<p>accessibility;</p> <ul style="list-style-type: none"> <li>• Vision and build quality over the long term;</li> <li>• Discrepancies between the codes and masterplan;</li> <li>• Commercial and retail units, including signage,</li> <li>• Case studies and examples;</li> <li>• Arts and Heritage;</li> <li>• Roads detail: specifically traffic calming and character and design of streets;</li> <li>• Landscaping and streetscape strategy;</li> <li>• Play strategy: including scale of plots, illustrations, importance placed on specialist buildings, interface between public places and spaces;</li> <li>• Robustness and appropriateness of materials;</li> <li>• Distinctiveness; and</li> <li>• Status of the guidance, commercial reality and marketability; and concerns the codes are too restrictive.</li> </ul>	<p>masterplan vision.</p> <p>The Council has high aspirations for this site in order to encourage economic prosperity and reducing deprivation levels in the local area. Development which has already taken place on the site such as the energy centre and care home are of a very high quality design and the design codes seek to ensure that subsequent proposals maintain this high standard.</p> <p>The housing types proposed are based on market conditions and the site characteristics to ensure that a high quality place is achieved.</p> <p>The Design Codes have been adapted to provide further clarity in relation to waterfront blocks and the relationships between public and private space at the waterfront and the linear park. The waterfront area with the open blocks will be carefully considered in terms of how it addresses parking and the relationship to other housing units on the site. Key principles to guide the form of the waterfront development has been introduced in the revised document. The Design Codes have also been adapted to provide further clarity between the parking courtyard areas and the amenity of these areas. Parking levels are discussed above in the response to Question 3.</p> <p>It is not intended that the Design Codes will be prescriptive in terms of housing design and detail in order to encourage design innovation. The case studies are a good way of presenting various aspects of the Design Codes as well as illustrating quality placemaking by inspiring the designer to design high quality housing whether through layout, design and /or materials.</p>
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	<p>It is agreed by a number of respondents that the buildings on the waterfront should create impact and drama and it is intended that the Design Codes will encourage this. The detail of the Design Codes does not extend to the internal spaces of proposed houses.</p> <p>In relation to the discrepancy point, the masterplan shown on the website is not the masterplan approved under the Planning Permission in Principle. Some of the design components are already described in detail in the masterplan or consents already granted for development and these are described in more detail in the Design Codes.</p> <p>The Design Codes relate principally to the remaining plots for private housing with the retail and commercial units at the foot of the basin and along Titan Boulevard already previously consented. The signage location and design is controlled through the permission for the retail and commercial units and any future applications. The eastern development plot is identified as mixed use and future development will be guided by key design principles. The case studies illustrate aspects of the Design Codes and are there to inspire. They are not intended as examples for designers to copy.</p> <p>The comments relating to arts and heritage are noted. The rich history and heritage of this site is reflected in the design and materials of the new buildings and will be taken forward in both the Arts Strategy and the Street Naming Strategy.</p> <p>In relation to comments on roads detail, traffic calming measures have already been implemented in terms of the spine road and will come forward with each development plot. The character</p>
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	<p>and design of the streets give preference to pedestrians over cars with the development linking into NCN7 cycle route. Further information has been provided in the Design Codes in relation to the character of the streets.</p> <p>It is considered that there is clear landscape and streetscape guidance provided in the Design Codes for the development plots and for areas around the basin. However, the street hierarchy and typology have been revisited in light of comments received through the consultation process. The Design Codes will be integrated with the Connecting Clydebank proposals and this is referenced in the document.</p> <p>With regard to the micro climate in relation to landscape, the designers will be expected to assess the microclimate within their own plots and demonstrate that this is understood and that the suggested design solution seeks to address this.</p> <p>With regard to the play strategy, it is agreed that the 'Getting It Right for Every Child' (GIRFEC) approach should be adopted and this will be developed through the planning permission granted.</p> <p>The local character and identity is addressed by the different street character and typology within the codes. The use of traditional materials such as clay bricks and metal is promoted as well as the use of industrial architecture in the design of buildings. The robustness and appropriateness of materials will be addressed by the use of more natural materials such as the use of clay bricks and aluminium windows. Future roof detail is covered in the Design Codes however the maintenance of design build is outwith the scope of this document.</p>
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	The status of the guidance, marketability, commercial reality and the flexibility of the guidance are all addressed above.
<b>Question 5: How well do you think the document balances the level of prescription with the opportunity for design innovation?</b>	
Summary of Responses	Council Responses
<p>The majority of the responses indicated that they thought the Design Codes were very balanced or had a good balance. Some respondents thought the balance was average, with 2 responses stating it was poor and one response indicating the document had a very poor balance.</p> <p>The majority of the responses raised more detailed points which are summarised into the following themes:</p> <ul style="list-style-type: none"> <li>• Micro-climate and its impact on the riverside;</li> <li>• The document needs to be strong to avoid poor development;</li> <li>• Design Codes are too prescriptive;</li> <li>• Clarity around SUDS is needed; and</li> <li>• Development and design must be based on the economy;</li> </ul>	<p>Noted.</p> <p>Designers will be expected to assess the microclimate within their own plots and demonstrate that this is understood and that the suggested design solution seeks to address this.</p> <p>The guidance is clear from the outset that the intention is to ensure that a high standard of design quality is maintained throughout the entire development to deliver the masterplan vision. As indicated in question 2 above some respondents indicated that the Design Codes required to be more prescriptive, however it is considered that the Design Codes try to get the correct balance between being overly prescriptive but encouraging design innovation as discussed above.</p> <p>In relation to SUDS, this has been addressed in the revised Design Codes and is determined by the Planning In Principle permission.</p>

	It is acknowledged that the economics of the site is very important if a successful development is to be achieved, however this does not preclude achieving a high quality development. The Design Codes have been developed to assist the planning application process so future development proposals can be assessed in a structured way which will result in more consistent and quicker decisions.
<b>Question 6: Which areas of the Design Codes would you say is the most successful?</b>	
<p>The majority of the responses stated the document was very good or strong in all areas. Some of the respondents points to specific sections of the document as being the most successful.</p> <p>However, one comment stated that they did not feel that the Design Codes was an integrated document and many areas appear to be outwith the scope of the codes.</p> <p>One respondent states that the biggest issue with the codes is that they masterplanning and pushing the development into a suburban scale instead of the urban setting within the masterplan.</p>	<p>Noted.</p> <p>The concerns raised by this respondent are addressed in full under question 4.</p> <p>Noted. The Design Codes builds upon the masterplan vision and supports a high quality urban place. Different character areas are proposed such as parkland, mews, etc but these areas are all set within an urban setting. A suburban approach to this site would not be appropriate and certainly is not being proposed. The Design Codes have been adapted to demonstrate the lowest and highest height of development to offer added clarity.</p>
<b>Question 7: In your own words, what areas would improve about the Queens Quay Design Codes document? What would you have added to the document that might assist further with development or design?</b>	
<p>The respondents suggested the following areas that would improve the Design Codes and these are summarised below:</p> <ul style="list-style-type: none"> <li>• Process for Street Naming;</li> </ul>	A Street Naming Strategy has been agreed by Planning

<ul style="list-style-type: none"> <li>• Perspective taken from within the proposed buildings, views and vistas captured;</li> <li>• Opportunities for social interactions;</li> <li>• Grading and flexibility regarding different types of housing;</li> <li>• Approach to Public Art;</li> <li>• More detail on the drainage strategy;</li> <li>• Clearer and more detailed specifications;</li> <li>• Assessment of open space provisions is required;</li> <li>• Fundamental issues with vision for projects and development being proposed;</li> <li>• The approach for the shared river frontage needs to be revisited;</li> <li>• Titan View vista;</li> <li>• Expectations on sustainable homes;</li> </ul>	<p>Committee and street names are being proposed for the various streets which require to be agreed by the Committee.</p> <p>Views and vistas are covered more by the Design and Access Statement, although referenced in the Design Codes, they determine the location of the main access routes through the site.</p> <p>The development form promotes many opportunities for social interaction especially at the head of the basin, Titan Boulevard and the Riverside linear park.</p> <p>The comment on grading and flexibility of housing, this is outwith the scope of the Design Codes.</p> <p>The provision of public art will be covered by a separate annex to the Design Codes - an Arts Strategy. The comments made in this regard are noted and will be filtered into the Arts Strategy.</p> <p>The comment in relation to the drainage strategy are noted and the provision of SUDS is addressed in the modified Design Codes and the conditions of the approved permissions.</p> <p>In relation to the comment on the need for more detailed specifications, it must be remembered that there needs to be a balance between being overly prescriptive and flexibility and it is considered that the modified Design Codes has achieved the correct balance.</p> <p>The overall Queens Quay site contains a considerable amount of public open space and public realm. Developers of parcels within the site are expected to consider the greenspace</p>
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	<p>requirements associated with their application for Approval of Matters as Specified by Conditions.</p> <p>In relation to the respondent that states there are fundamental issues with the document, the points raised in the representation have been addressed within the various responses above.</p> <p>This has been discussed above, the Design Codes have been adapted to provide further clarity in relation to waterfront blocks. Key principles to guide the form of the waterfront development has been introduced in the revised document. The Design Codes encourage a consistency of approach, as there are a range of different solutions which would meet the requirements of the Design Codes and the Planning in Principle Permission.</p> <p>The Design Codes provides details of views and vistas which link Queens Quay to the wider Clydebank context. Titan View is shown as an open vista in the Design Codes.</p> <p>With regard to the comment on sustainable housing, the new housing on Queens Quay will be linked to the District Heating System and a sustainability annex is currently being produced to address the connection of housing to the District Heating System. It is hoped that this site will be one of the most sustainable housing sites in Scotland.</p>
<b>Question 8: Do you have anything further to add?</b>	
The majority of the responses provided positive comments about the Design Codes. Where a respondent did not complete the survey but provided an email response their comments are	Noted.

<p>addressed below.</p> <p>The detailed comments received on this section have been summarised into the following issues:</p> <ul style="list-style-type: none"> <li>• Relationship between individual plots;</li> <li>• Impact of Covid-19;</li> <li>• Range of Tree Species;</li> <li>• Vision for the site is not strong enough;</li> <li>• The design of the District Heating Building;</li> <li>• Approval of the principles in the Masterplan should not be disregarded for those in the Design Codes;</li> <li>• Provision of a community growing space/garden/allotment;</li> <li>• Screen Planting;</li> <li>• Implications of dog walking on the SPA;</li> <li>• Opportunities to enhance public access;</li> <li>• Establish principles to embed sustainable travel behaviour;</li> <li>• Exclusion of the Titan Crane and existing developments from some of the diagrams;</li> <li>• Graphical issues;</li> <li>• Setting of the Titan Crane;</li> <li>• Housing and levels of accessibility;</li> <li>• Housing design quality;</li> <li>• Housing mix;</li> <li>• Leftover space to the east side of the Leisure Centre;</li> <li>• Health, heritage, and cultural life;</li> <li>• More on the town's notable ancient and Roman history could be included along with links to the wider cultural landscape;</li> </ul>	<p>The Design Codes have been modified to address this and the relationship between plots will also be addressed through the planning process.</p> <p>The current pandemic reinforces the importance of achieving a quality environment with sufficient quality open spaces and quality housing.</p> <p>The Design Codes have been modified to address tree species and further clarity on landscaping.</p> <p>The Council disagrees with the respondent and considers that the vision is very strong for the site. This is set out in the Design and Access Framework, and the masterplan which the Design Codes build upon. It is envisaged that Queens Quay will be a place to live, work and visit. People will be attracted to the River Clyde location, the facilities the site offers and the high quality environment.</p> <p>The comments on the District Heating building are noted; however, the Council does not share the view of the respondent and considers that the District Heating building is in the right location as it is quickly becoming a landmark building due to its design and complimenting the iconic Titan Crane. It will become an attraction in the future with visitors not only interested in its workings as well as being a visitor attraction for those visiting Clydebanks.</p>
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<ul style="list-style-type: none"> <li>• Design codes should reference a to Ian McHarg;</li> <li>• Public art and reference to shipbuilding;</li> <li>• Green space, ecology and leisure;</li> </ul>	<p>The principles of the masterplan are not being undermined, the Design Codes build upon the approved masterplan and are to be used to deliver the masterplan vision.</p> <p>The provision of community growing spaces and allotment is encouraged by the Design Codes and it will be explored as each development plot comes forward. There may also be scope of this use along the riverfront linear park.</p> <p>Referring to the comments made on screen planting, this is covered by the Planning Permissions in place and through future applications for the detailed housing plots.</p> <p>In relation to the concerns about the impact of dog walking on the Special Protection Area (SPA), the suggestion by the respondent for signage which outlines the international importance of the Inner Clyde foreshore for wintering migratory birds and which requests that dog walkers keep their dogs on the lead when walking them on or near the foreshore could be incorporated within the signage strategy for the site and is supported.</p> <p>A new riverfront path is being provided and the development will connect into the national cycle route of NCN7, thereby increasing public access to the River Clyde which has not been available for the public for more than 100 years from this site. Significant areas of public and greenspace are to be provided for both residents and visitors. The Council's Access Officer and the Sustainable Officer have been involved throughout the preparation of this guidance.</p>
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	<p>The comments made in relation to establish principles for sustainable transport behaviours are welcomed and noted and given the location of the site, residents and visitors will be encouraged to use sustainable modes of transport.</p> <p>The comments made in relation to the Titan Crane and other developed buildings are noted; however, to include the developed part of Queens Quay could be confusing as the Design Codes only relate to the central area of Queens Quay which is covered by the Planning Permission in Principle and is presently largely undeveloped.</p> <p>The graphical issues raised relating to the NCN 7 cycle path route and transport diagrams are noted and have been addressed in the modified Design Codes.</p> <p>In relation to the setting of the Titan Crane, the iconic Titan Crane sits 46 metres high so a 8 storey building would be significantly lower than the Titan Crane and would be further away from the crane than it would be high. A dense building form needs to frame the substantial basin and Titan Crane. Any new building at the basin will compliment but be subservient to the Crane.</p> <p>The comments on enlarging the housing function are noted. In relation to the levels of accessibility, although the Council is supportive of increasing the level of accessible housing on the site, this would be determined by further iterations of the Local Development Plan and the application process. It is not an issue for the Design Codes to take forward.</p> <p>The comments on housing design quality are noted. The</p>
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	<p>affordable housing already approved on site is of exceptional design and quality and it has inspired the production of the Design Codes to ensure that the private housing is of equal quality. In relation to the West Dunbartonshire Design Standard, it is agreed that this should also be referenced as an influencer of quality.</p> <p>Increasing housing mix on the site is supported, however this would be difficult to achieve through the Design Codes and is forming a level of prescription that is not in tandem with the objective of the Design Codes. The types and mix of housing is for individual housing developers to bring forward themselves and is beyond what the Local Development Plan and Design Codes are required to do. Some housebuilders may also come forward proposing a shared equity / shared ownership housing model.</p> <p>With regard to the leftover space, this area is identified as a commercial leisure opportunity within the Local Development Plan, it is however outwith the scope of the Design Codes.</p> <p>The Design Codes have been assessed from a health perspective and within the introduction of the modified Design Codes there is a clear ambition to improve the health and wellbeing of Clydebank. The comments regarding heritage and culture will be addressed through the street naming strategy and arts strategy.</p> <p>The comments made regarding ancient and Roman history and links to the wider cultural landscape are more suited to the Arts Strategy and will be addressed through it.</p>
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	<p>The comments regarding green space, ecology and leisure are noted and can be addressed more fully through the arts strategy.</p> <p>It should be noted that the work of Ian McHarg, who is a Clydebank Man, is referenced in the new Clydebank health and care centre and the street naming strategy.</p> <p>The comments on public art, shipbuilding and local narratives has informed the street naming strategy and will inform the future arts strategy which will be annexes to the Design Codes.</p>
<b>Detailed response from CRL/ Dawn Group within their separate document entitled: 'Queens Quay Design Codes Review Document'</b>	
The landowner, CRL/ Dawn Group submitted a series of detailed comments in addition to the general comments they made above. These comments have been separated out and summarised below under the relevant sections in which they relate to.	
<u>General</u>  The comments received in this section have been summarised as follows: <ul style="list-style-type: none"> <li>• Objectives for the site;</li> <li>• Masterplan and the Codes must not be an inhibitor of development;</li> <li>• Provides six key observations: Maintain and protect key historic connection routes and Vistas; Importance of scale</li> </ul>	<p>The objectives of the site owner are supported and agreed by the Council.</p> <p>The Design Codes set out structured guidance for designers and developers and they provide a structured approach to assess future planning applications. The intention is to ensure a high</p>

<p>fronting large civic spaces and streets; Development form and location should clearly define an urban block structure; the importance of a continuous defining built edge – avoid gaps; the planning and placement of parking has significant impact on the definition of an urban block structure and placemaking. Parking should be placed on street and/or in back courts; and storey heights.</p>	<p>standard of design quality is maintained in order to deliver the masterplan vision. Some of the design components are already described in detail in the masterplan or the consents already granted. Flexibility has been inbuilt in the Design Codes.</p> <p>The six observations are welcomed, noted and agreed as key urban design principles in creating a high quality development in this waterfront location. Key routes and vistas have been identified in the Design Codes which align with the masterplan. The importance of scale around the basin, at the riverfront and at key areas along the main spine road is recognised within the Design Codes.</p> <p>The Design Codes are based on urban principles with clear “fronts and backs “ with corners of urban blocks defined and articulated with the continuity of building edge to a street and related to adjacent developments. The Codes promote parking courts or parking part of the streetscape which supports the respondents comments. The expectation is that storey heights, urban form and scale should increase on approach to the landmark features such as the Titan Crane and the River. This has been addressed in the modified Design Codes.</p>
<p><u>Street Hierarchy and typology</u></p> <p>The comments to this section are summarised as follows:</p> <ul style="list-style-type: none"> <li>• A hierarchy of streets in terms of scale, character, use and traffic/ pedestrian functionality is essential and a</li> </ul>	<p>Noted and agreed. The Design Codes will include details of a hierarchy of streets such as Main Street, more secondary</p>

<p>definition of streets is needed to clarify and define the urban structure; and</p> <ul style="list-style-type: none"> <li>• There should be differing typologies to different situations;</li> </ul>	<p>residential streets and the mews streets. This will assist in promoting character areas and placemaking qualities. Different street typologies have been clarified in the modified Design Codes.</p>
<p><u>General Massing and clarity of urban form and block layout</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Rear of the riverside frontage needs to better define the urban blocks, development typologies and scales relative to the hierarchy of streets and density of developments proposed.</li> <li>• There needs to be a clearer urbanity and strength/continuity of edge to avoid ‘sub-urbia’ creeping into the development;</li> <li>• Scale and heights of buildings;</li> <li>• Mews philosophy, clarity and continuity of the urban edge and the Mews Street should not dominate the main residential street; and</li> <li>• Density of development; and</li> <li>• Character of north- south linking streets, street widths and urban form</li> </ul>	<p>The development typology has been developed further in the modified Design Codes with more detail on green infrastructure such as the treatment of green spaces and community gardens.</p> <p>The scale and heights of the buildings have been further clarified in the modified document together with clearly defined corners for the urban blocks.</p> <p>The mews development is a particular feature of the Design Codes. It does not prevent a strong dense urban edge with a mews development behind it. This type of development is commonly found in cities like Copenhagen, Glasgow–Park Circus and case studies has been added to the document to illustrate this.</p> <p>The Design Codes allow for a fairly density development as it moves towards the pocket park and the crane and this has been further clarified in the modified document.</p> <p>Street widths and location are determined by the original</p>

	<p>masterplan and are respected by the Design Codes. Street hierarchy is also set by the masterplan and is further defined within the Codes. The character of the Urban streets, the Mews streets and the Parkland streets is set out in the codes with precedent images, diagrams and sketches that support the hierarchical nature of each character zone. The Design Codes offers additional clarity around expectation where the masterplan does not offer detail. It is agreed that there should be variation in the extent of landscaping of the residential streets.</p>
<p><u>Mews Street</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Support for pedestrian focussed space;</li> <li>• Vehicular access and passing needs to be considered;</li> <li>• Nodes;</li> <li>• Mews blocks should be lower in scale; and</li> <li>• The route should be flanked by more flatted development as wings to primary functions.</li> </ul>	<p>The mews street is intended as a pedestrian and cycle space with key social 'nodes' providing focussed areas of landscaping, public realm and street play. Although the geometry of the mews streets should permit occasional use for emergency vehicles, they are not intended as shared surface routes for vehicles. Car access can be achieved via the rear car parking courtyards. As discussed above the mews development which is lower in scale can be accommodated within a dense urban perimeter.</p>
<p><u>Massing and nature of river frontage development</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Beneficial view's of the river are supported but physically not achievable;</li> <li>• The suggested pavilion end block is inappropriate and totally underwhelming;</li> </ul>	<p>Noted, the key principles are agreed.</p> <p>The development form at the riverfront has been amended to remove the reference to pavilions and the key principles inserted into the document to guide the river frontage development. It is accepted by the landowner and the Council</p>

<ul style="list-style-type: none"> <li>• Flooding interventions will views to the river from the Mews development will be difficult to achieve;</li> <li>• To maximise river views then the density of the river frontage must be of a higher scale and density;</li> <li>• a denser development will define the linear park better and provide increased overlooking and passive security;</li> <li>• Key principles along the river edge is: Scale – push as high as possible; Variety – length of the riverfront; articulation – both in scale and façade depth and layering; Edge – strength of edge along the riverfront, defining and overlooking; Corners – strength and dominance of corners; Breaks – in blocks to achieve light and view penetration; Landscape – define public and private spaces; and Memorability – contribute to a new memorable place of distinction and quality; and</li> <li>• Suggests the adoption of higher density and breaks in development utilising L, U or I shaped blocks at each corner with a break in the centre to bring in light and provide views out towards the river.</li> </ul>	<p>that a straight plain flatted block elevation would provide a relentless and imposing façade over the long length of this very prominent and valuable riverfront frontage. By providing an appropriate break in the blocks this will provide light penetration and views of the river whilst meeting the objectives of both the Council and the landowner.</p>
<p><u>Around the Basin:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Development around the basin needs to respond to and demonstrate the highest quality of design, scale, civic frontage and use to inform an activate this central public gathering space and define a destination at the heart of the site;</li> <li>• The western side of basin needs to respond to the scale and drama of the Titan Crane and should have the highest density throughout the site;</li> </ul>	<p>It is good that the landowner and the Council are aligned in the objective of achieving the highest quality of design, scale, use and civic frontage and maximising the basin location. It is agreed that the western side of the basin should have the highest residential density to create drama/ impact and frame the sizeable basin area.</p> <p>The area to the east of the basin will be shown in the codes as more generally ‘mixed use’ and the more specific description removed, thus offering a more flexible approach and allowing for</p>

<ul style="list-style-type: none"> <li>At the eastern side it would be appropriate to consider office or commercial uses. A commercial development of scale along the east side of the basin should not be precluded from consideration on the design codes.</li> </ul>	<p>a more mixed use development of scale. The pavilions shown on this eastern side within the Design Codes Review document is not acceptable. General principles are proposed in the modified Design Codes of how this part of the site is to be taken forward in the future such as concealing parking and being compatible with the surrounding urban form of development</p>
<p><u>Balconies:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>Orientation and aspect of the riverside frontage provides significant architectural opportunity to maximise the benefit and value of river aspect and south west orientation;</li> <li>Suggestion that this elevation should explore a typology of the vertical garden, incorporating deep balconies to each unit; and</li> <li>Adopting such an approach would assist a richness to this façade and introduce activity across both the length and height of the development.</li> </ul>	<p>Agreed and the Design Codes will include reference to the provision of balconies especially along key frontages</p>
<p><u>Landscaping:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>Private spaces and its boundary and integration/interface with streets and public spaces needs to be carefully</li> </ul>	<p>Agreed and the Design Codes will set the principles to avoid this</p>

considered to avoid a 'back fence' situation.	situation occurring
<p><u>Public car charging:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Further consideration is needed around local capacity and should be civic approach; not something that developers can deal with separately.</li> </ul>	Noted but it is not for the Design Codes to address.
<p><u>Car Clubs:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• Inappropriate for the Design Codes to address the requirement or for design codes to effectively give private companies a commercial advantage by prescribing that certain areas are given over to these enterprises.</li> </ul>	Noted. Car Club spaces are only mentioned to offset reduced parking numbers. This would be addressed more fully as applications come forward for the respective development plots.
<p><u>Renewable Energy:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• It would be useful for the codes to address design issues to do with photovoltaic cells, air source heat pumps etc. as these have the potential to become unsightly with major impact on development quality.</li> </ul>	Noted, this will be addressed by the Sustainability annex.

<p><u>Sub-stations and utility infrastructure:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• There will be a need for approximately another 8 substations. Consideration should be given to how developers are to incorporate these necessary elements of infrastructure into their design proposals.</li> </ul>	<p>Noted, the treatment of substations is addressed in the modified Design Codes.</p>
<p><u>Materiality and robustness:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• The only issue highlighted is that UPVC as a material, reacts well within a marine environment. Colour is obviously a major consideration and there needs to be a position where colours other than white could be considered.</li> </ul>	<p>Noted. The use of aluminium windows is a preference but this will be determined through the application process. Aluminium windows are being used in the affordable housing and health and care centre.</p>
<p><u>Geographic extent:</u></p> <p>The comments received in this section have been summarised as follows:</p> <ul style="list-style-type: none"> <li>• The codes should go beyond Queens Quay to the adjacent landholdings</li> </ul>	<p>Noted and this may occur in the future. However, the site boundary for the Design Codes is the application site for the Planning in Principle Permission.</p>





## QUEENS QUAY DESIGN CODES

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16 September 2020

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# INTRODUCTION

West Dunbartonshire Council's Local Development Plan and supporting documentation highlight Queen's Quay as a location where high quality design and attention to place making is an essential component of new development, in order that Queen's Quay becomes a destination neighbourhood. These Design Codes set out structured guidance for designers and developers. They provide a structured approach for West Dunbartonshire Council to assess forthcoming Planning Applications in order to ensure that a high standard of design quality is maintained throughout the entire development.

# CONTEXT

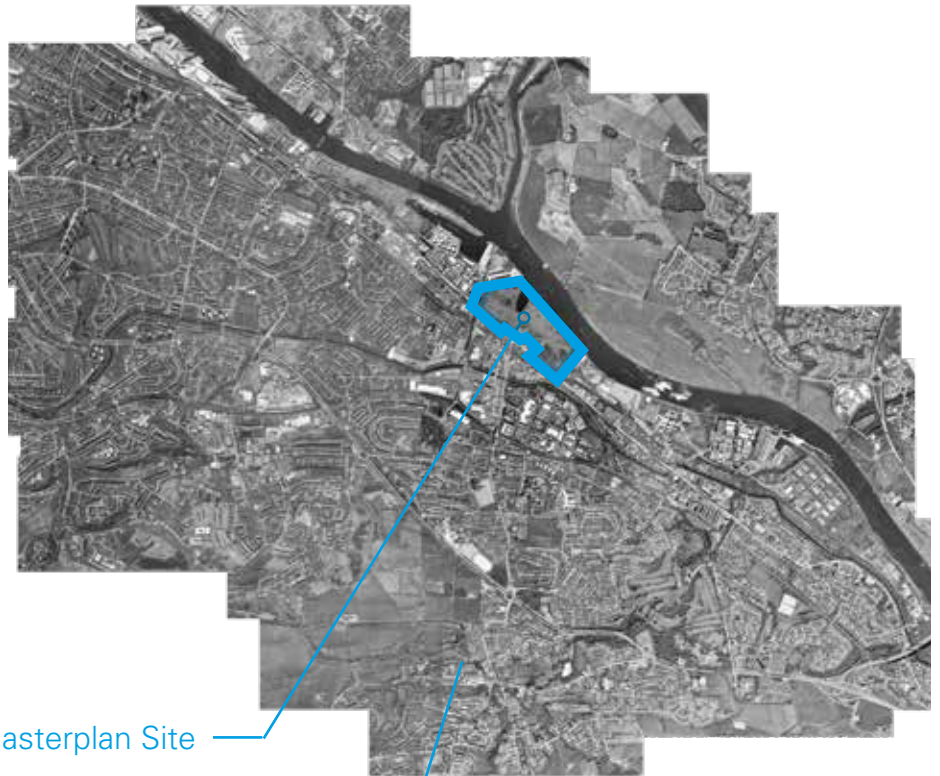
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## CLYDEBANK CONTEXT

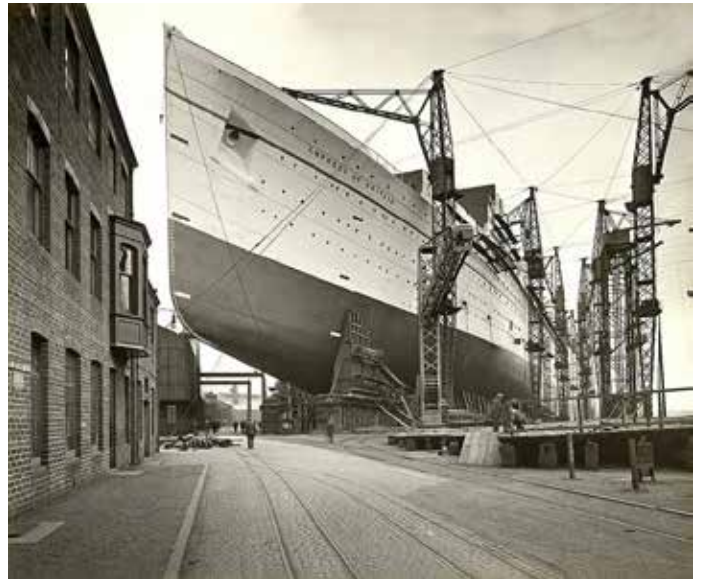
The site is in a prominent location on the northern bank of the River Clyde and is strategically important for Clydebank's ongoing regeneration.

The approved masterplan for Queen's Quay will see the redevelopment of a significant area of Clydebank's townscape on the site of what was originally John Brown shipyards. This is an opportunity to change the gravitational pull of the town's heart towards the Clyde.



Queens Quay Masterplan Site





At its peak employing over 10,000 people, John Brown Shipyards was one of the most commercially successful shipyards in the world, becoming famous internationally for its shipbuilding quality.

Following the yard's closure in 2001, only a few features remain. Dominating the site is the Titan Crane, now an icon for the town's future regeneration.

There is an opportunity to change the gravitational pull of the town's heart towards the Clyde where it sat at the peak of the shipbuilding industry

## Existing Townscape

The once intact townscape of Clydebank has been badly eroded through damage during the Blitz and inappropriate demolition and redevelopment during the period 1960–2000, leaving few key landmarks remaining.

The result is a lack of defined 'street edge', large areas of 'waste-land', no 'celebration of corners' and a lack of height eroding the urban townscape, all resulting in a lack of a sense of place.

Although there are some housing areas adjacent to the site, these are relatively isolated and the new development offers an opportunity to knit these in to a new urban fabric.





## Existing Public Buildings

The development site is well - located for pedestrian access to a wide range of public facilities at Clydebank Town Centre.

- **Within a 5 minute (400metre) walking distance:** Leisure centre, Town Hall, Clydebank College, plus the proposed Clydebank transport Hub which includes improvements to the existing train station
- **Within a 10 minute walking distance:** Golden Jubilee National Hospital, Shopping Centre. Local Primary Schools, Rothesay Dock industrial area

## Public Transport and Cycling

Clydebank Train station and Chalmers Street Bus Station sit within five minutes walking distance of the site. There are bus stops along Glasgow/ Dumbarton Road.

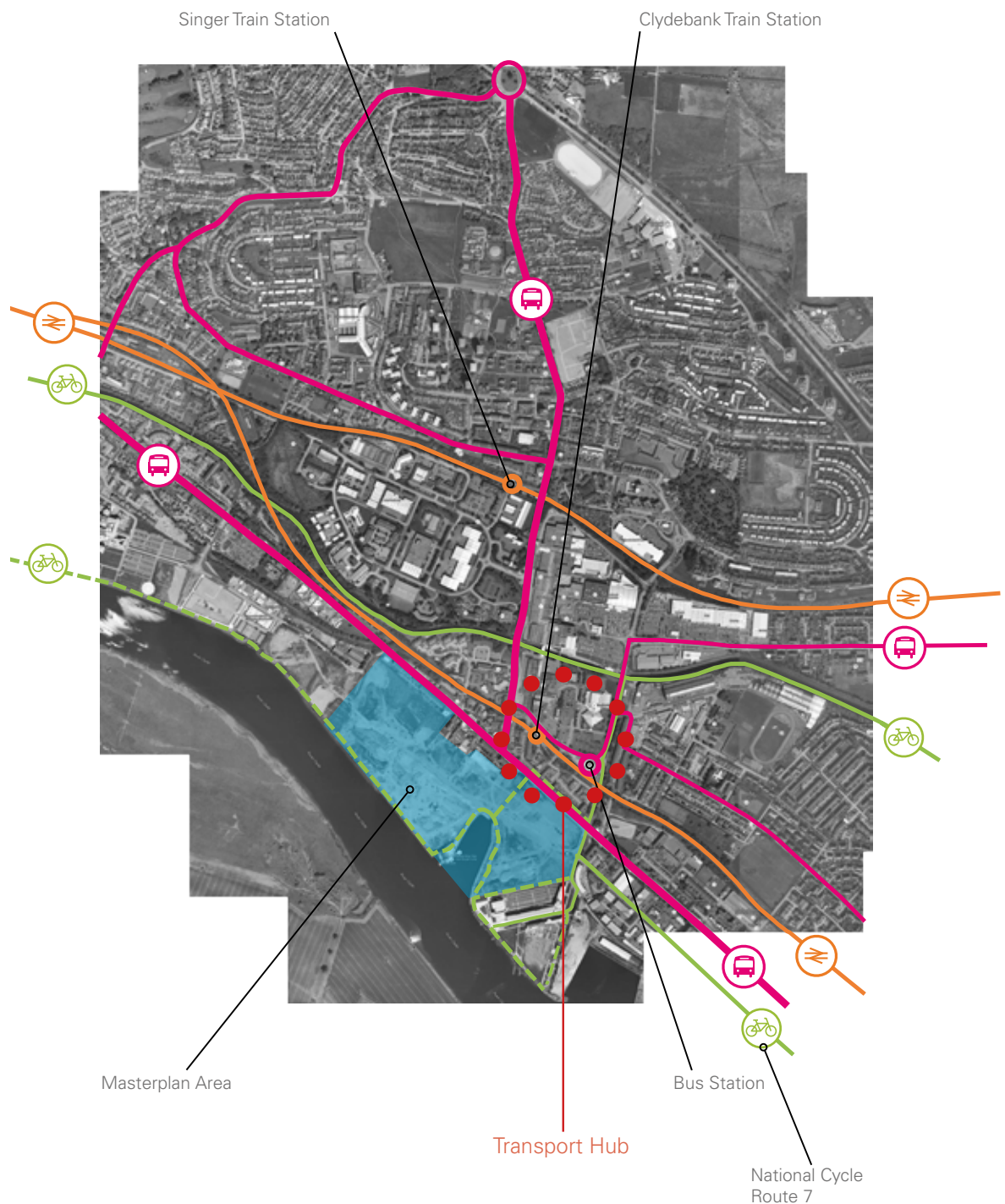
## Existing Green Network

Greenspaces within Clydebank are isolated and the town centre lacks open greenspace of a significant useable scale. The riverside walkway west of the site is not accessible to the public, but the aim in the long term is to link it to the Queens Quay area to form a continuous riverside walkway.

## Public Transport Hub

Connecting Clydebank is a transformational project which seeks to revitalise the civic core of Clydebank; forming essential links with the ongoing Queen's Quay development, the present town centre and the many active and public transport routes into the area. It aims to link different strands of investment to reinforce Clydebank as a destination.

The project originated from a successful Charrette process in 2015 (refer to the Clydebank Charrette Action Plan) and has been progressing in consultation with funding partner Sustrans following the successful funding award from the Sustrans Community Links Programme in 2016.





# OBJECTIVES AND ASPIRATIONS

Clydebank has a rich and colourful past as an important industrial heartland. This history is a big part of what makes it a special place. In recent years Queens Quay has been an industrial wasteland, however it is now being transformed into an exciting new neighbourhood.

## A high quality destination

The purpose of this document is to set out an Aspirational Vision for Queens Quay based upon:

- Using remaining elements of the shipyard to help define the place
- Using materials that complement the industrial character but can also integrate new development.
- Minimal change to the robust character of the existing dockyard quay structures
- Massing, scale and detailing which complement the historic, large scale structures on the site and its previous industrial character
- Providing active travel routes which connect Queens Quay to the town centre
- Engaging with existing streets and creating a new urban street pattern
- A new health centre as a busy focus

## A Sustainable Neighbourhood

Queen's Quay is intended to be one of the most sustainable new residential areas in Scotland.

It will be energy efficient - new housing at Queens Quay should be connected to a District heating network System and the Design Codes incorporate a Sustainability Annex which suggests suitable material choices and specifications for new homes.

- Improved green connections with boulevards and linear greenspace, pocket parks and edible landscapes designed to support health, heritage and cultural life, with developers encouraged to apply for Building With Nature Accreditation.
- A Sustainability Annex within the Design Codes provides more detail about the on site District heating network System and what it will achieve with low carbon heat.



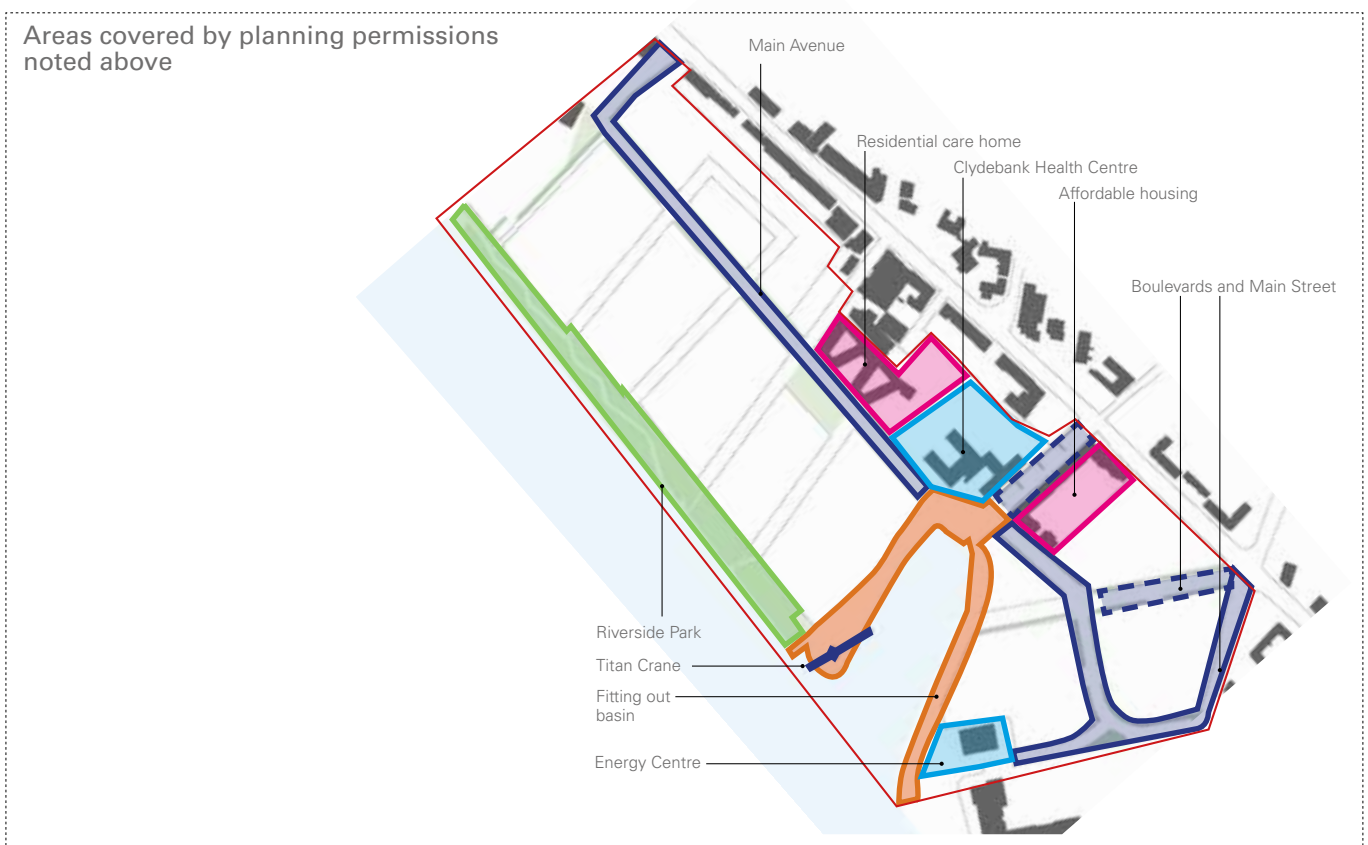
Recent exemplar developments visited by West Dunbartonshire Council planning and regeneration officers, in order to identify precedent projects that demonstrate high design quality

# THE MASTERPLAN

Prior to the development of the Design Codes, a masterplan was prepared for Queen's Quay by Clydeside Regeneration and Dawn Group, in partnership with West Dunbartonshire Council. This masterplan already has some planning consents associated with it which cover detailed proposals for infrastructure, together with a new care home, health centre, affordable housing and some areas of public realm and greenspace. They include:

- **DC 15/234 Planning Permission in Principle:** Mixed use development including residential (including affordable/community/housing), retail, financial/service, restaurant, public house, office, hotel, care home, health centre, assembly and leisure uses, with associated car parking, access road, quay wall improvements and landscaping (Planning Permission in Principle)
- **DC16/240 Spine Road:** formation of new spine road (including junction alterations at Cart Street, Alisa Road and Cable Depot Road) formation of public realm around the basin and river edge
- **DC16/244 Care Home:** a two/three storey home for life and a day care centre
- **DC17/231 Energy Centre:** the largest ambitious district heating network system in Scotland powered by heat pumps which will extract water from the River Clyde
- **DC18/033 Connecting Clydebank:** new controlled and uncontrolled crossings, improved footpaths, better cycle access, public realm and road works to Dumbarton Road, Glasgow Road and Hall Street in order to improve the public realm connecting the main shopping area to the north of the site and the new Queens Quay to the south.
- **DC18/057 Clydebank Health Centre:** this will act as a catalyst for significant change, bringing a wide range of health services together - a focal point and landmark feature.
- **DC18/272 Mixed Use Development on Titan Boulevard** (part of Plot 5 which is excluded from the scope of the design codes).
- **DC18/275 Titan Boulevard:** new street, public space, landscaping and associated street furniture.

Future planning applications should familiarise themselves with the content of the above applications relating to the external amenity, greenspace, riverside location and infrastructure.



The Design Codes build upon the approved masterplan and intend to deliver the masterplan vision. They are guided by the masterplan's objectives which are:

- **Creating a quality public realm:** An enhanced urban (rather than suburban) environment with safe pedestrian connections to the town centre.
- **Improving connections:** Linking to other development sites along the water's edge and creating better pedestrian and cycle routes to the Titan Crane and Riverside Park.
- **Creating a special place:** Developing the site as a key destination with opportunities to enhance cultural offerings in the area while creating a sustainable community.
- **Regeneration:** Changing the gravitational pull of the town's heart back down towards the Clyde where it sat at the peak of the shipbuilding industry.

There are distinct areas within the masterplan as follows:



**The Fitting-Out Basin:** The existing concrete deck structure has been made safe for day to day public use as well as events; it will be the location for a seating areas and information display.

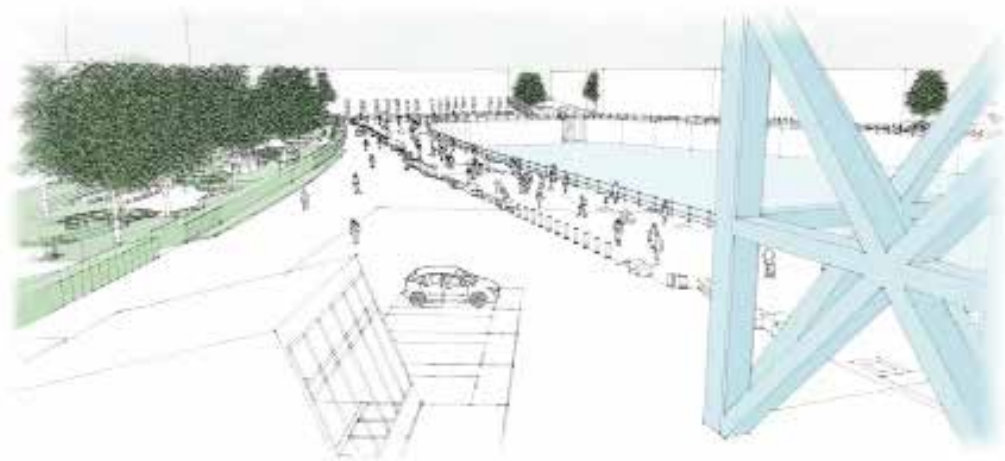


illustration from Queen's Quay Infrastructure Masterplan Detail Design Statement  
For Approval of Matters Specified in Conditions DC15/234; Ian White Associates



**Riverside Park:** West of the basin, a greener approach is proposed and new homes will look out over the river towards countryside. The river edge provides a wildlife habitat; behind this the Clyde Path (a continuous four metre wide shared path/cycleway) sits within a pattern of subdivisions derived from the layout of the berths and buildings that previously occupied the site. This park subtly accommodates slopes to raise the development plots up above flood level. All residential streets will have path connections to this area which will be lit to enable safe use at night, as part of the cycle network.

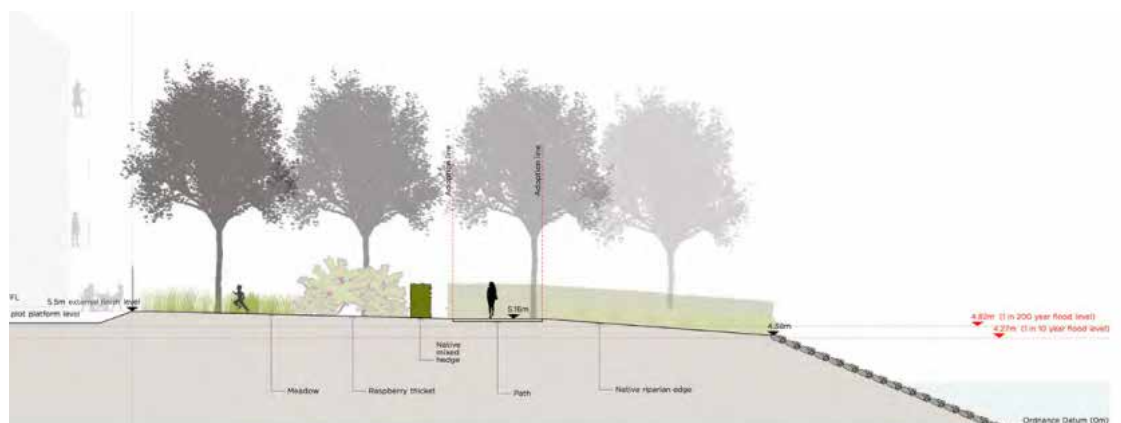


illustration from Queen's Quay Infrastructure Masterplan Detail Design Statement  
For Approval of Matters Specified in Conditions DC15/234; Ian White Associates





**Main Avenue:** The proposed Main Avenue takes the line of the previous plate gantry, which ran between the platers shed and the fitting-out basin. A strongly defined tree-lined street is the main traffic route, with generous verge zones behind the footways providing space for an avenue of street trees.

To calm traffic speed, the six metre carriageway of the Main Avenue is broken into sections corresponding to future junctions with residential side-streets. In-carriageway bus stops with raised kerbs are provided at intervals on either side of the street.

There are two greenspaces within the main street corridor. A Pocket Park contains play, lawn and garden seating spaces, and at Cable Depot Road a Community Orchard has a selection of fruit trees significant to the region, planted on a five metre grid amongst meadow with seating and mown grass paths.

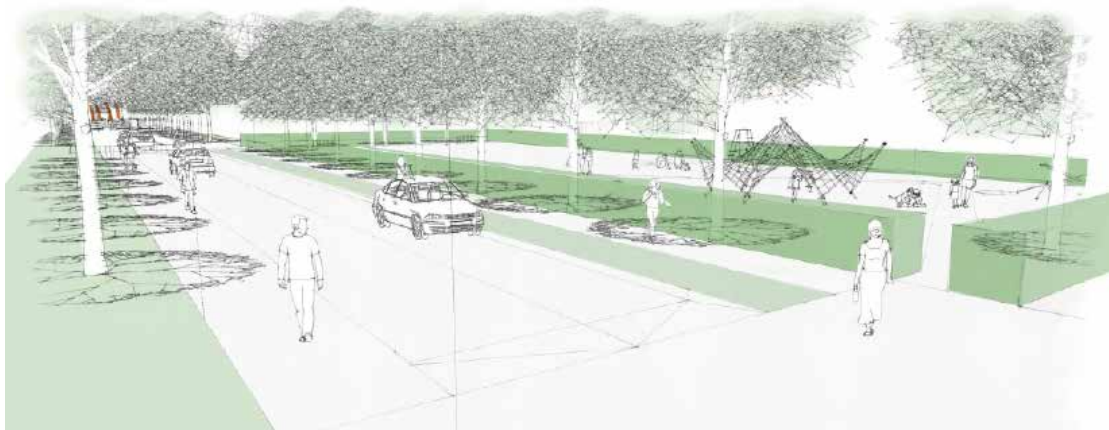


illustration from Queen's Quay Infrastructure Masterplan Detail Design Statement  
For Approval of Matters Specified in Conditions DC15/234; Ian White Associates



**Boulevards:** Spatial connection between the Basin and Dumbarton Road is provided by two broad avenues which intersect at the basin. They are aligned axially with the Titan Crane and provide strong visual and spatial connections to and from Dumbarton Road, bridging between the town and its reclaimed, revitalised waterfront.

These linear spaces are intended to be busy and dynamic, with strong built frontages and flexible shared space to enable commercial life, where appropriate. Trees line the axial view to emphasize a green connection to the river. Materials will co-ordinate with the current Council proposals for public realm on Dumbarton Road.

Neither will connect onto Dumbarton Road for vehicles, but instead will provide pedestrian and cycle connections with occasional use by emergency vehicles.



illustration from Queen's Quay Infrastructure Masterplan Detail Design Statement  
For Approval of Matters Specified in Conditions DC15/234; Ian White Associates

# LANDSCAPE POLICY & MASTERPLAN APPROACH

The Council's Local Development Plan 2 is the first development plan in the UK to be awarded Building With Nature's Excellence Award, which is the highest award in their accreditation scheme.

Building with Nature is the UK's first benchmark for green infrastructure. It defines the characteristics of high quality green infrastructure, and awards those who show dedication to securing a range of benefits for people and wildlife by showing a commitment to high quality design, implementation and management and tenancy of green infrastructure features.

This means that the policies within Local Development Plan 2 ensure that Green Infrastructure is considered from the outset of the development process, throughout its construction, and sustainably managed after the development has been completed. Policy CP2 of the Plan requires all developments to take a green infrastructure approach first and the Council wish to see this approach undertaken within Queens Quay, they recommend that developers may also wish to seek Building with Nature accreditation.

The masterplan for Queens Quay contains significant open spaces forming an integrated green network which forms part of the West Dunbartonshire Green Network.

[The landscape strategy provides:](#)

- Connected biodiverse habitats
- Active travel routes (pedestrian and cycle)

Opportunities for play and recreation which align with West Dunbartonshire Council's Play Strategy. (The Getting It Right for Every Child' (GIRFEC) approach should be adopted, where appropriate, and this will be developed through the planning consent process)

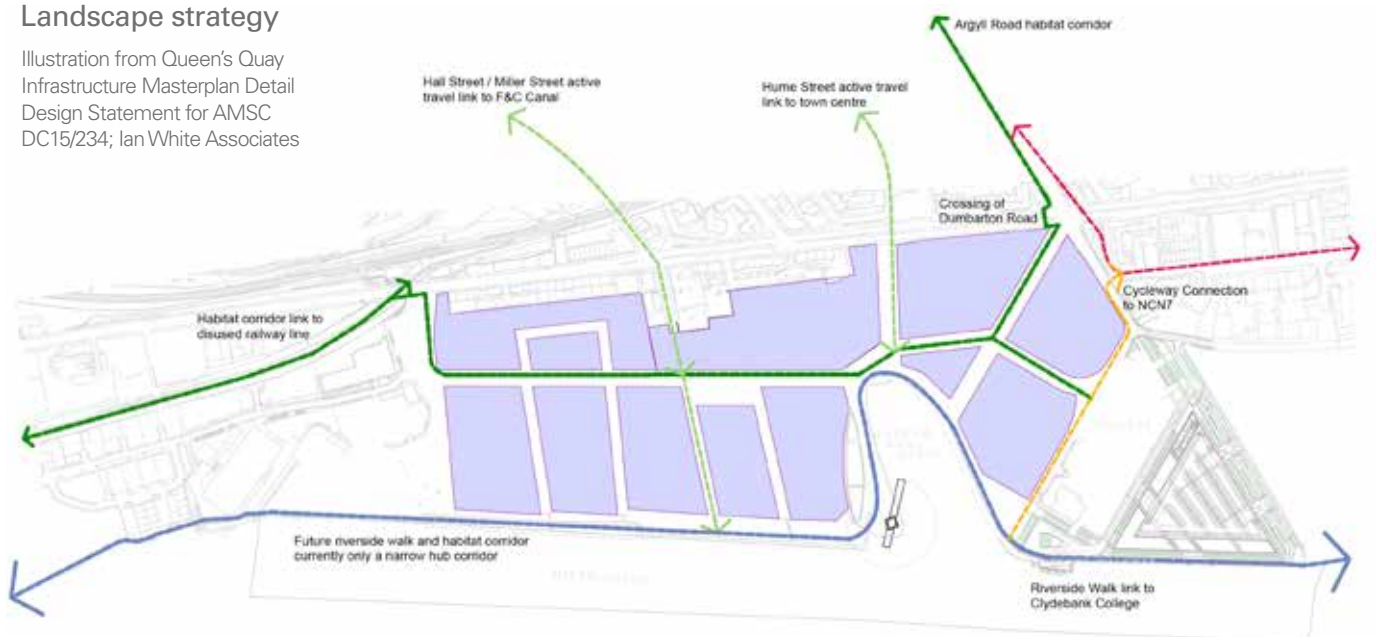
- Opportunities for community growing spaces; an Edible Landscape Strategy has been prepared to guide this long-term process of engagement, planting and management.

[The landscape components within the masterplan include:](#)

- A linear riverside habitat park enhancing biodiversity and containing a continuous Clyde Walkway route from east boundary to west
- A Main Avenue giving access to all plots
- Spatial connection between the Basin and Dumbarton Road via two broad avenues
- Retention of the fitting-out basin refurbished for safe public access
- Pocket parks containing community greenspace and play
- Residential streets that provide a safe and green environment

## Landscape strategy

Illustration from Queen's Quay  
Infrastructure Masterplan Detail  
Design Statement for AMSC  
DC15/234; Ian White Associates



# SUSTAINABLE URBAN DRAINAGE

West Dunbartonshire Council require that new developments satisfy the principles of, and comply with, the Sustainable Urban Drainage Systems Design Manual for Scotland and Northern Ireland, as consented in the approved masterplan.

It is proposed to discharge surface water direct to the Clyde following appropriate SUDs treatment. SEPA has classified the adjacent area of the Clyde as having an overall status of 'moderate ecological potential' so guidance determines that the development should apply minimal SUDs. This should include the provision of source control SUDs mechanisms but should not include permanent open water features such as basins.

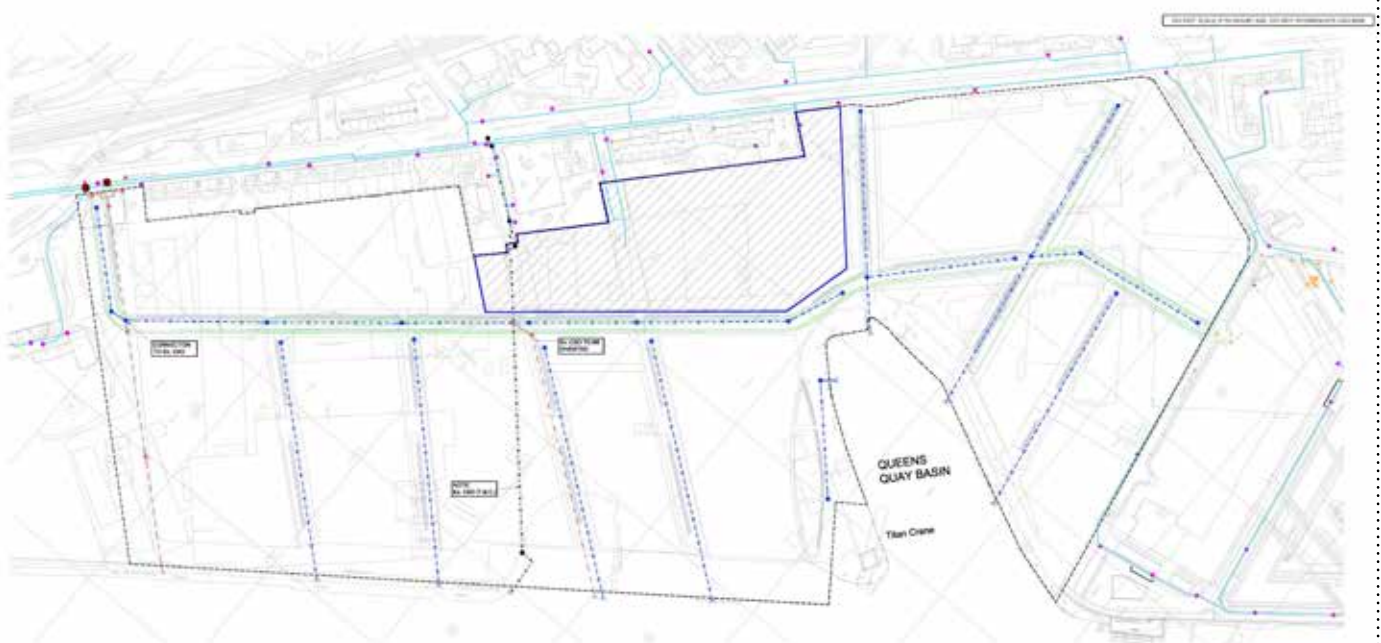
Given the historical brownfield nature of the development, it is not proposed to discharge SUDs through a soakaway type infiltration. A single level of SUDs treatment should be embraced within each development plot and associated road, which is then conveyed by surface water sewers to the River Clyde via new outfalls.

The SUDS strategy is approved in the Planning Permission in Principle and each developer should come forward with details that comply with the overall SUDS Strategy. The final design of SUDs is outwith the scope of this document and is dependent on feedback from consultation from SEPA and Scottish Water.

The following measures are likely to be used:

- permeable surfacing (subject to approval from West Dunbartonshire Council)
- filter drains or trenches

Illustration: a proposed surface water drainage layout has been prepared as part of a Drainage Impact Assessment for the Queens Quay masterplan



# THE DESIGN CODES

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# SCOPE

The Queens Quay Design Codes build upon West Dunbartonshire Council's Aspirational Vision, setting out the parameters for development of remaining plots which have not been included in planning applications to date. The intention is to ensure that a high standard of design quality is maintained throughout the entire development in order to deliver the masterplan vision.

These Design Codes set out structured guidance for designers and developers, and provide a framework for West Dunbartonshire Council to assess Planning Applications. They are organised as follows:

Guiding Principles are identified, (see Page 18 of the Design Codes), which should guide all new development.

- Although design codes guide the development in three dimensions, two dimensional Regulating Plans are included with them, to enable code users to locate where the provisions of the code will apply, and to express how the codes relate to plots and different character areas.
- Detailed guidance for specific character areas
- Materials which are suitable for a maritime environment
- Case studies - precedent projects selected to illustrate specific design aspirations.

It is anticipated that development at Queens Quay will be phased, with development ongoing for five to ten years. As a consequence, the overall development site will be parcelled into smaller development plots (*see following page for details*). In some locations, each plot plus an adjacent residential road will be parcelled together.

The areas covered by the Design Codes are noted in the table below and illustrated in the diagram 'Parcel Subdivisions' overleaf.

table 1; development plots covered by Design Codes

Site	Residential road	Greenspace	Use
1	no	no	mixed
2	no	no	mixed
3	no	no	mixed
4&5	no	no	mixed
7	yes	no	residential
8	no	yes	residential
9	yes	yes	residential
10	yes	yes	residential
11	yes	yes	residential
12	yes	yes	mixed



# PARCEL SUBDIVISIONS



plot boundary - housing



plot boundary - mixed use

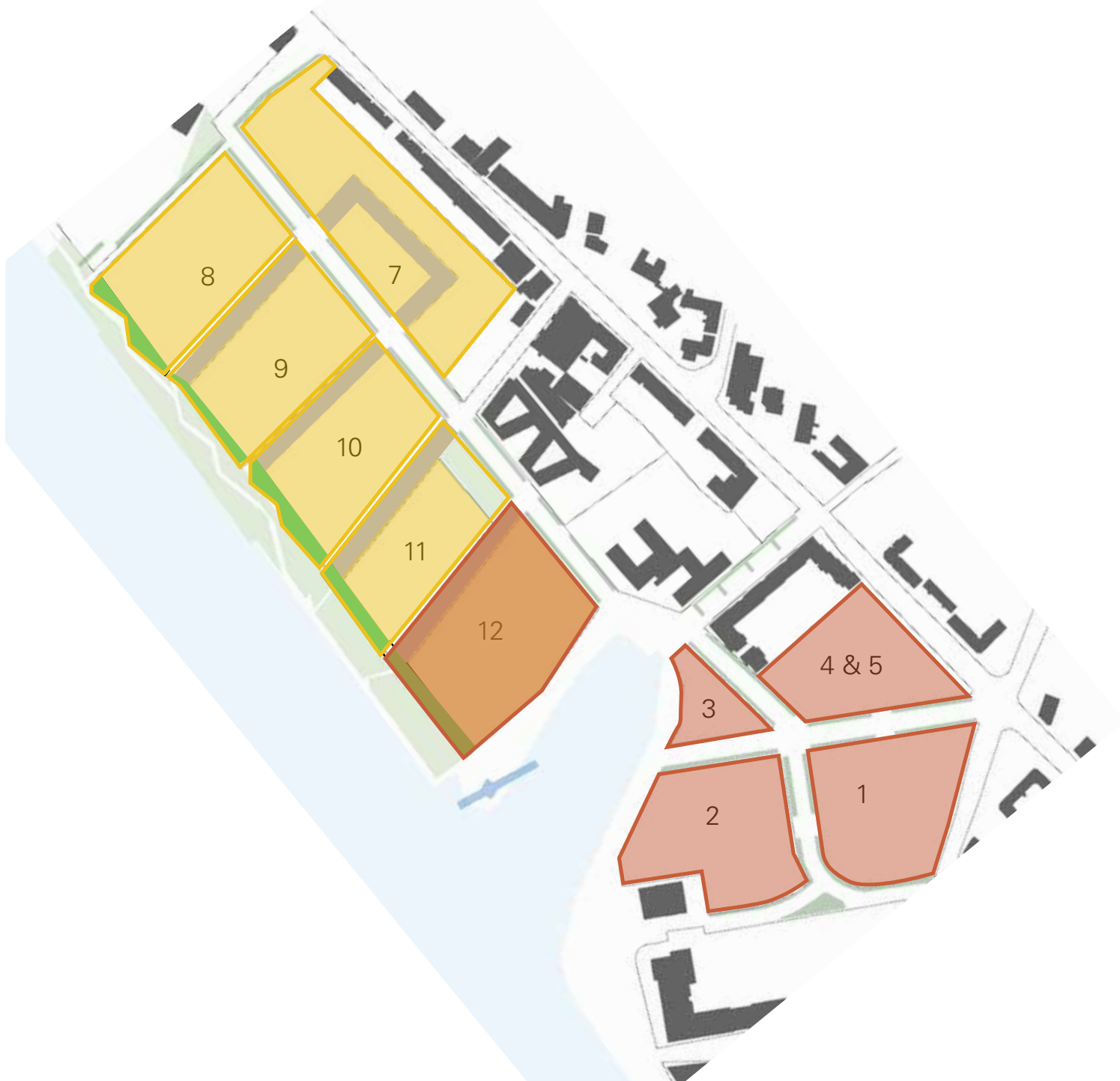


residential access associated with each plot



Riverside Walkway greenspace associated with specific development plot, in order to allow construction access and to ensure an integrated approach to boundaries and planting

(Ref IWA drawing no 1924/52)



# DESIGN PRINCIPLES

A series of guiding principles which underpin the design codes are noted below. These are based on seven key observations derived from the experience of recently built projects across the whole Clydebank Rebuilt Masterplan Area.

## 1 Maintain and protect key historic connection routes and vistas

Development opportunities should be matched to the most appropriate development plots, to ensure that all key vistas associated with the original slipways and streets are protected and maintained.

## 2 The importance of scale fronting large civic spaces and streets

The scale of facades fronting larger civic spaces and streets responds proportionately to those spaces.

## 3 Development form and location should clearly define an urban block structure rather than suburban principles.

Buildings should either be built to the back of the pavement, or have a well defined and enclosed front garden intended as a privacy strip, rather than in-curtilage parking. They should have clear 'fronts and backs'. Entrances should be directly off public streets and development should define and articulate corners of urban blocks. Any large inward investors (non-retail) will be addressed as they arise.

## 4 The importance of enclosing streets and spaces.

Each new development should be planned to complement and integrate with its neighbours. The continuity of built edges at streets, particularly when planned and executed between adjacent developments, should be carefully considered when defining new urban block structures.

## 5 The placement of parking

The planning and placement of parking has a significant impact on the definition of an urban block structure and placemaking. Parking should be placed either on the street and/or within back courts. Courtyard car parks brought through to the street edge will erode the possibility of defining a clear built urban edge, thereby creating a 'business park' typology rather than urban townscape.

## 6 Storey Heights

The site has three key features; the river, the Titan Crane, and the new energy centre. The design of the development and its urban form and scale should recognise and respond to these key features and landmarks. Development should therefore significantly increase in density and height the closer it comes to both the crane, river and energy centre.

## 7 New development should include a green infrastructure plan

**Developers are encouraged to apply for Building With Nature Accreditation** which ensures that new developments deliver for the natural world and healthy communities. It provides a framework of quality standards, an assessment and accreditation service, and national awards recognising the design and delivery of high quality green infrastructure.

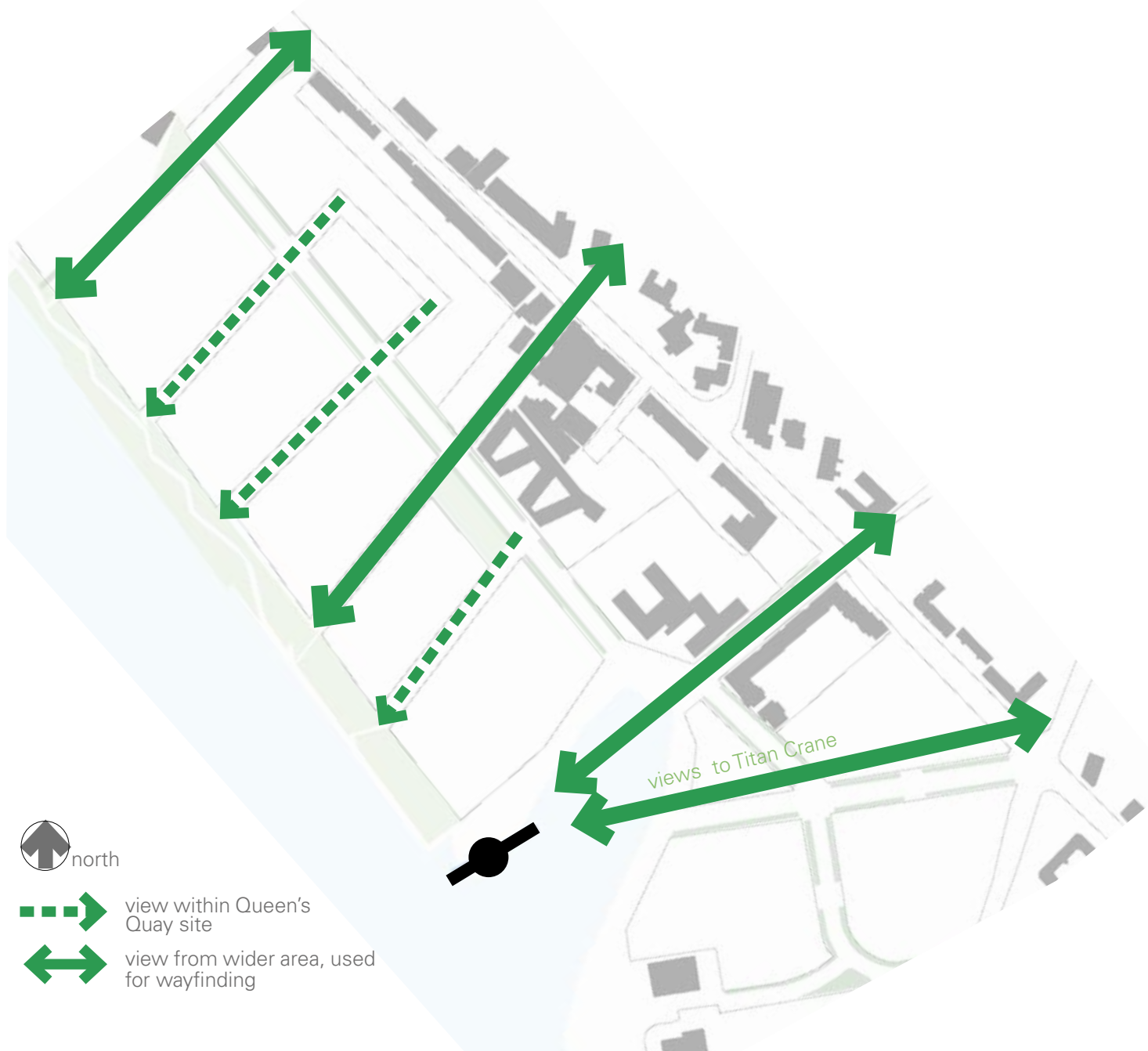
*For example, Leeds Climate Innovation District turns a central brownfield site into a resilient, green, mixed-use neighbourhood. Its green infrastructure plan provides greenspace which is an important contributor to sustainability and wellbeing. A biodiversity line runs along the riverbank, and green roofs provide a habitat for birds and pollinating insects. Retaining existing trees and planting more local species helps local wildlife. The green infrastructure network will be enriched with food plants to make it a productive landscape in which the local community can invest and draw benefits in the form of healthy diet, exercise, community participation and seasonal events.*

# VIEWS AND VISTAS

A fundamental principle behind the masterplan is to create visual connection to the Clyde and the surrounding area, so new streets are positioned to both reinforce and create these connections. Although the new Queens Quay development creates a 'gravitational pull' down to the river, there are other significant connections back into the town, and to the hills beyond that which 'stitch' the new masterplan into the wider physical and cultural landscape.

In some locations, the proposed Boulevards provide vistas which link Queens Quay to the wider Clydebank context. These help to stitch the old in with the new. Some partial vistas begin within the Queens Quay site and provide views to the Clyde via new residential streets which are located where the original slipways were positioned. It is essential that these visual and physical connections to the Clyde are retained. As a minimum, they will need to provide pedestrian and cycle access to the Riverside Walkway - vehicular access need not continue the full length of each vista.

Retaining views and vistas needs to be balanced against the need for shelter in what is an exposed, semi-rural location. Because of this, designers will be expected to assess the microclimate within their own plots and demonstrate to West Dunbartonshire Council Planning Department that this is understood and that their proposed design solution seeks to address this.



# STREET NETWORK & ROAD HIERARCHY

In addition to requiring compliance with local and national design guidance, the Design Codes define a hierarchy for different types of streets - based on their significance in terms of both placemaking and movement for pedestrians, cycles and vehicles. Applying the hierarchy will help to create a series of attractive, sociable urban spaces as well as controlling traffic and promoting the attractiveness of walking, cycling and the use of public transport.

The principles of the street hierarchy are evolved from the approved masterplan although their exact alignment and design will be fixed through further planning consents on a plot by plot basis with designs as illustrated in the design codes.

**New residential streets:** the location of residential streets, and their junctions with Main Street have been fixed by the existing masterplan. They link the Main Street to development plots, form a route to off-street parking areas and are the location for on-street parking.

**Mews and boulevards:** These routes are predominantly for pedestrians and cyclists, and will be only occasionally used for emergency traffic, delivery and maintenance. Boulevards have a location fixed by the existing masterplan; they provide a physical link to the wider Clydebank area for residents and visitors. Routes running through the centre of the riverside plots are envisaged as fully pedestrianised "Mews" primarily utilised by the residents of Queens Quay.

**Pedestrian & cycle links:** these routes travel through greenspace and are only used by pedestrians and cyclists, their location is fixed by the masterplan.

Table 2; street design and geometries (design code areas only)

	Residential streets	Mews and boulevards	Pedestrian & cycle links
<b>Design speed</b>			
Target speed for traffic	20 mph	10 mph– emergency vehicles only	Not applicable
<b>Street dimensions</b>			
Minimum carriageway width	4.8 metres	3.8 metres	2.5 metres
On street parking	Yes either or both sides, 2.6m width	Not applicable	Not applicable
Minimum footway width	2 metres to either side	Not applicable	Not applicable
Verge	Yes – same width as on street parking zone	Not applicable	Not applicable
Direct plot access for traffic	No	Not applicable	Not applicable
Vehicular access to parking courts	Yes	Not applicable	Not applicable
<b>Public transport</b>			
Pedestrian access to transport hub	Within 400 metres walking distance	Within 400 metres walking distance	Not applicable
Bus access	No (Main Street only)	No (Main Street only)	Not applicable
<b>Street design details</b>			
Traffic calming	Yes (linear route)	No - retractable bollards at entrance	No - retractable bollards at entrance
Vehicle swept paths	Yes – all vehicles	Yes – emergency vehicles only	Yes – emergency vehicles only
Junction sightlines	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction spacing	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only
Junction radius	TBA by WDC Roads Dept	Not applicable	Emergency vehicles only

# STREET HIERARCHY

## Street hierarchy outwith Design Code areas



Dumbarton Road



Main Street - a masterplanned large-scale vehicular access route currently under construction



Pedestrian and cycle route with occasional access for emergency vehicles



Pedestrian and cycle route only

## Street hierarchy within Design Code areas



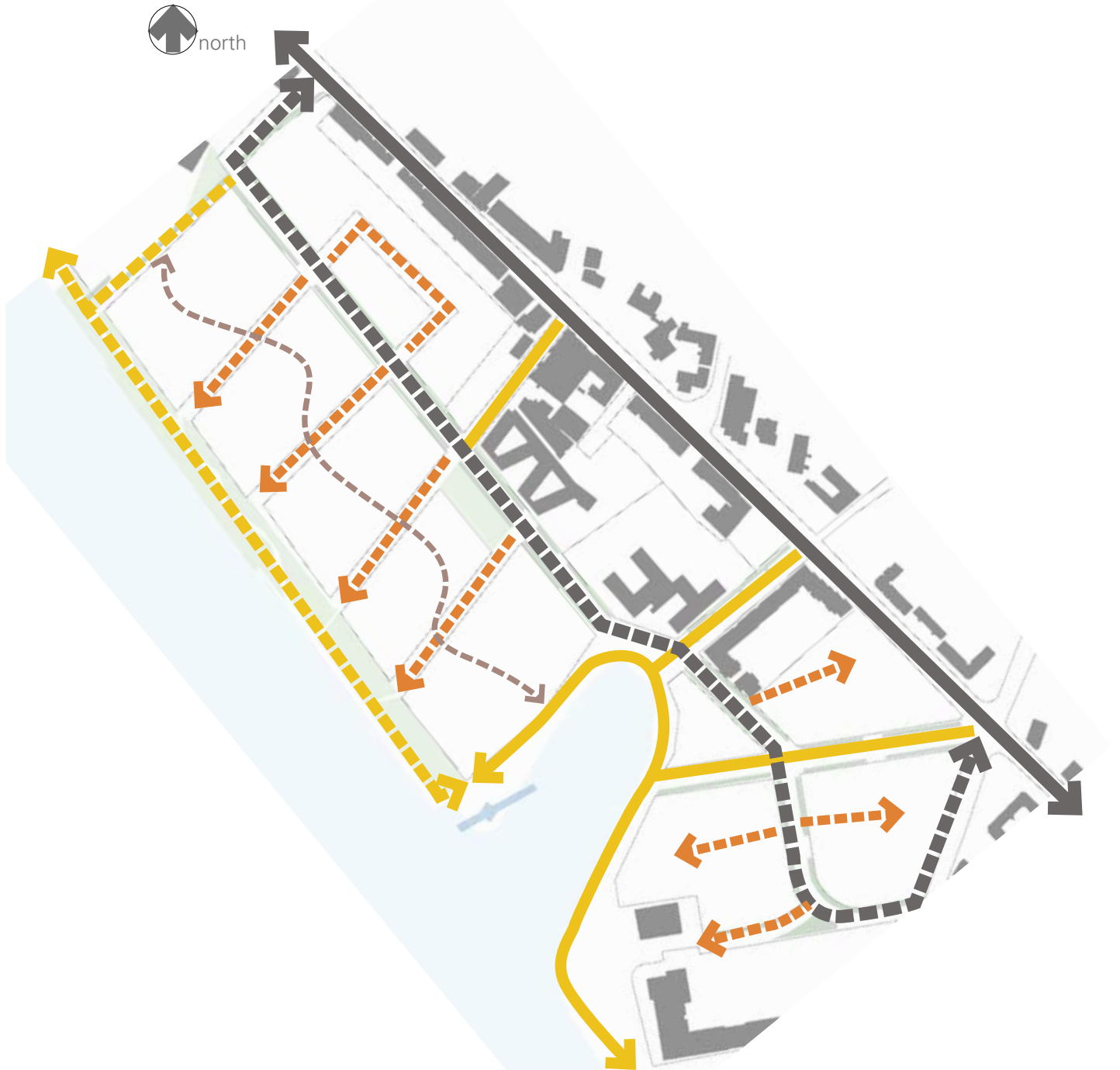
Residential access road - vehicles, pedestrians and cyclists



"Mews" - residential pedestrian and cycle links with occasional access for emergency vehicles



north





# DENSITY & MASSING

There is a natural division within Queens Quay. The plots shaded in yellow are within easy walking distance to the train station and public transport hubs. They are also located in an area where a higher density is more appropriate because of the larger scale of development, and greater building heights beside the proposed mix of uses and the Titan Crane.

The plots shaded in grey are further from the transport hubs and train station, and are not located beside buildings and spaces with a larger scale urban character. They are more suitable for a lower density development, with lower building heights while still retaining an urban character.

The development block adjacent to the pocket park bridges areas of higher density (to the east) and lower density (to the west). It should have a simple clarity of urban edge and character, bringing people to the park spaces to the north and south of the block.



# BUILDING TYPOLOGIES

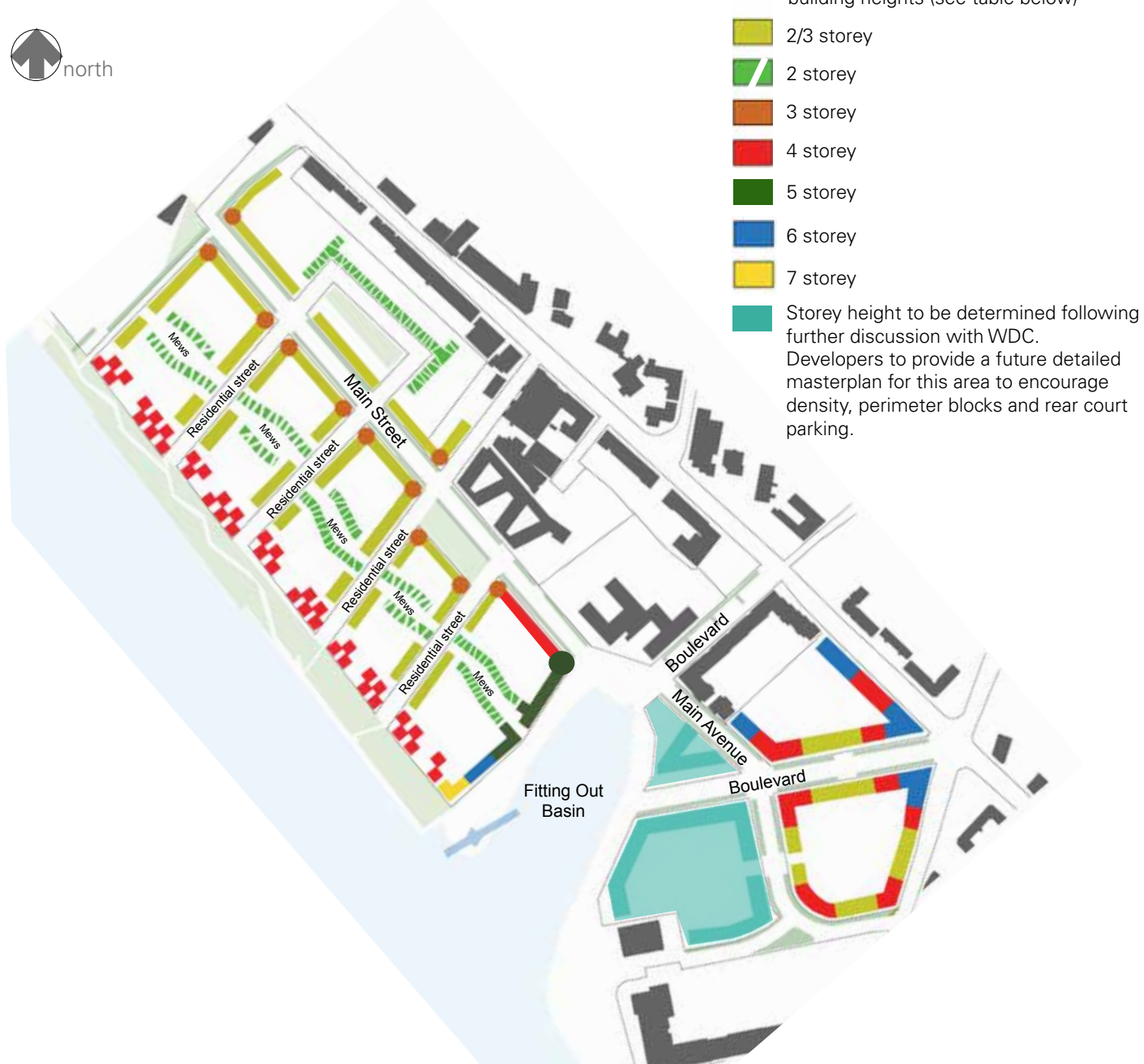
The location of different building typologies supports the key design principles described on page 17:

- They maintain and protect key historic connection routes and vistas
- They provide opportunities for scale fronting larger civic spaces and streets, with storey heights which respond to their location
- Development form and location clearly defines an urban block structure
- Development encloses streets and spaces
- Parking can be located appropriately
- Mixed use development is focused around the Fitting Out Basin and the new health centre and care home.

A consistent approach to materiality is required across all development plots.



# BUILDING HEIGHTS










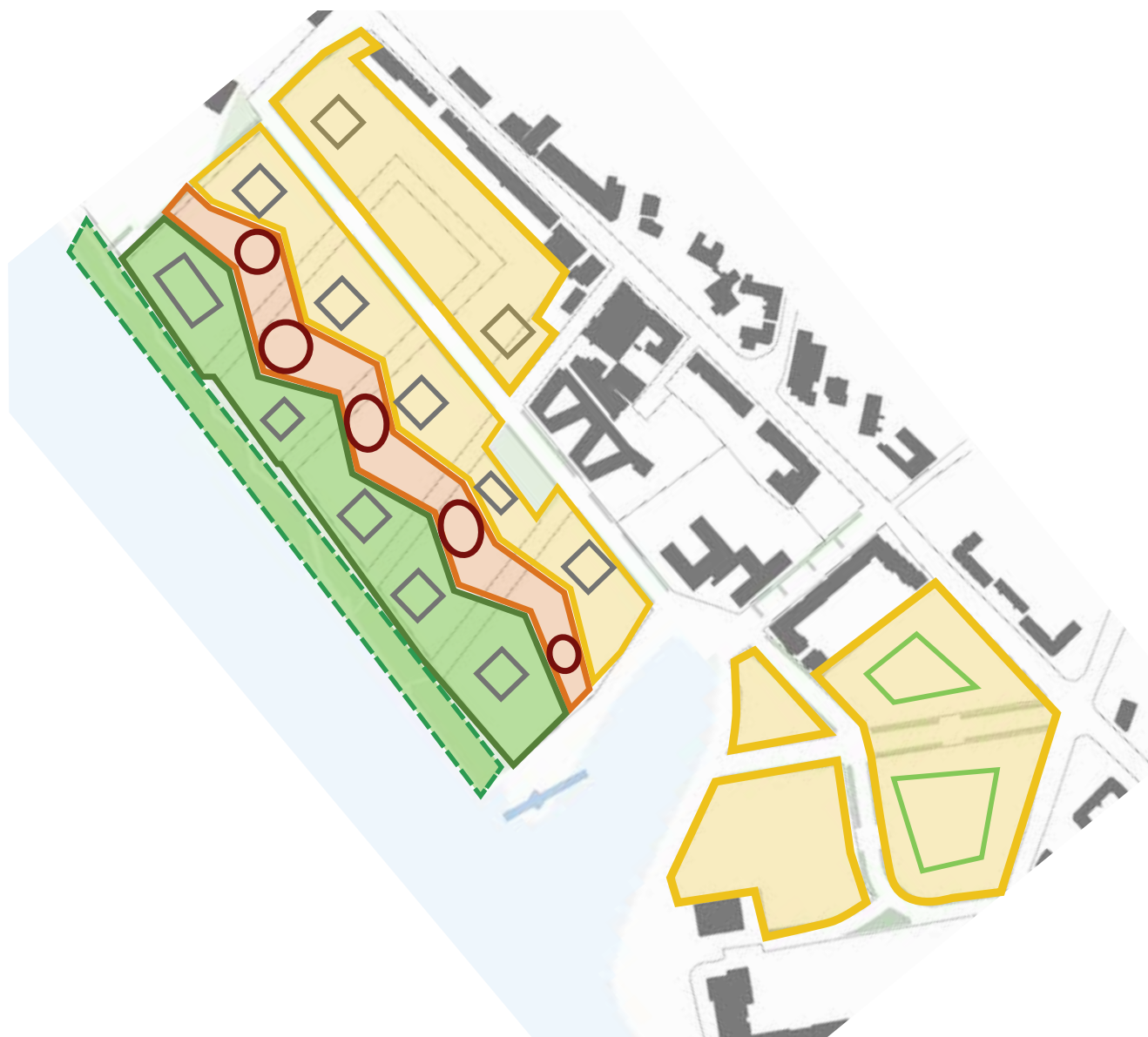
Location	Building height	Frontage continuity	Privacy distances from frontage to footpath	Commercial/ retail use at ground floor
Development facing on to Fitting Out Basin	4 to 8 storeys	Yes	Not required where there is a retail/ commercial use at ground floor, otherwise minimum 1.5 - 2 metres	Preferred where shown as mixed use
Boulevards with some commercial uses (Mixed use typologies )	4 or 6 storeys	Yes	Not required where there is a retail/ commercial use at ground floor, otherwise minimum 1.5 - 2 metres	Preferred
Main Street	2 to 3 storeys	Yes	Minimum 1.5 - 2 metres	No
River frontage	3 to 8 storeys	No – discontinuous blocks to allow views to river	Minimum 1.5 - 2 metres	No
Residential streets	2 to 3 storeys	Preferred	Minimum 1.5 - 2 metres	No
Mews houses facing onto residential pedestrian and cycle links	2 storeys	Preferred	Minimum 1.5 - 2 metres	No



# LANDSCAPE: CHARACTER ZONES

The site has been divided into eight landscape character areas/typologies. The landscape requirements for each character area are set out in the design codes; these include key information on relationships and layout, precedents, furniture, materials and planting.

-  **Streets - Urban:** a formal civic character relating to the scale of the wider street and town houses on the main route through Queens Quay
-  **Streets - Parkland:** residential areas that reflect the soft green landscape of the riverside park
-  **Nodes:** areas of enhanced public realm and landscaping
-  **Mews:** intimate, smaller scale housing along a pedestrian route running east to west through residential blocks
-  **Parking Courts:** informal but functional back of house spaces: communal parking, servicing and bin storage areas
-  **Backcourts:** less dense parking courts incorporating communal parking, servicing and bin storage areas set within amenity and green space
-  **Park:** new development links to, and borders, the expansive riverside park



## LANDSCAPE: HIERARCHY OF NODES

Along the route running through the centre of the riverside plots, it is proposed to create neighbourhood nodes. At these spaces the street should open up to create an area of public space that is sheltered from the prevailing winds and weather.

Each node should be activated by building frontage and soft landscaping. The pedestrian should be dominant in these spaces.

These nodes increase in significance, not in size, as they move towards the basin. Any proposed developments should take cognisance of this and demonstrate to the Planning Authority how each node relates to the other.



# PARKING

Queens Quay is very close to the improved Clydebank Transport Hub, so there is an opportunity to create a new urban area which prioritises movement on foot and by cycle, rather than by car. The Design Code therefore stipulates maximum parking numbers for individual development plots.

There is a natural division within Queens Quay where parking density changes. Plots which are closer to the train station and public transport hub also contain elements of primary building frontage to Dumbarton Road and the Basin. This is an area where proximity to the new transport hub, plus a higher dwelling density will support a more urban character. This can potentially be undermined by larger areas of parking so a maximum targeted parking ratio of 70% is to be provided in these areas.

It is likely that car ownership will be higher for plots which are further from the transport hubs and train station. In these locations the density is lower. Because of this, they can achieve higher parking levels. A maximum targeted parking ratio of 100% is to be provided in these areas. (Parking allowances should include garage spaces).

Developers should provide a parking strategy, including a parking matrix which demonstrates how parking is allocated within development areas (for example, smaller flatted dwellings may have no allocated spaces while larger new homes could have two) and how they are to meet the respective parking targets.

The Scottish Building Standards require 5% of parking spaces to be 'accessible'. These spaces require a wider parking bay and must be close to the building entrance.

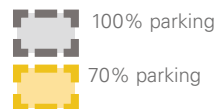
Car club spaces could be provided to offset lower numbers of parking spaces. Electric charging points should be provided.

Visitor parking for cars, small vans and motorcycles should generally use shared, public on-street parking. Cycle storage should comply with Cycling by Design by Transport Scotland.

## Parking character



## Parking ratios



Plot No	Parking ratio PER RESIDENTIAL UNIT	Cycle storage PER RESIDENTIAL UNIT
1	70%	2 spaces
2	70%	2 spaces
3	70%	2 spaces
4&5	70%	2 spaces
7	100%	1 space
8	100%	1 space
9	100%	1 space
10	100%	1 space
11	70%	2 spaces
12	70%	2 spaces



# WASTE AND RECYCLING

The preference is for a circular route for cleansing vehicles through rear courts; this will entail a coordinated approach between developers, which should be included as a condition for planning consents. This approach will allow bins to be wheeled to the rear of properties.

There should be no more than 7 metres of a 'pull distance' for the bins from the location of the bin stores to the cleansing vehicle. Each household will require three 240 litre bins (grey/brown/blue) emptied on a fortnightly cycle.

At flats, this figure is translated into 1100 litre bins on a ratio of 3:1 waste:recycling bins. For example, 13 flats would have four 1100 litre bins (3 waste, 1 recycling). These should be stored in a communal collection area.

Where properties are higher rise (for example 7 storeys) an external bin store should be provided rather than an internal space. Where blocks are mixed use, this store should provide separate space for commercial uses.

Waste and recycling to be further considered by West Dunbartonshire Council on a plot by plot basis.



north

Cleansing department vehicle routes through parking courtyards



# AGGREGATED DIAGRAMS

This drawing collates and displays all of the previous diagrams. The attributes of each diagram come together to work as a whole, creating a mesh of urban design parameters for Queens Quay.

It is essential that these parameters are met by each development plot to ensure conceptual continuity through the Queens Quay area. This will then establish Queens Quay as a valued and desirable location to live, work and socialise.

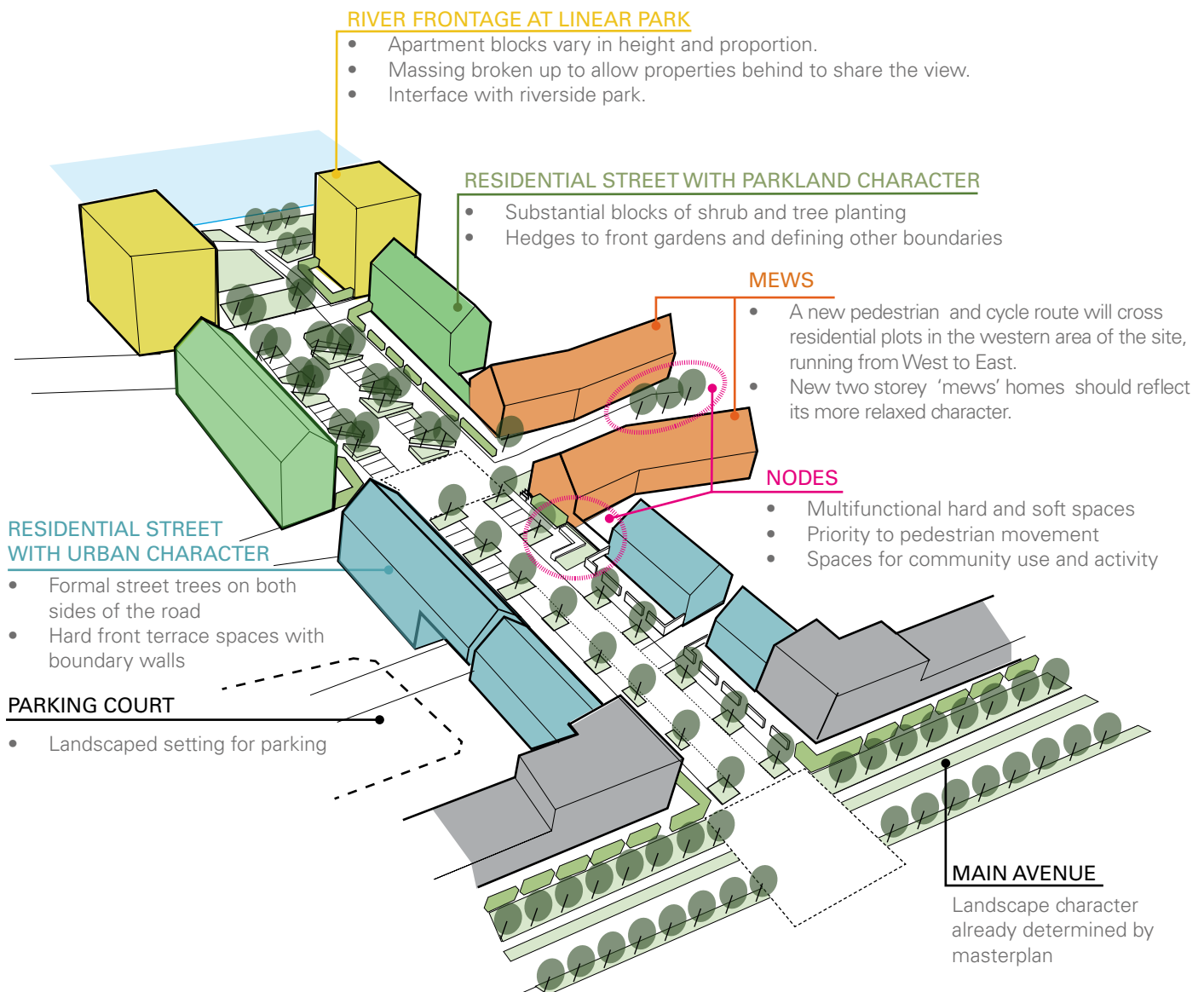


# CHARACTER AREAS

The Design Codes demonstrate how the landscape and streetscape changes from a more urban approach beside Main Street and the Fitting Out Basin, to a parkland character beside the river. Previous pages have identified the key principles and issues which need to be addressed by developers. The following pages explain how these principles apply to specific character areas:

- The river frontage at the linear park
- Residential streets with parkland character and residential streets with urban character
- Mews
- Nodes

The diagram below shows a typical street where the landscape and streetscape treatment will vary from a more urban approach beside Main Street to a parkland character beside the river. Refer to the diagram on Page 22 for the locations of each landscape/streetscape character type.



# RIVER FRONTAGE AT LINEAR PARK LANDSCAPE AND STREETScape CHARACTER

Flats at the riverfront will make the most of views to the river for as many residents as possible. This is achieved by breaking up the massing of flats beside the river so that properties behind are not overshadowed and share views.

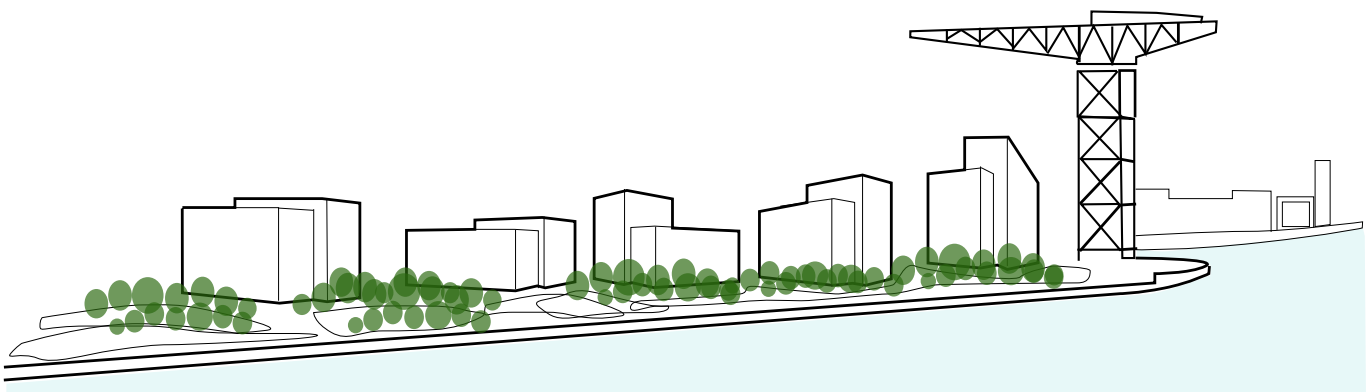
These apartment blocks should vary in height and proportion, bringing drama and a memorable edge to the waterfront.

There are opportunities to maximise the number of flats fronting or looking onto the river. This could be achieved in a number of ways – either by providing more development of a scale along the river frontage, or to extend the higher flatted blocks back further into the site, providing oblique views out to the river and a less monolithic edge.

Each development plot will have an edge that seamlessly interfaces with the proposed riverside park. The apartment blocks here will sit 'in the park', with a greater extent of greenspace extending up into each street and wrapping around the buildings. A central private space is created between the higher buildings which retains views through the block. Planting in this area will be the same as the riverside park.

Riverside development at the western side of the basin should have the highest residential density to create drama/ impact and frame the sizeable basin area. At the eastern side it would be appropriate to consider office or commercial uses.

The orientation and aspect of the riverside frontage provides opportunities for facades to incorporate balconies and other features such as vertical gardens.



Diagrammatic illustration: building heights vary, and the frontage of urban blocks is broken up with set-backs in some locations. This layout minimises overshadowing and allows permeable access and views towards the river from the interior of urban blocks.





## Key principles control new development along the river edge:

- **Scale:** blocks at a range of scales and heights at a scale that complements the linear riverside park
- **Variety:** uniformity is to be avoided, while utilising the materials and details included in the Design Codes
- **Articulation:** there should be differing scales, façade depth and layering
- **Edge:** new development should have an impact for people walking through the new linear park.
- **Corners:** interesting, characterful facade treatment at corners
- **Breaks:** between blocks to achieve light and view penetration
- **Landscape:** this should clearly differentiate between, and define, public and private spaces
- **Memorability:** development should contribute to a new memorable place of distinction and quality.

## Where residential streets meet the linear park

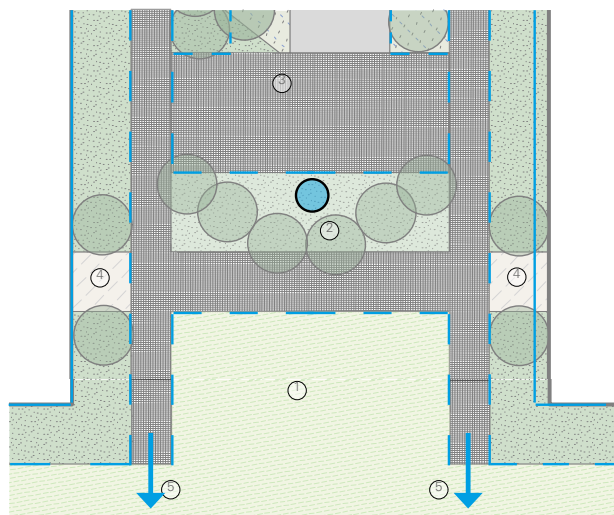
The masterplan locates linear residential streets so that they open up vistas towards the river. Where these streets meet the linear park parkland greenspace should extend into the street as shown in the diagrams to the right.

A hard landscaped area of public realm terminates access roads and provides a drop off space at the entrances to flats which face out onto the Clyde. It is important that these spaces are overlooked by adjacent properties.

The boundary to the plots should be defined by either brick walls to match the building or with planted hedges. The height of boundary treatments must allow casual surveillance of the park.

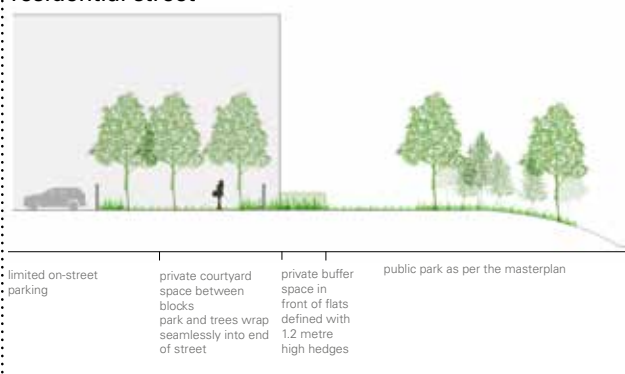
Level changes between ground floor level / external private space could be used - up to a maximum height of 600mm. This will ensure surveillance is maintained but provide some privacy.

## Indicative approach: residential streets meet linear park



- ① Parkland greenspace extends into street
- ② Feature artwork at the end of each street. Parkland sweeps up to the road end.
- ③ Hard landscaped area "designed-in" to street; extent of hard surfaces screened with planting (no parking)
- ④ Entrances to flats direct to street and across parkland
- ⑤ Direct connections into the park

## Indicative approach: cross section at end of residential street



## Not preferred

fencing & 'standard' back gardens



## Preferred

good surveillance & overlooking



## Precedent

- greenspaces within pedestrian environment
- small scale trees





# RESIDENTIAL STREETS WITH URBAN CHARACTER

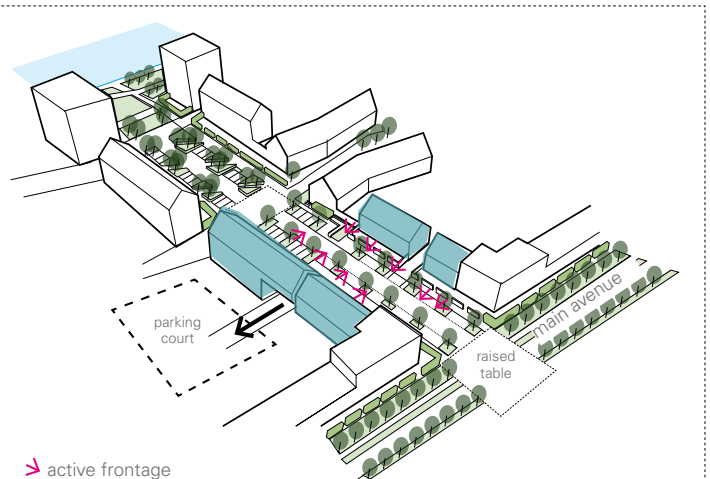
Refer to diagram on page 23 for the location of streets with urban landscape character.

These areas have a formal civic character which relates to the scale of the wider street, flats and town-houses on the new Main Avenue and Dumbarton Road. They are urban areas with formal planting and hard landscape materials. They should have an active frontage, with entry to homes from the street and gardens/ privacy areas enclosed by continuous boundary walls.

Detailing should be crisp and rigorously geometric. Materials reflect the palette for Main Avenue within the existing masterplan, and buildings and external features should use the same materials eg. brick boundary walls and metalwork.

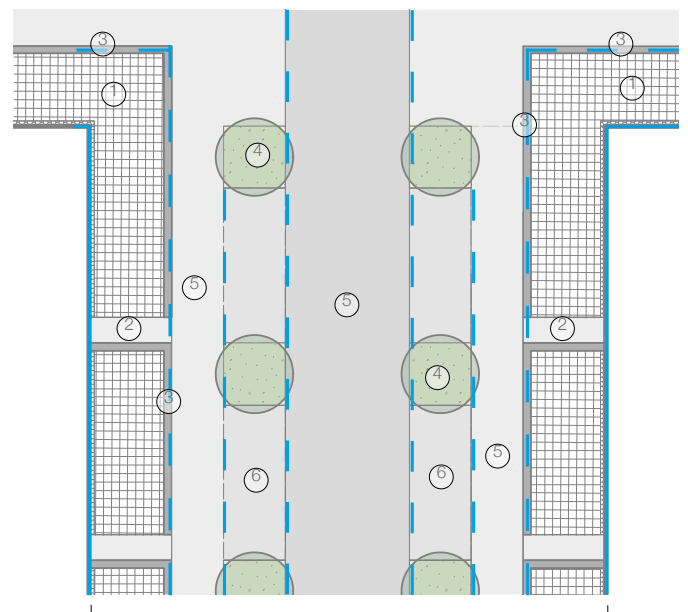


- ① Hard surfaced front courts with clearly defined thresholds
- ② Front entrances to all dwellings direct from the street
- ③ Brick boundary wall with brick cope and steel gates (1,200mm high)
- ④ Formal street trees (semi mature size) in large tree pits with grass and emergent bulbs beneath (max 12m centres)
- ⑤ Bitmac carriageway, parking bays and footways
- ⑥ Parallel visitor parking on both sides of road (max 2 spaces in a row before break)



Indicative approach; street plan & cross section

3.1m\* 2m\*\* 2.5m 4.8m 2.5m 2m\*\* 3.1m\*  
to tie in with Main Avenue



\*minimum 1.5 - 2 metres

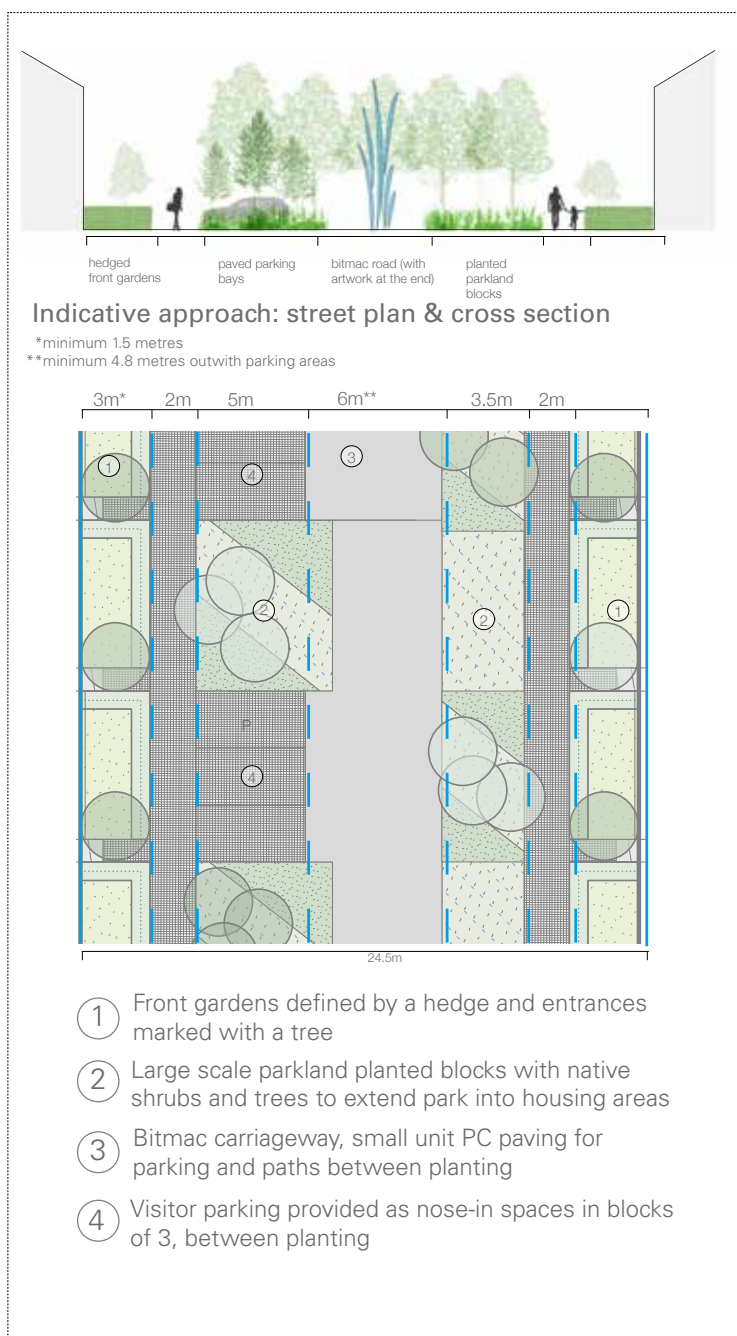
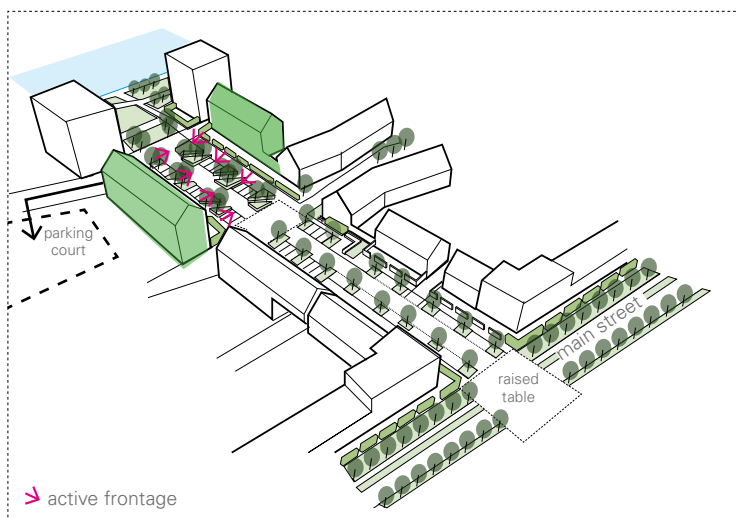
\*\*minimum 2 metres

# RESIDENTIAL STREETS WITH A PARKLAND CHARACTER

Refer to diagram on page 23 for the location of streets with parkland landscape character.

In this area, houses and blocks of flats feel as if they are set in the riverside park. The character is green and leafy with a palette of native plants and an informal feel. Visual and physical links to the park and the river are retained.

Planting should have a native coastal palette, paving is less formal and nose-in visitor parking clusters are broken up by swathes of shrubs and grasses that reflect the park character. Careful attention should be given to creating an appropriate micro climate given the exposed nature of the site.





# MEWS - CHARACTER

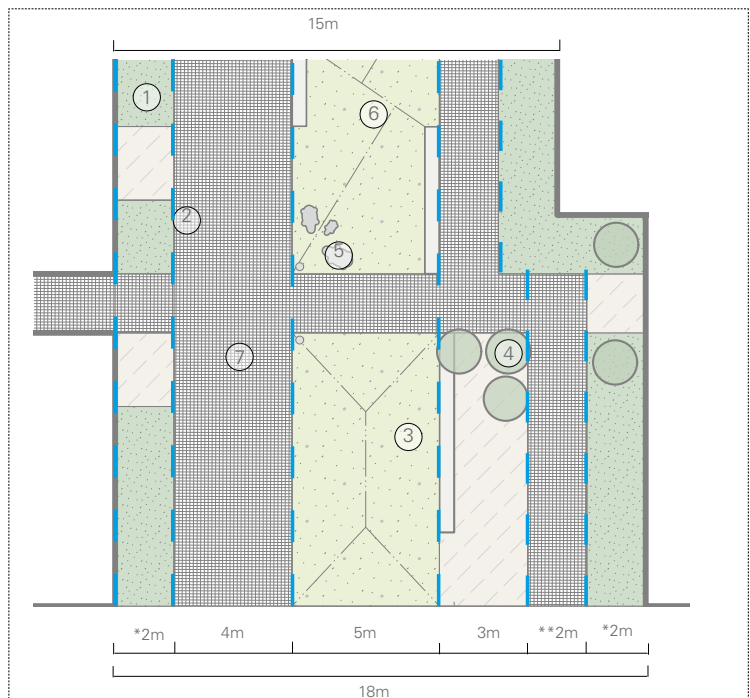
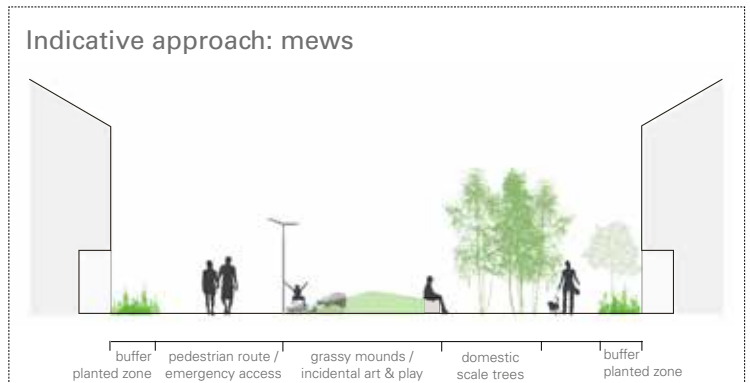
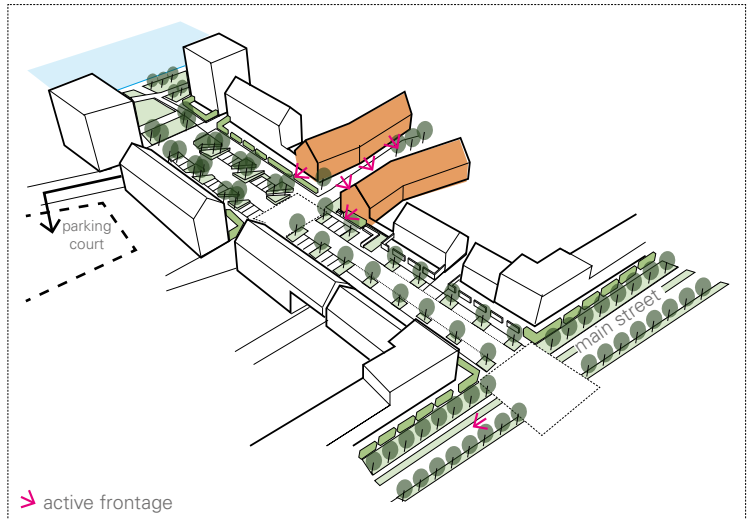
Refer to diagram on page 23 for the location of mews.

A new pedestrian and cycle route will cross residential plots in the western area of the site, running from West to East. (although service and emergency vehicles must be able to access). New two storey 'mews' homes should reflect its more relaxed character.

This should function as a social 'connector' - a fluid and seamless 'lane' that runs east to west through development plots and which is attractive and functional.

The Mews is smaller in scale (15 metre to 18 metre in width) and has a less formal character, with groups of trees defining convivial spaces for seats and bike racks. Small scale pedestrian paths cut off the lane to create convenient links to car parks.

Playful artworks and greenspace replace the traditional road and create incidental spaces for children and a unique sense of place.



\*minimum 1.5 - 2 metres

\*\*minimum 2 metres

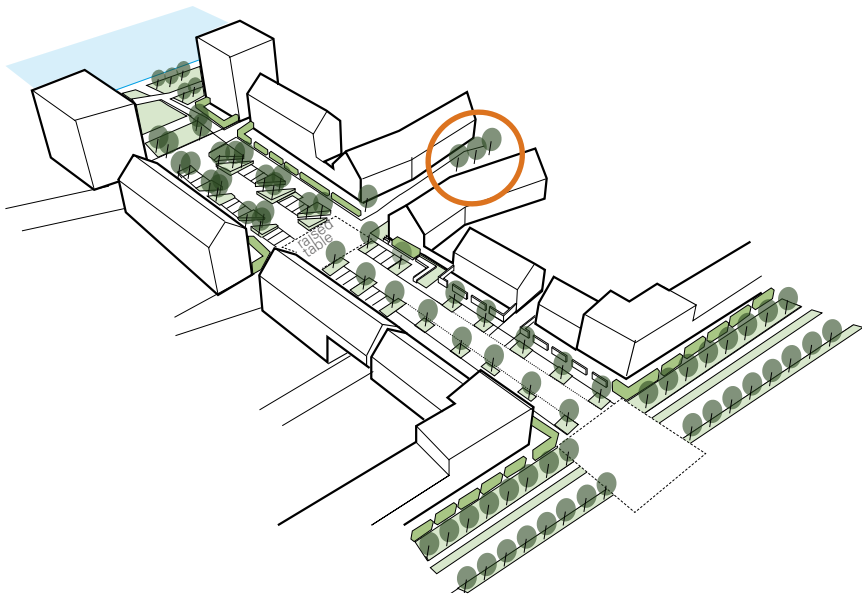
- ① Buffer planting - creates privacy and defines building thresholds
- ② No boundary walls/treatments to plot edges
- ③ Greenspaces running through the street - widening with steps in the building to create space for activities
- ④ Small domestic scale trees holding in spaces and corners
- ⑤ Incidental features/ sculptures for play
- ⑥ Landform within the greenspaces to create variety and interest and integral seating
- ⑦ Occasional / emergency vehicular access route - kept to the south (shadier) side of the street



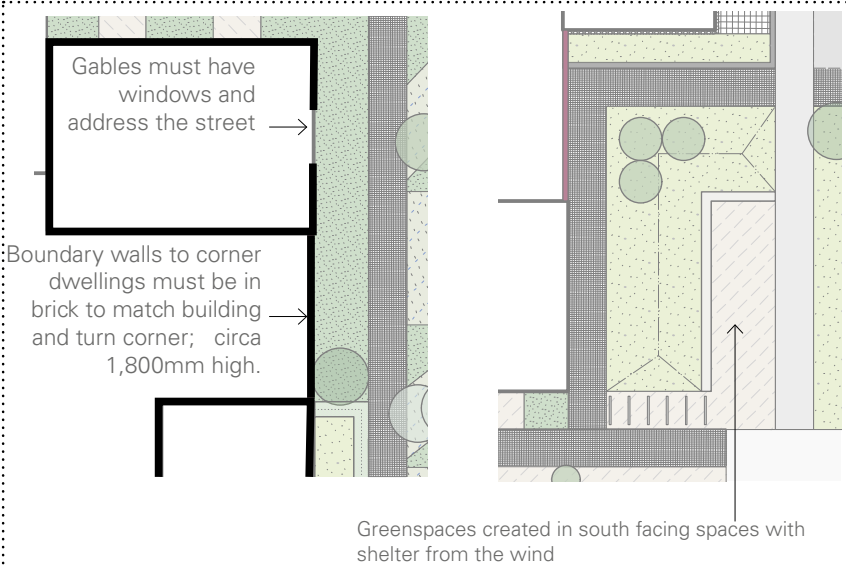
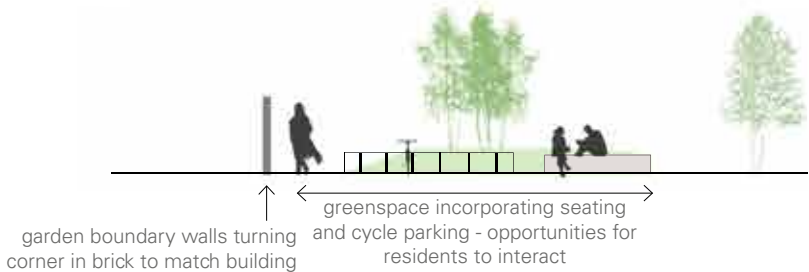
# NODES - CHARACTER

Nodes are intended as important hubs for orientation and for social activities. In these locations, appropriate flexible space should be created for small gatherings or positive street-scale community activities.

Some nodes are located within the pedestrian walkway running through mews housing Other nodes are at residential streets and will give pedestrians priority through a large raised table. This space will allow vehicle access on a 4.8m wide carriageway, bounded by seating areas, trees, bike and visitor parking.



## Indicative approach: nodes



Diagrammatic illustrative layout for design option for nodes at mews



## BACKCOURTS - LANDSCAPE CHARACTER

Larger plots designated for residential and mixed use have the potential for a balanced approach between landscape and limited built form. These spaces provide an excellent opportunity to enhance the development in a number of ways. These spaces could create a more dispersed parking solution, provide community space for activities such as food growing, create habitat and ecological benefit such as woodland or community greenspace, or be used for rain gardens and rainwater harvesting, urban forest and forest school activities.

Refer to further precedent images overleaf.

## PARKING COURTS - LANDSCAPE CHARACTER

Parking courts are functional spaces which will provide residents parking, bin storage and access.

Each should have a distinctive character and a strong sense of enclosure – either through built form, tree planting or appropriate boundary treatments. Courts should be designed to accommodate wheelie bins and recycling storage so that they do not dominate views. They should provide easy and direct access to dwellings. Parking areas should not reduce useable rear garden areas and sufficient space must be given to green buffers around the edges of these spaces, and to provision of trees both to the edge and at the centre of the space. To ensure the character of these areas is appropriate and does not deteriorate over time the indicative dimensions set out overleaf are suggested as the minimum for buffer and planted spaces.



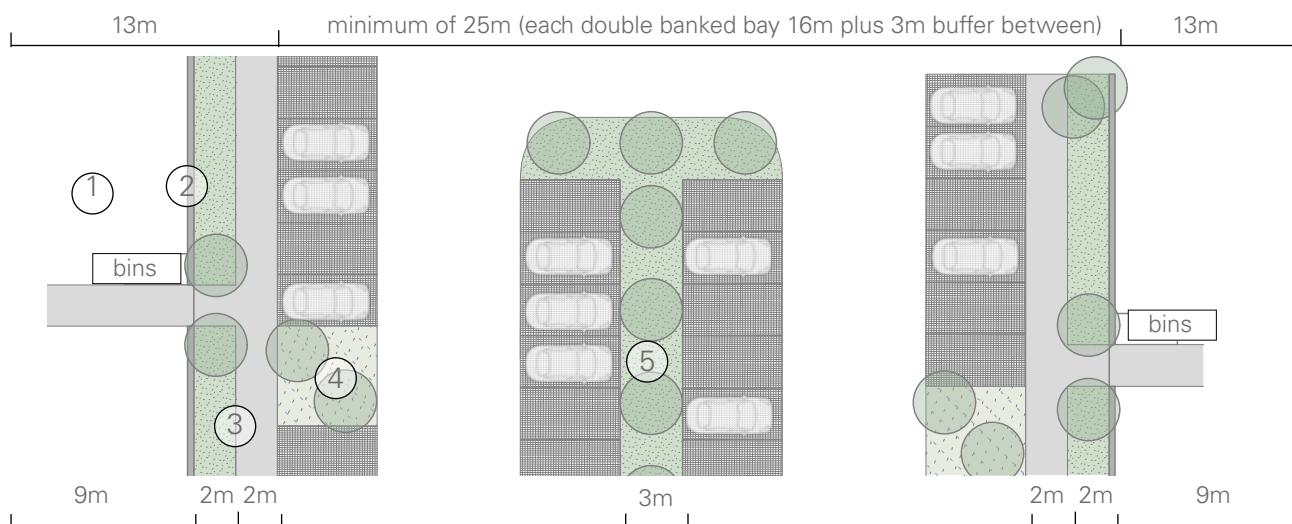
Parking integrated with greenspace



An integrated approach should be taken for parking areas and greenspace in backcourts, as demonstrated by this residential development at Shuna Crescent, Glasgow



## Indicative approach: parking (overall dimensions will vary dependent on the number of parking spaces required)



- ① back gardens / communal areas minimum 9m deep with bin storage at boundary allowing access from gardens and parking court
- ② boundary treatments should be solid and not permeable (timber or brick), maximum 1.2 metres high
- ③ a planted buffer including trees should be around edge
- ④ max 6 parking spaces around edge of space before broken up with a planted block - including multistem trees
- ⑤ central buffer with avenue trees between bays - minimum 2m width with trees planted in linked tree pits



precedent projects from top left, clockwise: backcourts accommodate a range of community uses; bin stores integrated into boundary walls and parking screened; avenue planting within parking areas; integrated landscape and parking areas

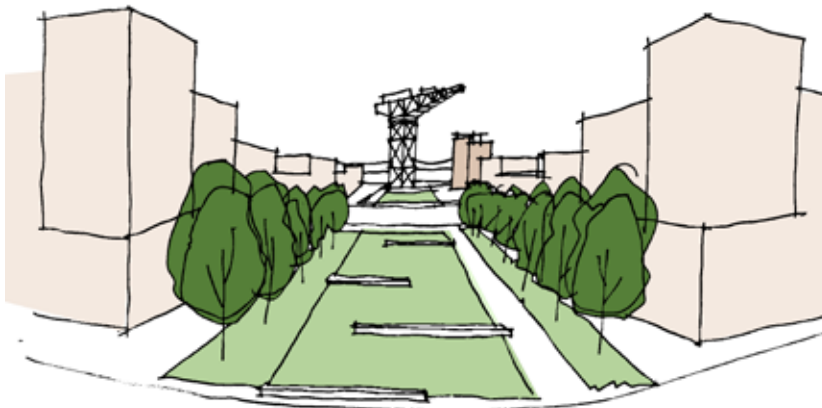


# MASTERPLANNED AREAS - STREETSCAPE CHARACTER



The boulevards linking the town centre and Queens Quay, together with the area around them, will be busy, mixed use areas.

New development at each Boulevard will have a 'Gateway' function. Where buildings are situated close to Dumbarton Road, they provide an opportunity to signal the transformation of the new Queens Quay to the wider Clydebank area. High quality designs and materials are therefore essential. Some retail and commercial development is encouraged in plots alongside the boulevards, located at the ground floor of each development block.



pedestrian boulevard leading to the Titan Crane



shared surface boulevard from masterplan design statement

Development at Main Avenue will complement its large scale 'civic' location.

Because of Main Avenue's generous scale, new development beside it will need to 'contain' the space within the street, so buildings of a reasonable height, together with a continuous façade, are essential. Building typologies and massing at Main Avenue will change from three storey townhouses at the most western plots where the density is lower, to 6-7 storey corners closest to the Basin and the Titan Crane.



active frontage + hedge as boundary treatment



limetree avenue & wildflower planting from IWA masterplan documents

# MATERIALITY

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# HISTORY + COMMUNITY

The redevelopment of Queens Quay will contribute to the wider regeneration of Clydebank. As such it is important to be sensitive to the cultural significance of the sites history.

The Queens Quay site was formerly the John Brown Shipyards. Before that it was owned by the Thomson brothers. Prior to that it was agricultural land owned by the Cochno's and was miles from Glasgow's urban borders. The rapid development of Clydebank is directly related to the ship building activities. People moved to the area in droves for work in the yards. As such it is important that this part of the history of Clydebank is referenced in the new buildings of Queens Quay.

Likewise it is to be recognised that the shipyards are also a substantial source of ill health in the community. Legacy issues around exposure to dust and chemicals including asbestosis still prevail from the shipyards labour force, who still reside in the area. The decline of the shipyards is also linked to the social deprivation in the area.

As such the proposals should have a **nod to the past** but must take cognisance of the circumstances this caused, and so **look to the future**. Consequently literal interpretations or representations of industry are not felt to be appropriate. New designs should be aspirational and point towards a healthier Clydebank, with a new urban heart.

The rich history and heritage of this site is reflected in the design and materials of the new buildings and will be taken forward in both Arts Strategy and the Street Naming Strategy.



# HISTORICAL SITE REMNANTS

Based on a 1918 Map of John Brown Shipyards the original workshops have been shown dashed on the adjacent diagram. Entrance Gates to the shipyard sat at the ends of Hall Street and Wallace Street. What remains today of historical significance includes the Titan Crane, the quay wall, the Town Hall, Library and the Baths. While the culturally important buildings are sandstone, the majority of buildings in the shipyard were brick. As such it is felt that a predominantly brick character is important for Queens Quay.

Beyond brick, it is clear that metal is of core significance to the site. Primarily steel, as this was the main product from the foundry. This was used to form the ships hulls, mechanics and structures. The shipyard and possibly even Clydebank, would not have existed without the foundry.

Secondary in importance to steel are copper, zinc (galvanising) and brass.

This historical and contextual analysis provides the basis for a robust material selection for the site. These materials are in wide use today, however when applied as a material palette they contain an embedded link to the social history of Clydebank.

The future designers of Queens Quay must take care to apply these materials sensitively, so that the buildings and streets exhibit a nod to the past but demonstrate a clear view on an aspirational future.



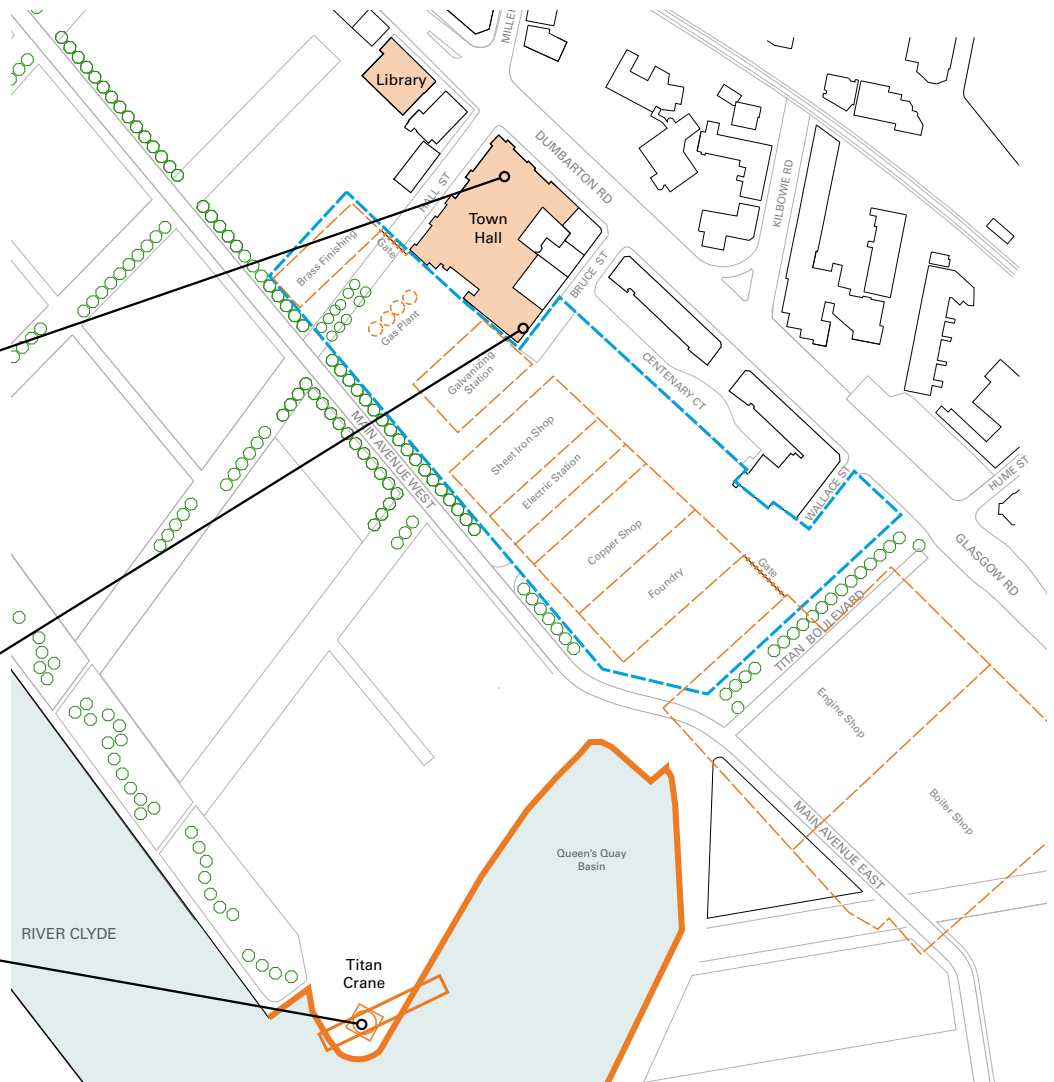
Clydebank Town Hall



Facade of Old Baths



Titan Crane and Queens Quay Basin



# ROBUSTNESS

To ensure the longevity of Queens Quay, a detailed analysis was carried out to identify materials used in the locality and how they had performed over time. It is known that the weather conditions on the banks of the Clyde differ to inner city locations, through prevailing winds, a harsh environment and a greater exposure.

Following the analysis the materials used were categorised into two broad categories, successful materials and less successful materials.

- The images below describe materials that are less successful
- Smooth panels, particularly in large format had not weathered well.
- Smooth tiling, brick or terracotta in a stack bond pattern do not weather well.
- Timber cladding weathers unevenly depending on prevailing winds.
- Laminated panels were noted to exhibit signs of delaminating.

As such designers should not propose this type of material or detailing at Queens Quay.

## Less Successful





# ROBUSTNESS

Materials that were successful exhibited a number of common traits. These should be utilised in the material selection and detailing at Queens Quay.

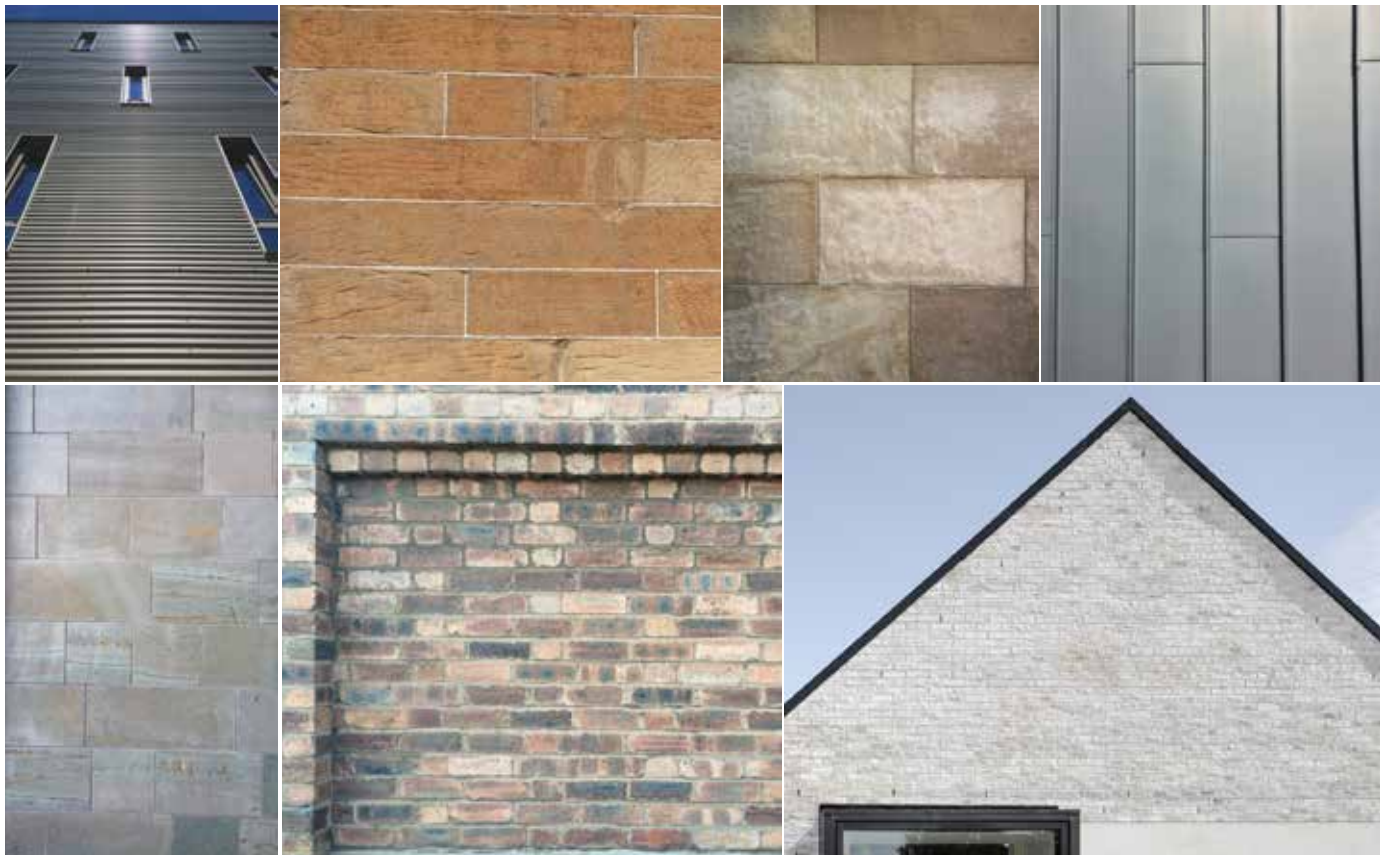
Surface texture is an important feature, traditional style brick laid in stretcher bond, multi bricks with a variety of colour and depth of tone, traditional stonework and ribbed metal, or standing seam panels all aged well in Clydebank.

There are also opportunities for a brighter colour palette for some small-scale features, linked to the Arts Strategy.

Cropped eaves lines are also important to achieve a consistent weathering appearance across the facade. The successful examples were evenly exposed to the elements and so were able to weather naturally.



## Material Successes



# MATERIALS PALETTE FOR QUEENS QUAY

## Simplicity

For a clean and consistent aesthetic across the site, no more than two cladding materials should be used on a building. Given the scale of the Queens Quay development some variety is encouraged within a pre-determined palette of materials. It is envisaged that the buildings may be articulated differently and may vary in colouration between sites, provided they draw from the material palette noted below.

Designers should carefully consider the local climatic conditions while specifying cladding with appropriate combustibility classifications for its type and location, taking cognisance of all relevant legislation and guidance.

## Planned Weathering

The local climate at Queens Quay can be severe, with strong prevailing winds and driving rain. Detailing on all buildings must be robust to ensure that rainwater is cast off eaves, copes and flashings.

Consideration should be given to the intended long term aesthetic of materials. For example, oil canning (wrinkling of the otherwise flat panel caused by heat expansion) of standing seam metal cladding must be considered and shown to be part of a planned aesthetic for the building. Brick detailing, such as corbelling, can provide features or areas of interest, however consideration needs to be given to the possibility of staining where water runs off or is directed.

## A Materials Palette for Queens Quay

For the reasons stated previously, a palette of materials is provided overleaf. These are deemed to be favourable by the Planning Authority at Queens Quay. The proposed materials of any building submitted to the Planning Authority in relation to Queens Quay should demonstrate compliance with this Materials Palette.

## The Materials

Preferred materials are listed on the following pages.

## Built Form - Materials Palette

### Cladding Material 01 - Brick

Traditional style clay stock brick, laid in stretcher bond, specified as a multi for depth of tone and variety of colouration. Colour matched mortar is preferred. Technical specification of the selected bricks should be appropriate to the local climate. Performance of moisture absorption, durability and active soluble salts must be considered.

Buff / Cream brick is favoured. All proposed bricks should tone with one on another on a single building. A contrasting Grey / Black brick could be considered to highlight key features. All bricks should tone with one another between sites.



**Preferred**  
Buff / Cream  
Traditional  
Multi Clay  
Stock Brick



**Preferred**  
Buff / Cream  
Traditional  
Multi Clay  
Stock Brick



**Not Preferred**  
Red / Brown  
/ Orange /  
Yellow / Blue  
Bricks  
Wirecut and  
or Extruded

### Cladding Material 02 - Metal

Steel - Corten is favoured due to its suitability to the environmental conditions. Care should be exercised to ensure that the Corten is pre-weathered off site, or detailed to weather in-situ without staining adjacent surfaces.

Copper, zinc and brass can be used however they should be considered in the context of surrounding buildings and streetscape. Smooth jointing is not to be used, standing seam or ribbed panels are preferred.



**Preferred**  
Expanded Metal  
Mesh  
Anodised  
Aluminium or a  
Metallic Finish



**Preferred**  
Profiled Anodised  
Metal Cladding  
Anodised  
Aluminium or a  
Metallic Finish  
Powder Coat.



**Preferred**  
Profiled Anodised  
Metal Cladding - Zinc,  
Copper or Brass.



**Not Preferred**  
Flat Smooth  
Preformed Panels



Windows

All windows should be provided in a dark grey finish, preferably polyester powder coated aluminium externally. Cills and balustrades should complement the window, window colour, cladding and aperture.



**Preferred**  
Dark Grey PPC  
Aluminium  
Windows

Could be aluminium clad timber or all metal window frames. Colour galvanised balustrades to match window. Colour matched precast or aluminium cills.



**Not Preferred**  
white PVC  
Windows  
or coloured  
UPVC windows  
with sightlines  
10% greater  
than aluminium  
equivalent.



**Balconies**  
are  
encouraged

Roofs

The roofs on all buildings should be specified to be in keeping with the material palette noted previously. Where roofs are tiled consideration should be given to the eaves and verge details.

**Verge Details** - The clean lines of a polyester powder coated aluminium flashings are preferred to proprietary dry verge systems. A skew wall detail, a parapet detail, or clipped eaves with a fibre cement slate roof finish could achieve this.

**Eaves Detail** - Raked box eaves are preferred to traditional boxed eaves details. Clipped eaves are preferred to projecting eaves, unless a clear case is made for the aesthetic approach.



**Preferred**  
Skew Wall



**Preferred**  
Parapet



**Not Preferred**  
Traditional  
Box Eaves



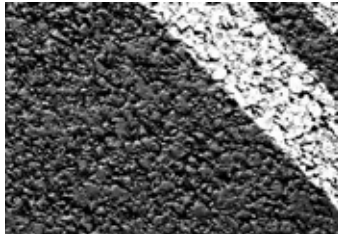
**Not Preferred**  
Proprietary  
Dry Verge

## Streets - Urban landscape and streetscape materials

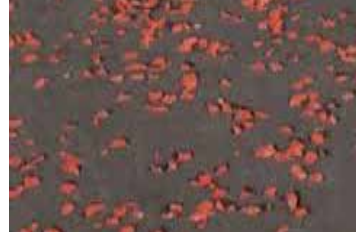
### Furniture & Materiality

Hard surfaces in the Urban character area should be of same quality and finish as the materials delivered on the Main Avenue, as part of the Queens Quay masterplan.

**Road carriageways and pavements** - hot rolled asphalt finish (no aggregate chippings preferred)



✓ Preferred  
hot rolled  
asphalt finish



✗ Not preferred  
coloured  
chippings

**Boundary treatments** - 1.2 metre high brick walls to match buildings; 'brick on end' copes (noting Requirement for specials/ resin bonded bricks with overhangs for protection)



✓ Preferred  
brick copes  
brick type  
& detailing  
to match  
building



✗ Not preferred  
railings or  
fences that  
create visually  
permeable  
boundary  
treatments

**Gates to entrances** - 1,200mm high steel gates - powder coated steel gates in dark grey to match building windows and metalwork; vertical bar style, no adornment



✓ Preferred  
colour to match  
metal work on  
facades (grey);  
vertical flat bar/  
simple design



✗ Not preferred  
adornment



✓ Preferred  
avenue trees  
(max 12m  
spacings)  
emergent bulbs  
in grass verges

#### Street trees in masterplanned areas

(specification controlled by previous planning applications - Lime (Tilia) Avenue tree planting to match the boulevard - (Tillia cordata 'Greenspire' preferred - trees to be planted in tree pits with load bearing tree soil, at max 12m centres (between 2 parallel parking spaces)

#### Screening for substations

✓ Preferred  
substation  
screened by  
planting  
or  
substation  
screened  
by bespoke  
enclosure



✗ Not preferred  
standard prefabricated enclosure  
visible from street





## Streets - Parkland landscape and streetscape materials

### Furniture & Materiality

The south end of the residential streets (beyond the node spaces) relate most strongly to the waterside park should be much greener and open in character - with a focus on the pedestrian and public spaces instead of the vehicle servicing and access.

Road carriageways and pavements - vehicular carriageways should be in bitmac with parking bays and pavements in 'textured' precast concrete setts in silver-grey tones - creating safe spaces for pedestrians and a softer feel. If appropriate to the drainage strategy these could also be permeable areas.



✓ **Preferred**  
parking bays and pedestrian areas in textured setts  
different textures define routes and spaces



✗ **Not preferred**  
single surface type for all road / pavement / parking areas  
no definition of routes and spaces

Boundaries must be defined - but done so with hedges and planting (minimum 900mm high) - not by wall or fences. Boundaries should not be left undefined or left open



✓ **Preferred**  
boundaries defined by hedges and planting



✗ **Not preferred**  
open boundaries to front gardens  
large areas of lawn

The general feel should be soft and leafy as if the housing is in the park



✓ **Preferred**  
trees planted in groups with shrubs/ground cover below  
visitor parking located within streetscape in blocks of 3 spaces



✗ **Not preferred**  
incutillage parking spaces

### Planting

The planting at the Parkland character area should include:

- blocks of native shrubs and grasses taking inspiration from the planting palette and overall structure as the park
- informal clusters of native trees (groups of 3-7)
- front gardens to include grass and a native tree in each front garden
- a native mixed species hedge (or beech) to the boundary (coastal hardy varieties)



## Mews - landscape and streetscape materials

**Furniture & Materiality** The furniture and materiality to the pedestrian 'mews' must reflect a more tactile and human scale environment, with playful elements and textures used to define public/semi public/private zones.

**Pedestrian environment** - The mews should be detailed in small format 'textured', exposed aggregate, pre-cast concrete sets in silver-grey tones.



✓ Preferred

'textured' sets in silver grey



✗ Not preferred

red mono block laid herringbone

**Boundary treatments** - there should be no boundary to front gardens - instead buffer planting strips created with opportunities for seating / personalisation



✓ Preferred

planting forms buffer to windows & area to personalise seating opportunities no boundary



✗ Not preferred

too narrow to buffer windows or provide opportunities for personalisation

**Furniture within the social spaces** along the mews should be made from timber and precast concrete and integrate with the design of the greenspace.



✓ Preferred

robust materials integrated with space design/landform



✗ Not preferred

generic design free standing bench - not integrated with planting

**Incidental spaces for play** (not play areas) should be incorporated within the street greenspace



✓ Preferred

objects for play integrated into the streetscape



✗ Not preferred

standard catalogue play equipment, rubber play surfacing, fenced off areas

**Preferred planting** - Clusters/ groups of small native street trees - to hold in spaces and edges.





## Nodes - landscape and streetscape materials

Nodes should take on the furniture and materials of the area within which they are located. The general feel should be civic and like a small plaza or pocket park rather than a street junction

Feature areas (at building thresholds, around seats, under trees, furniture and greenspaces) should be surfaced in a smooth, porous alternative material such as a resin bound gravel.



**Preferred**

feature seating areas and routes defined by greenspace and planting



**Not preferred**

wide open spaces

seating not located in spaces



**Preferred**

brick boundary walls to use same brick type as adjacent buildings to enclose back gardens on corners (walls to be 1800mm height)



**Not preferred**

timber fences as boundaries on residential streets



**Preferred**

seating arranged around edge of space  
greenspace and trees



**Not preferred**

standard road junction design

no usable greenspace or trees

# CASE STUDIES

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# CASE STUDIES

Each of the Case Studies on the following pages illustrates aspects of the requirements within the Design Codes. They are not intended as examples for designers to 'copy'.



## CITU Climate Innovation District, Leeds

The £800m Climate Innovation District in Leeds, part of the city's major South Bank Leeds regeneration plans, draws on Scandinavian best practice and harnesses the latest technology to deliver new low-carbon homes alongside manufacturing, leisure, offices, a care home, primary school and climate resilient public realm.

These are the first family homes being developed in Leeds City Centre for over 90 years. With an ambition to accelerate the transition towards zero carbon cities, the Climate Innovation District is forging a powerful collaboration with local and international partners to deliver an exemplar model of how to economically build zero carbon neighbourhoods that also provide healthier, smarter and better-connected cities.

### THEMES

- ☐ Street -urban character
- ☐ Street- parkland character
- ☐ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☐ Mews
- ☐ Mixed Use
- ☐ Parking
- ☐ Brick
- ☐ Simple eaves details
- ☐ Massing broken-up
- ☐ Vertically proportioned windows
- ☐ Simple proportions
- ☐ Good boundary treatments
- ☐ Building step up and down in height
- ☐ Active busy frontages





#### THEMES

- ☐ Street -urban character
- ☒ Street- parkland character
- ☒ Node (good quality, small scale public realm)
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- ☒ Good boundary treatments
- ☒ Building step up and down in height
- ☒ Active busy frontages

#### Urban form: case study - Marmalade Lane designed by Mole Architects for K1 Cambridge

Marmalade Lane has an urban character but still incorporates a range of shared spaces and communal facilities designed to foster community spirit and sustainable living.

These include extensive shared gardens as the focal space of the community, with areas for growing food, play, socialising and quiet contemplation.





## Urban form: case study - Laurieston Living, Glasgow

This is a unique place, constructed on a brownfield site in a regeneration area with many of the positive attributes of historic Laurieston, prior to high-rise flats undermining the tenemental way of life in the area. It has been designed to encourage the activity of the City Centre to extend south deep into the heart of the new community.

Inspired by the tenements and townhouses of Glasgow, the urban blocks of Laurieston are simple and well-proportioned. The housing plan defines a series of attractive urban streets and public spaces to create a rich and diverse urban residential district.



### THEMES

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- ☒ Good boundary treatments
- ☒ Building step up and down in height
- ☒ Active busy frontages



## Urban form: case study - Fernan Gardens, Glasgow

This development of housing for older people is focused on a protected landscaped courtyard which provides attractive parking and garden spaces.

It uses larger windows for and simple proportions to create an attractive place to live.



### THEMES

- ☒ Street -urban character
- ☐ Street- parkland character
- ☐ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☒ Mews
- ☐ Mixed Use
- ☒ Parking
- ☒ Brick
- ☒ Simple eaves details
- ☐ Massing broken-up
- ☒ Vertically proportioned windows
- ☒ Simple proportions
- ☒ Good boundary treatments
- ☐ Building step up and down in height
- ☐ Active busy frontages





### Urban form: case study - Pennywell, Edinburgh

Attractive homes with an urban character created using simple details and vertically proportioned windows

#### THEMES

- ☒ Street -urban character
- ☒ Street- parkland character
- ☐ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☐ Mews
- ☐ Mixed Use
- ☐ Parking
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- ☒ Massing broken-up
- ☒ Vertically proportioned windows
- ☒ Simple proportions
- ☒ Good boundary treatments
- ☒ Buildings step up and down in height
- ☒ Active busy frontages







#### THEMES

- ☒ Street -urban character
- ☒ Street- parkland character
- ☒ Node (good quality, small scale public realm)
- ☐ Riverfront - park
- ☐ Riverfront - Fitting Out Basin
- ☐ Mews
- ☒ Mixed Use
- ☐ Parking
- ☒ Brick
- ☒ Simple eaves details
- ☒ Massing broken-up
- ☒ Vertically proportioned windows
- ☒ Simple proportions
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- ☒ Building step up and down in height
- ☒ Active busy frontages



#### Urban form: case study - Ryles Yard, Eddington

There are a range of building heights and types. A shared palette of material and similar proportions integrates development across different plots. Buildings shape attractive urban spaces .

Urban form: case study -  
Goldsmith Street

This development features careful design of windows to minimise overlooking, and a asymmetric roof profile that allows good sunlight and daylight into the streets.

Provision for parking has been pushed to the perimeter, so the streets feel safe and 'owned' by pedestrians rather than cars.

Bin stores have been thoughtfully used in the front gardens to create buffer zones between the public footpath and the front doors, giving a humane gradation of public to private territory but one that is in no way oppressive.



THEMES

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## Kings Cross regeneration

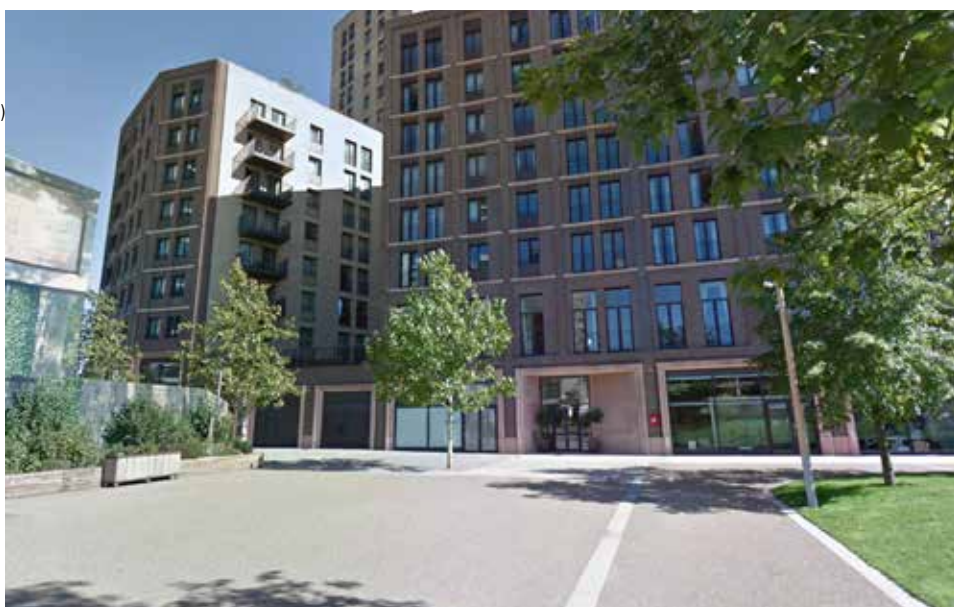
The redevelopment of the area around King's Cross train station is a compact, mixed-use, regeneration project in a very central location in London, close to one of its principal transport hubs. It has transformed a 27-hectare brownfield site into a vibrant and thriving area, including office, retail, hotel and residential uses.

It is an exemplar for sustainable place-making for a community with a long-term future that has minimal impact on the environment. There are many aspects to creating sustainable places. At King's Cross it has included promoting energy efficiency, encouraging green transport, reuse of heritage buildings and a massive program of tree planting.



### THEMES

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# QUEENS QUAY DESIGN CODES APPENDIX: HEALTH IMPACT ASSESSMENT

05 August 2020

## Queens Quay Design Codes – Comments from health inequalities perspective 12/07/2020 (Health and Social Care Partnership)

The aim of the design codes is fitting for Queens Quay where “New designs should be aspirational and point towards a healthier Clydebank, with a new urban heart”. The West Dunbartonshire HSCP Strategic Needs Assessment highlights that the greatest “burden of disease” for West Dunbartonshire is due to Cancer, Mental and Substance Use Disorders and Cardiovascular Diseases.

Place-based approaches can have a positive impact on these health outcomes over time and these Design Codes form part of a wider strategy to influence future population health. It may be helpful to see the ambition to improve health and wellbeing stated clearly at the start as this sets the tone and lays the thread for the elements within that are in line with the evidence on how place can have a positive impact on health.

### Evidence Based Elements

There is a distinct commitment to improving health and wellbeing and the elements within the codes are in clearly line with the following dimensions of the Place Standard Tool and the PHE Spatial Planning for Health: An evidence resource for planning and designing healthier place.

Place Standard Tool Dimensions	PHE Spatial Planning for Health - Modifiable Features
✓ Identity and sense of belonging (history, heritage and culture)	
✓ Streets and spaces (includes buildings, landmarks, public realms and views)	❖ Public realm improvements ❖ Compact neighbourhoods
✓ Support and services (waste management, type of shops)	❖ Increased access to facilities and amenities
✓ Public Transport	❖ Encouraging use of public transport
✓ Moving around (includes walking and cycling routes as well as space for wheelchairs, push-chairs and prams)	❖ Active travel to work and school ❖ Prioritising pedestrians and cyclists ❖ Improved street connectivity ❖ Improved walking and cycling infrastructure ❖ Increased walkability
✓ Natural space (parks, river, planting)	❖ Provision of access and engagement opportunities with the natural environment ❖ Aesthetic park improvements ❖ Neighbourhood tree planting ❖ Urban food growing
✓ Play and recreation (for children, young people and adults)	❖ Access to recreational space
✓ Social interaction (spaces to meet)	
✓ Feeling safe	
✓ Traffic and parking	❖ Improved air quality and reduced exposure to air quality through traffic calming measures and reduced parking
✓ Housing and community (quality, adaptable)	❖ Energy efficient homes ❖ Daylight and ventilation ❖ Provision of diverse housing types
✓ Influence and sense of control	



## Health Inequalities Impact

The Design Codes will have significant impact on the people who live at, work at and visit Queens Quay. Although the Design Codes may not be the appropriate place for detail on physical accessibility (largely covered by other guidance/regulations) they do influence the extent to which certain people may “feel” as if Queens Quay is a place for them.

It is worth noting that certain groups of the population are likely to experience poorer health and steps may need to be taken to ensure that the overall development is inclusive. This can be tackled through signage, public art, imagery in advertising (both for the site and on the site). A future discussion on the aspirations for the nature of outlets in the mixed-use area may be helpful in relation to food offer.

Although beyond the scope of the Design Codes, clarity would be helpful on the learning and educational arrangements for children and young people who will live at Queens Quay. In addition, for later detail, some thought may need to be given to types of housing that can adapt to people’s needs through the life-course ensuring that Queens Quay can become a “lifetime neighbourhood”.

## Note on Process and Limitations

Rapid desktop HIA (Health Impact Assessment) referencing:

- PHS Health Inequalities Impact Assessment Guidance
- Place Standard Tool V2 Draft and;
- PHE Spatial Planning for Health: An evidence resource for planning and designing healthier places.

Health Impact Assessment seeks to identify potential positive health impacts and how these might be enhanced and potential negative health impacts and how these might be mitigated. The assessment below is limited as is the subjective view of one person whereas HIA in full is a collaborative process involving a range of stakeholders.



## **WEST DUNBARTONSHIRE COUNCIL**

### **Report by the Strategic Lead - Regulatory**

**Planning Committee: 30<sup>th</sup> September 2020**

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**Subject: Queens Quay Design Codes: Sustainability Annex**

#### **1. PURPOSE**

- 1.1** To seek approval of the sustainability annex for the Queens Quay site.

#### **2. RECOMMENDATIONS**

- 2.1** It is recommended that the Committee approves the sustainability annex set out in Appendix 1 for consultation.

#### **3. BACKGROUND**

- 3.1** The Queens Quay site has an innovative district heating system which is near completion. Heat energy is extracted from the River Clyde using heat pump technology to achieve high temperatures and then distributed to each building through highly insulated pipes running beneath the roads. It is intended that the low carbon district heating network will supply every building on the site with heat energy for space heating and domestic hot water, thus helping to reduce the Queen Quay carbon footprint and helping the development towards carbon neutrality in line with the Council's ambition for West Dunbartonshire to be carbon neutral by 2045.
- 3.2** The Queens Quay Design Codes provides structured guidance for developers and designers taking forward development on Queens Quay. It is intended that Queens Quay will be one of the most sustainable new residential and mixed use areas in Scotland. New housing and other uses can be connected to the District Heating Network and by the inclusion of a Sustainability annex as part of the Design Codes it is intended that all future housing is connected to the District Heating System whilst the building fabric still meets the minimum quality standards of the Building (Scotland) Act.
- #### **4. MAIN ISSUES**
- 4.1** The draft Sustainability annex, is contained in Appendix 1 and it is based on maximising the benefits of the District Heating Network and to ensure that both carbon emissions and occupant costs are kept low throughout the Queens Quay development. The requirements of the annex brings together the District Heating Network with the building regulations put forward by the Building Standards Domestic Technical handbook set in the Building

(Scotland) Regulations 2004. New dwellings on Queens Quay must be designed to comply with Gold Aspect 1 of Section 7 in the Domestic Technical Handbook thus balancing the need for sustainable development with low occupant running costs. By connecting to the district heating network and achieving Silver Aspect 2 of Section 7 in the Domestic Technical Handbook most properties can then achieve Gold Aspect 1.

- 4.2** The Local Development Plan 2 contains a number of policies to encourage and support this Sustainability annex. Policy RE 4: Heat Generation requires developers to ensure that their developments can be connected to the district heating network and that the necessary capacity, infrastructure and land for future connections are safeguarded within the Queens Quay site. Policy RE 5 requires, where appropriate, all new buildings (unless they are exempt) to integrate low and zero carbon technologies within their construction etc. New development that connects to the district heating system will be in conformity with these two policies.
- 4.3** The forthcoming Renewable Energy Supplementary Guidance will provide further detail and specific considerations for developments within Queens Quay in line with the two policies noted in paragraph 4.2 above.

#### Next Steps

- 4.4** The draft Sustainability annex will be published for consultation for a period of 6 weeks. An advert will be placed in the local press and the draft Sustainability annex will be made available on the internet and to relevant organisations such as community councils. Following this, the consultation responses will be reported back to the Committee along with the finalised document which will form Annex:1 to the Queens Quay Design Codes.

### **5. PEOPLE IMPLICATIONS**

- 5.1** There are no people implications.

### **6. FINANCIAL IMPLICATIONS**

- 6.1** There are no financial implications.

### **7. RISK ANALYSIS**

- 7.1** There are no known risks to the Council.

### **8. EQUALITIES IMPACT ASSESSMENT (EIA)**

- 8.1** None.

### **9. CONSULTATION**

- 9.1** Consultation will be carried out as detailed in Section 4.4 above.

## **10. STRATEGIC ASSESSMENT**

**10.1** This proposal does not impact on any of the Council's strategic priorities.

**Peter Hessett**  
**Strategic Lead - Regulatory**  
**Date: 30<sup>th</sup> September 2020.**

**Person to Contact:** Pamela Clifford  
Planning & Building Standards Manager,  
Email: [Pamela.Clifford@west-dunbarton.gov.uk](mailto:Pamela.Clifford@west-dunbarton.gov.uk)

**Appendices:** Appendix 1- Queens Quay Design Codes : Sustainability Annex

**Background Papers:** None

**Wards Affected:** Ward 6 Clydebank Waterfront



# ANNEX: SUSTAINABILITY

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West Dunbartonshire Council are committed to a sustainable future and it is in the process of agreeing a Climate Change Strategy. The Council embraces the pathway to net zero carbon by 2045 and the reduction of emissions through mitigation measures and projects is the most important means of reducing our environmental impact and associated costs. Development on the Queens Quay site has an important role to play in meeting zero carbon targets.

### District Heating Network

Queens Quay has an innovative district heating network that will supply every building on the site with low carbon heat. Heat energy is extracted from the River Clyde, using heat pump technology to achieve high temperatures and then distributed to each building through highly insulated pipes running beneath the roads. This heat energy is then used to provide space heating and domestic hot water thus helping to reduce Queens Quay carbon footprint and helping the development towards carbon neutrality, in line with the Council's ambition for West Dunbartonshire to be carbon neutral by 2045.

### Low Running Costs

The district heating network is owned by West Dunbartonshire Council but operated and managed by Vital Energy. The unit cost of heat will compare favourably with a traditional gas equivalent when costs for all regular plant maintenance and annual gas safety inspections are properly considered because the customer heat interface units are supplied and maintained by the District Heating company. Consumers are expected to benefit from annual cost savings from the extremely low carbon energy source.

### Building Performance

The tool used to evaluate building energy performance for statutory compliance purposes is known as Standard Assessment Procedure, or SAP. This tool considers a variety of factors to determine how well a building will perform. Connecting to the district heat network provides a significant improvement in the SAP results. This measure alone will surpass Silver Aspect 1 in Section 7 of the Domestic Technical Handbook set in the Building (Scotland) Regulations 2004.

### Fabric Performance

To ensure that carbon emissions and occupant running costs are kept low, it is necessary to ensure a minimum performance specification for the buildings at Queens Quay. Consequently, every new dwelling in Queens Quay must be designed to comply with Silver Aspect 2 of Section 7 in the Domestic Technical Handbook. This will ensure that buildings on Queens Quay are efficient in their consumption of energy. Simply by connecting to the district heat network and achieving Silver Aspect 2, most typologies can achieve Gold Aspect 1 of Section 7 in the Domestic Technical Handbook. This will ensure very low carbon emissions from the buildings on Queens Quay and is therefore the sustainability standard for Queens Quay

The Sustainability Standard for Queens Quay	
Minimum Fabric Performance Standard	Silver Aspect 2.
Minimum Carbon Emissions Performance Standard	Gold Aspect 1.

### Photovoltaics

Gold Aspect 1 will be achievable without the use of photovoltaics. However, if a developer chooses to install photovoltaic panels then the occupants running costs and carbon emissions will be further reduced. Developers may wish to consider this as a sales option, which could be offered to prospective home buyers.

### Working with Industry

West Dunbartonshire Council wish to ensure that Queens Quay remains attractive to prospective investors and future residents. The levels Silver Aspect 2 and Gold Aspect 1 can be achieved using current day building techniques and components